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To: Councillor Crockett, Convener; Councillor Milne, Vice-Convener; and Councillors Allan, Boulton, Corall, Cormie, Delaney, Finlayson, Grant, Jaffrey, MacGregor, McCaig, Jean Morrison MBE, Thomson and Yuill.

Town House,
ABERDEEN 29 October 2012

ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE

The Members of the **ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE** are requested to meet in Council Chamber - Town House on **TUESDAY, 6 NOVEMBER 2012 at 2.00 pm.**

Please note that all members of the Council are invited to attend the meeting for the presentation at item 1.1 on the agenda.

JANE G. MACEACHRAN
HEAD OF LEGAL AND DEMOCRATIC SERVICES

BUSINESS

1 PRESENTATION

1.1 Aberdeen Masterplanning Process (Pages 1 - 12)

2 REQUEST FOR DEPUTATIONS

2.1 Request for Deputation by Kingswells Community Council in Relation to Item 9.1 on the Agenda

3 DETERMINATION OF EXEMPT ITEMS OF BUSINESS

3.1 Members are Requested to Resolve that any Exempt Business on this Agenda be Considered with the Press and Public Excluded.

4 MINUTES AND APPOINTMENTS TO OUTSIDE BODIES

4.1 Minute of Previous Meeting of 11 September 2012 (Pages 13 - 32)

4.2 Appointments to the North East Scotland Fisheries Development Partnership (Pages 33 - 34)

The Partnership acts as an advisory body to the Councils of North East Scotland and the industry and provides support to fishing dependent communities.

(5 Members to be appointed).

5 MOTIONS

5.1 Notice of Motion by Councillor Malone - Referred from Council on 13th June 2012 (Pages 35 - 38)

“To instruct the Director of Enterprise, Planning and Infrastructure to support the request by the International School of Aberdeen and install a safe pedestrian crossing similar to other crossings on North Deeside Road at the entrance of the school. At present there is no safe crossing and pupils and community members must negotiate a very busy road”

5.2 Notice of Motion by Councillor Townson - Referred from Council on 13 June 2012 (Pages 39 - 44)

“Council agrees to develop a youth employment strategy for Aberdeen in consultation with relevant public and private organisations.”

5.3 Notice of Motion by Councillor Jennifer Stewart - Referred from Council on 10th October 2012

“That this Council considers the possibility of scheduling future meetings of the Development Management Sub Committee so as to avoid meetings being held in July, with a suggestion that meetings be held in late June and early August instead to assist with meeting performance indicators for dealing with planning applications.”

5.4 Motion by Councillor Yuill - The Glens and Deeside Way (Pictish Way) (Pages 45 - 48)

6 PERFORMANCE MANAGEMENT AND SERVICE WIDE ISSUES

6.1 *Delivering Transformational Change in Asset Management and Operations Services (Pages 49 - 52)

(Please refer to separate appendix at item 12.1 on the agenda)

7 **FINANCE**

7.1 Capital Monitoring - Enterprise, Planning & Infrastructure Projects (Pages 53 - 58)

7.2 2012/13 Revenue Budget Monitoring (Pages 59 - 70)

8 **ENTERPRISE**

8.1 Visit Aberdeen (Pages 71 - 74)
(Please refer to separate appendix at item 12.2 on the agenda)

8.2 Aberdeen Digital Connectivity (Pages 75 - 82)

8.3 International Partnership and Twinning Applications (Pages 83 - 94)

8.4 Support for Local Employability Training Providers (Pages 95 - 98)

9 **PLANNING AND SUSTAINABLE DEVELOPMENT**

9.1 West Huxterstone, Kingswells Masterplan (Pages 99 - 190)

9.2 Loirston Development Framework (Pages 191 - 216)

9.3 Maidencraig Masterplan (Pages 217 - 238)

9.4 Adoption of Masterplans, Planning Briefs and Development Framework as Supplementary Guidance in Support of the Aberdeen Local Development Plan (Pages 239 - 306)

9.5 Adoption of Draft Supplementary Guidance to Local Development Plan (Pages 307 - 362)

9.6 Supplementary Guidance: Wind Turbine Development in Aberdeen City (Pages 363 - 388)

9.7 Technical Advice Note: The Repair and Replacement of Windows and Doors (Pages 389 - 424)

9.8 Aberdeen City and Shire Strategic Development Plan (Pages 425 - 486)

9.9 MUSIC Project Update (Pages 487 - 494)

10 INFRASTRUCTURE

PROPOSED TRAFFIC ORDERS COMING TO COMMITTEE FOR THE FIRST TIME

- 10.1 Various Small Scale Traffic Management and Development Associated Proposals (New Works) (Pages 495 - 506)

TRAFFIC ORDERS AT THE MIDDLE STAGE OF THE PROCEDURE

- 10.2 Justice Mill Lane - Traffic Management Proposals - Initial Statutory Consultation (Pages 507 - 512)

TRAFFIC ORDERS AT THE LAST STAGE (WHERE THE MAIN STATUTORY OBJECTION PERIOD IS OVER)

- 10.3 Traffic Orders at the Final Stage of the Statutory Process (Pages 513 - 518)

OTHER INFRASTRUCTURE, ROADS AND PARKING ISSUES

- 10.4 Aberdeen Western Peripheral Route - Advance Works & Services (Pages 519 - 522)

(Please refer to separate appendix at item 12.3 on the agenda)

- 10.5 Strategic and Local Transportation Projects (Pages 523 - 556)

- 10.6 Permit Misuse Policy (Pages 557 - 562)

- 10.7 Garthdee Controlled Parking Area - Permit Changes (Pages 563 - 566)

- 10.8 City Wide Flooding Issues (Pages 567 - 574)

- 10.9 Market Street/ Guild Street Junction - Rationalisation of Turning Movements to Improve Capacity (Pages 575 - 580)

11 COMMITTEE BUSINESS STATEMENT AND MOTIONS LIST

- 11.1 Committee Business Statement (Pages 581 - 602)

- 11.2 Motions List (Pages 603 - 610)

12 ITEMS WHICH THE COMMITTEE MAY WISH TO CONSIDER IN PRIVATE

- 12.1 Supplementary Report - Delivering Transformational Change in Asset Management & Operations Services (Refer to item 6.1 on the agenda) (Pages 611 - 658)
- 12.2 Appendix - Visit Aberdeen Draft Business Plan - Stage Two Executive Summary (Refer to item 8.1 on the agenda) (Pages 659 - 676)
- 12.3 Appendix - Aberdeen Western Peripheral Route - Advance Works & Services (Refer to item 10.4 on the agenda) (Pages 677 - 678)

Please note that reports marked with an * have implications for agreed Priority Based Budget (PBB) options.

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Aberdeen Masterplanning Process
REPORT NUMBER:	EPI/12/231

1 PURPOSE OF REPORT

- 1.1 This report updates the Committee on the principles, and clarifies administrative aspects, of the Aberdeen Masterplanning Process, adopted as supplementary guidance to the Aberdeen Local Development Plan. The Process gives direction as to how masterplans will be prepared to guide future development. The Aberdeen Masterplan Process can be viewed at:
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31782&slD=14394>

Members please note that the report will be accompanied by a presentation at the committee meeting.

2 RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
- (a) retain the adopted Aberdeen Masterplanning Process;
 - (b) note the administrative changes to the community engagement in the process namely:
 - (i) Action Note to be circulated immediately following public meetings;
 - (ii) Community councils to be given advance copy of draft masterplans a minimum 14 days prior to the relevant Committee meeting;
 - (iii) Written response from statutory consultees, including community councils, to be appended to Committee reports following public consultation.

3 FINANCIAL IMPLICATIONS

- 3.1 Generally developers will meet the cost of preparation of masterplans including all consultation and engagement. On average, the cost of preparing a masterplan or development framework for a large area is approximately £250,000. This will vary depending on the complexity and scale of the site and generally includes input from design,

planning, transportation, engineering, environmental assessment and public relations. Given the number of masterplans highlighted in the Appendix to this report, this represents a potential saving to the City Council in excess of £3 million. This figure could be much higher depending on the number of phased masterplans required to support development.

- 3.2 The Masterplanning Process generates proposals that will result in efficiencies in the determination of future planning applications related to development frameworks and masterplans, leading to a reduction in Council staff time to assess future detailed proposals.

4 OTHER IMPLICATIONS

- 4.1 There are no known property, legal or equipment implications arising from this report.
- 4.2 Masterplanning reduces the risk of piecemeal and inappropriate development in the City. Mixed use development and the efficient use of land will contribute towards the Council's aim of promoting sustainable development.
- 4.3 Masterplanning contributes to efficiencies in determining future planning applications. By encouraging acceptable forms and uses of new development to be agreed publicly, it allows more informed decision making to be made earlier, saving time and resources for staff, applicants and the public.
- 4.4 In accordance with the Local Development Plan, Appendix 4, The Action Programme and the Infrastructure and Developer Contributions Manual, masterplans explicitly reference how development will address requirements for cumulative and local transport infrastructure, public transport routes and walking and cycle paths as far as they are known at the early stages of planning.
- 4.5 A Strategic Environmental Assessment has been undertaken on all masterplan sites as identified in the Aberdeen Local Development Plan as part of the preparation for the proposed Aberdeen Local Development Plan.

5 BACKGROUND/MAIN ISSUES

- 5.1 The Aberdeen Masterplanning Process has been prepared as a guide to developing and delivering masterplans in Aberdeen City. The adopted Aberdeen Local Development Plan emphasises the need to masterplan all major sites and identifies 11 Masterplan Zones in the City. Developers are expected to prepare Masterplans for each zone and coordinate the planning and delivery of associated infrastructure requirements. Reflecting current national design guidance, the Process seeks to enable the delivery of sustainable places.

5.2 The Masterplan Process identifies the need to consider an integrated approach to site planning, urban design, sustainable transport, ecology, landscaping and community involvement. There is also a recognised need to ensure quality of design for new developments in Aberdeen, from the largest and most significant areas of change to smaller individual sites. The Process identifies 3 main types of supplementary guidance: Development Frameworks, Masterplans and Planning Briefs

- **Development Frameworks** set out a baseline, or two dimensional, spatial framework for the way in which large areas, or areas in multiple ownership, are to be developed.
- **Masterplans** present detailed guidance, in three dimensions, on design matters, specific topics, or land uses for sites in single ownership, or for buildings or external spaces within a larger development framework.
- **Planning Briefs** concentrate on site specific guidance highlighting planning policies, constraints and opportunities. A planning brief informs any planning application made for the site. Planning Briefs can range from small sites in conservation areas up to the scale of a masterplan.

5.3 For some sites a basic development framework may require to be developed into a more detailed three dimensional Masterplan, describing and illustrating the proposed urban form for each phase of development. The initial sites where this is expected are identified by the Local Development Plan. Masterplans may include design codes and will include an estimated timeline for delivery as well as a response to the ALDP Action Programme and Infrastructure Requirements.

5.4 All Development Frameworks and Masterplans are expected to be adopted as supplementary guidance to the Aberdeen Local Development Plan.

5.5 Each Development Framework, Masterplan and Planning Brief must address the following 4 key themes, although the level of detail will be dependent on the scale and complexity of the proposed development site.

- **Context**

This includes a detailed description and appraisal of the site and surrounding area, planning policy review and options appraisal combined with a vision statement outlining the aims and objectives of what the supplementary guidance will achieve.

- **Identity**

This details the design principles of the masterplan that will be used to inform the more detailed design stages including settlement

pattern, street layout, open spaces, landscape, building scale and massing, infrastructure and services.

- **Connection**

This includes accessibility within the site, concentrating on pedestrian, cycle, public transport and general access, external links and the infrastructure necessary to support these connections.

- **Communication**

It is essential that a communication and engagement plan is developed and agreed at inception to ensure appropriate levels of community involvement can be maintained throughout masterplan preparation.

- 5.6 All development frameworks and masterplans are required to include a Delivery Strategy, to which all applicants will have to comply, specifying how the key strategic elements of infrastructure will be delivered for the entire development framework or masterplan site. These elements will be site specific but would normally include transportation infrastructure (footpaths, cycle ways, roads), public transport provision, education provision, community facilities, strategic open space etc.

The strategy will be expected to link infrastructure provision to a Phasing Plan for the entire site and to clearly identify and justify thresholds and trigger points for the delivery of these key strategic facilities (based, for instance, on the capacity of the road network identified in an agreed transportation assessment or the capacity of local schools based on advice from the Council's education service)

Where sites are in multiple ownership it will obviously be essential that evidence is provided to show that all landowners and prospective developers work together to agree the Delivery Strategy.

- 5.7 In order to prevent piecemeal development and problems with delivery of key infrastructure for development framework or masterplan sites it is expected that an application for planning permission in principle or detailed planning permission for the entire site based on the development framework or masterplan will be submitted in advance of any other applications for development of the site. This application and its associated legal agreement will be the mechanism for controlling development and infrastructure delivery for the site as a whole.

- 5.8 All planning applications for national or major development must comply with the Pre-Application Consultation (PAC) process outlined in the Planning etc (Scotland) Act 2006. If PAC is required, applicants must submit a Proposal of Application Notice at least 12 weeks prior to the submission of the planning application. As there is no upper time limit for consultation, it is recommended that Notice be submitted early to coincide with the masterplanning process to enable all engagement to contribute to the consultation process and avoid duplication.

- 5.9 Methods of engagement will vary according to the scale, complexity and nature of the proposed development. The engagement process will include, but not be exclusive to, the following:

Elected Members

Elected Members represent constituents in the community, have detailed knowledge of issues affecting their wards and should be involved in the Process. This need not continue beyond information gathering and should not prejudice any Member's position in determining future planning applications.

Transport

Roads authorities, public transport operators (including bus and rail), NESTRANS and Transport Scotland must be involved as necessary throughout the development of the proposal.

Agencies

Scottish Environmental Protection Agency, Historic Scotland, Scottish Natural Heritage or other statutory or relevant consultees as appropriate will be involved in the early stages of planning to determine key issues. In addition, utilities, NHS Grampian, Grampian Police Architectural Liaison Officer, emergency and other services, should also be consulted.

Community

Local communities, community councils and other groups must be actively engaged throughout the Masterplan process.

Other

Planning Obligations Officer.
Landowners and tenants affected by proposals.

- 5.10 The minimum administration of the masterplanning process itself can be summarised as follows:

Task	Responsibility
Identify and allocate sites to be masterplanned through the Local Development Plan	ACC
Scope of work for masterplan	Developer/ACC
Engagement at inception to establish background information and identify issues;	Developer
Public meeting, presentation, workshop or exhibition as appropriate to confirm issues and review initial design options and concepts;	Developer
Develop initial design concepts with Masterplan team	Developer/ACC
Second public meeting, presentation, workshop or exhibition as appropriate to confirm design concepts;	Developer
Review and update draft masterplan This includes detailed scrutiny and analysis of proposals by officers, co-ordinated by the	ACC/Developer

Masterplan team.	
Informal submission of final draft masterplan to community councils a minimum of 14 days prior to the Committee meeting;	ACC
Report to Enterprise Planning and Infrastructure Committee for development frameworks and single masterplans (as strategic documents), or Report to Development Management Sub-Committee for masterplan phases as identified in an approved development framework and planning briefs (as technical documents); All documents to act as interim planning advice (see definitions below)	ACC
Public consultation on final draft as interim planning advice approved by the relevant Committee	ACC
Report to relevant committee on the results of that public consultation	ACC
Submission to Scottish Ministers for approval to adopt as supplementary guidance (see definition below) to the development plan	ACC

5.11 Definitions

‘Interim Planning Advice’ – this specifies that the Development Framework, Masterplan or Planning Brief is in the public domain and available for the purposes of a formal City-wide consultation. As such it becomes a material consideration in the determination of any planning application. The duration of consultation will typically be 4 weeks, given that public engagement has taken place throughout the preparation of the proposals. This allows responses to be collated and reported back in the next committee cycle.

- 5.12 **‘Supplementary Guidance’** (SG) – this is adopted and issued by a planning authority in connection with a Local Development Plan, as a result any such guidance will form part of the Development Plan. Any masterplan seeking adoption as supplementary guidance to the development plan, which applies to all of those in Aberdeen City, must accord with the requirements of the Planning etc. (Scotland) Act 2006. In essence all development frameworks, masterplans and planning briefs must be subject to a formal consultation period pending committee approval and prior to submission to the Scottish Government for consideration to adopt as supplementary guidance to the development plan. Before adoption, the SG must be publicised and a period for representations to be made specified, this includes the targeting of key consultees and stakeholders who may want the opportunity to comment. Following the specified consultation period and as a result of comments received, relevant changes will be made to the final document before reporting back to committee and subsequent submission to Scottish Ministers. After 28 days have

elapsed, the authority may then adopt the guidance unless Scottish Ministers direct otherwise.

- 5.13 Throughout each stage detailed in section 5.8 above, the developer and their agents are expected to work with input from appropriate officers in the City Council and the Planning Obligations Officer, co-ordinated by the Masterplanning Design and Conservation Team.
- 5.14 Appendix 1 to this report highlights the development frameworks and masterplans and their status as supplementary guidance for information. Appendix 2 to this report outlines the Actions and expected level of detail from supplementary guidance in the context of the whole planning process.
- 5.15 We place high value on early engagement in the planning process. As such, it is much more constructive to engage with communities early on and ensure that issues are addressed prior to completion of masterplans. As part of the actual masterplanning process, an Action Note will be published by the developer or their agents following every public meeting and circulated between the developer, community council and City Council to ensure consistency of message and reduce the chance of any misunderstanding between parties.
- 5.16 Although there is no statutory obligation to consult with community councils prior to committee reporting, it is current practice to circulate masterplans to community councils prior to the relevant Committee meeting for information on what will be made public during the consultation process. This ensures awareness and understanding of the content of the finalised draft document. Given that community councils and other stakeholders views have been considered and addressed where appropriate during the preparation of the masterplan, this will be formalised to a minimum 14 day period prior to the committee meeting to ensure consistency in all masterplans. It does not supersede the statutory public consultation, during which the community councils' formal views, along with other stakeholders will be considered.
- 5.17 Following statutory consultation, in the second report to Committee, we will include written responses to the consultation, including those from community councils, with the report, in addition to a summary of general issues and our responses to them.

6 IMPACT

- 6.1 The proposal contributes to the Scottish Government Priorities, in particular safer and stronger – improved opportunities and quality of life, and greener – improving the built environment and sustainable use of our natural resources.

- 6.2 The proposal contributes to the following Single Outcome Priorities: 1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2 – we realise our full economic potential with more and better employment opportunities for our people; 10. We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.
- 6.3 The report contributes to the 5 Year Business Plan in terms of objective – Communication and Community Engagement; sharing our plans and aspirations for the city, – delivering an up-to-date plan, – facilitating new development projects to improve Aberdeen’s living environment and, – support open space initiatives.
- 6.4 The report contributes towards the Aberdeen City Council Administrations vision for Aberdeen – The Smarter City: 2012 – 2017, particularly creating a City which is a great place to live, bring up a family, do business and visit. It specifically contributes to the follows objectives: Governance – encouraging citizens to participate in design and development; Living – improving opportunities for physical activity; Environment – energy efficient design and construction, attractive streetscapes and access to green space; Economy – improve access to range of affordable housing; Mobility – encourage cycling, walking and promoting a sustainable transport system which reduce carbon emissions.
- 6.5 The report is consistent with the Council’s Corporate Plan in particular with regard to delivering high levels of design from all development, maintaining an up-to-date planning framework, sustainable development and open space provision.
- 6.6 The report is consistent with the Planning and Sustainable Service Plan, in particular engaging the community in the planning process, and the delivery of masterplans/development frameworks in line with The Aberdeen Masterplanning Process.
- 6.7 An Equalities and Human Rights Impact Assessment has been prepared. In summary, many of the aims of the proposal will have positive benefits, including:
- encouraging effective public engagement at the earliest opportunity in the planning process;
 - ensuring that this engagement continues throughout;
 - detailing infrastructure provision where appropriate including transportation, education, utilities, affordable housing; and
 - encouraging walkable neighbourhoods and access to open space which will have long term health benefits.


7 BACKGROUND PAPERS

- 7.1 Aberdeen Local Development Plan 2012
http://www.aberdeencity.gov.uk/Planning/ldp/pla_local_development_plan.asp
- 7.2 Aberdeen Masterplanning Process
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31782&SID=14394>

8 REPORT AUTHOR DETAILS

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Appendix 1
Masterplans

Site/Area	Type	Status
Masterplan Zone 1: Dubford and Murcar		
(OP2) Murcar	Development Framework (requires an update)	approved
(OP25) Dubford	Development Framework	approved*
Masterplan Zone 2: Grandhome		
(OP12) Grandhome	Development Framework (further phased Masterplans to follow)	ongoing
Masterplan Zone 3: Stoneywood		
(OP24) Stoneywood	Development Framework and Masterplan	approved+
Masterplan Zone 4: Newhills Expansion and Dyce Drive		
(OP26) Craibstone North	Development Framework (further phased Masterplans to follow)	ongoing
(OP28) Rowett North		
(OP29) Craibstone South		
(OP30) Rowett South		
(OP31) Greenferns Landward		
Masterplan Zone 5: Greenferns		
(OP45) Greenferns	Development Framework and Phase 1 Masterplan (further phased Masterplans to follow)	approved*
Masterplan Zone 6: Maidencraig		
(OP43) Maidencraig South East	Masterplan	pending
(OP44) Maidencraig North East		
Masterplan Zone 7: Kingswells		
(OP40) West Hatton and Home Farm (Prime 4)	Development Framework and Phase 1 Masterplan	approved*
(OP40) Phase 2 and 3	Masterplan (further phased Masterplans to follow)	approved*
(OP42) West Huxterstone		pending
Masterplan Zone 8: Countesswells		
(OP58) Countesswells	Development Framework and Phase 1 Masterplan (further phased Masterplans to follow)	pending

	follow)	
Masterplan Zone 9: Friarsfield		
(OP51) Friarsfield	Development Framework	approved+
Masterplan Zone 10: Oldfold Farm		
(OP62) Oldfold Farm	Masterplan	approved+
Masterplan Zone 11: Loirston		
(OP77) Loirston	Development Framework	pending
(OP78) Charleston	(phased Masterplans to follow)	
Other Masterplans		
Aberdeen City Centre	Development Framework	adopted
Aberdeen Harbour	Development Framework	approved*
Bon Accord Quarter	Masterplan	adopted
Cove	Masterplan	approved*
Former Davidson's Mill, Bucksburn	Development Framework	approved+
	Phase 1 Masterplan (further phased Masterplans to follow)	approved+
Friarsfield	Development Framework	approved+
Foresterhill	Development Framework	approved*
North Garthdee Farm (land at)	Planning Brief	approved+
RGU Garthdee	Development Framework	approved+
RGU City Centre Campus	Planning Brief	approved+
Future Masterplans		
AECC/Murcar		
North Dee/South Dee		

- Approved: interim planning advice awaiting adoption
* indicates subject to Committee approval to submit to Scottish Ministers
+ indicates screening results to be submitted prior to submission to Scottish Ministers
- Adopted: endorsed by the Scottish Government and adopted as supplementary guidance
- Pending: awaiting decision from the 6 November EP+I Committee

Appendix 2: Actions and expected level of detail

	Development Plan		Supplementary Guidance		Applications	
<i>stage</i>	Aberdeen City and Shire Structure Plan (to be Strategic Development Plan)	Aberdeen Local Development Plan	Development Framework	Masterplan	Planning Permission in Principle	Detailed Planning Application or Matters Specified in Conditions
<i>consultation</i>	11 weeks for finalised draft plan	8 weeks for Main Issues Report, 16 weeks for Proposed Plan.	Engagement as detailed in 5.10 in the main report above, ongoing throughout masterplan process, and formal 4 week statutory consultation.		28 days or as otherwise agreed in any processing agreement for statutory consultees 21 days neighbour notification	
<i>level of information</i>	Housing and employment land allocation	Site analysis, identification and allocation in accordance with Structure Plan	Design layout principles, landscape and visual impact assessment, use mix identified.	Detailed design layout including 3 dimensions Landscape and visual impact assessment. Ecological impact as necessary.	Supported by development framework and/or masterplan	Further detailed building design, development layout, landscape proposals etc.
<i>transport study</i>	Cumulative Impact Assessment Strategic Transport Fund (STF) arising from that	Accessibility criteria to inform allocations Action Plan – Indicative local mitigation for each masterplan area Infrastructure and Developer Contributions Manual (including STF)	High level transport assessment Reference to STF contribution Delivery response to Action Plan and Infrastructure + Developer Contributions Manual Indicative phasing and trigger points		Formal detailed statutory Transport Assessment Roads Construction Consent Section 75 Legal Agreement including Planning Obligations such as transport, affordable housing, community facilities, education, core path etc	
<i>level of information</i>	Major infrastructure requirements identified.	Major and Local infrastructure requirements identified.	Principles of junction strategy, indicative layout street layout, development block structure, key external connections.	Building layout, details of street and principles of junction strategy, transport solutions for all modes, street types, junction location.	Supported by development framework and/or masterplan. Significant level of detail to define impact mitigation and movement within and outwith the area.	
<i>environment</i>	Strategic Environmental Assessment		Environmental assessment screening and/or scoping opinion		Environmental Assessment if required	
<i>note</i>	The above is indicative only and can be supplemented with Drainage, Flood Risk Assessment, Habitats Regulation Assessment, or any other assessments as necessary depending on the site characteristics or location. For Supplementary Guidance, an agreed scope of work will be prepared between ACC and the developer. For major applications processing agreement is undertaken between ACC, the developer and statutory consultees.					

ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE

ABERDEEN, 11 September, 2012. Minute of Meeting of the ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE. Present:- Councillor Crockett, Convener; Councillor Milne, Vice-Convener; and Councillors Allan, Boulton, Cameron (as substitute for Councillor McCaig), Corall, Cormie, Delaney, Finlayson, Grant, Jaffrey, MacGregor, Jean Morrison MBE, Thomson and Yuill; Councillor Stewart (for article 7), Councillor Greig (for article 11) and Councillor Townson (for article 5).

The agenda and reports associated with this minute can be found at:-

<http://committees.aberdeencity.gov.uk/ieListDocuments.aspx?CId=140&MId=2522&Ver=4>

ANNOUNCEMENTS

1. The Head of Service for Planning and Sustainable Development provided details in relation to two Scottish awards received by the service, namely the Trinity Group for Service Quality and a commendation for the Local Development Plan for Quality in Development Planning.

The Convener congratulated Jane Forbes, Planner for being nominated by the RTPi APC (Royal Town Planning Institute – Assessment of Professional Competence) Assessors as one of the top ten candidates for 2012.

DETERMINATION OF EXEMPT ITEMS OF BUSINESS

2. Prior to considering the matters before the Committee, the Committee resolved in terms of Section 50(A)(4) of the Local Government (Scotland) Act 1973, to exclude the press and public from the meeting for article 38 so as to avoid disclosure of exempt information of the Class described in paragraphs 6, 9 and 10 of Schedule 7(A) to the Act.

MINUTE OF PREVIOUS MEETING OF 31 MAY, 2012

3. The Committee had before it the minute of its previous meeting of 31 May, 2012.

The Committee resolved:-
to approve the minute.

SPEED LIMIT REVIEW – MOTION BY FORMER COUNCILLOR FARQUHARSON – EPI/12/182

4. With reference to article 15 of the minute of its meeting of 31 January, 2012, the Committee had before it a further report by the Director of Enterprise, Planning and

ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE

11 September, 2012

Infrastructure (a) regarding the motion submitted by former Councillor Farquharson which had requested that officers explore the possibility of changing the whole structure of traffic speed controls within the City of Aberdeen; and (b) providing information on (1) the blanket city wide 20mph for residential uses implemented by Portsmouth Council; (2) traffic controls implemented by other comparable cities; and (3) the prosecution rates for motorists exceeding the existing 20mph speed limit in the city.

The report recommended:-

that the Committee –

- (a) note the content of the report and that officers have undertaken the review of speed limits in line with Scottish Government guidance;
- (b) approve the outcomes of the speed limit review as detailed in the report:-
 - (i) A93 from Bielside to Milltimber possible reduction of speed limit from 40mph to 30mph;
 - (ii) B997 from The Parkway to just north of Dubford Road, reduction in speed limit from 40mph to 30mph;
- (c) Instruct the appropriate officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required as described in the report. If no significant objections are received, then progress with the public advertisement and report the results to a future meeting of this Committee; and
- (d) Agree that given the findings of the review no further action is required in regard to the Notice of Motion by former Councillor Farquharson.

The Committee resolved:-

- (i) to approve recommendations (a), (b) and (c);
- (ii) to report back on the feasibility and costs for converting existing 20's Plenty Zone speed limits to mandatory 20mph speed limits; and
- (iii) to report back on reducing the speed limit on (1) the A944 city boundary at Westhill to Hazlehead from 50mph to 40mph, including the possible introduction of fixed speed cameras for enforcement and all costings associated with the proposal, consulting with Aberdeenshire Council in the production of this report; and (2) Skene Road from 50mph to 40mph as a result of the West Huxterstone, Kingswells development, including all associated costings.

MOTION BY COUNCILLOR TOWNSON – YOUTH EMPLOYMENT STRATEGY

5. In terms of Standing Order 21, the Committee had before it the following motion by Councillor Townson which had been referred from Council at its meeting on 13 June, 2012:-

“Council agrees to develop a youth employment strategy for Aberdeen in consultation with relevant public and private organisations”

The Committee resolved:-

that officers be instructed to submit a report to the Committee on this matter.

ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE

11 September, 2012

MOTION BY COUNCILLOR MALONE – PEDESTRIAN CROSSING AT ENTRANCE TO INTERNATIONAL SCHOOL OF ABERDEEN

6. In terms of Standing Order 21, the Committee had before it the following motion by Councillor Malone which had been referred from Council at its meeting on 13 June, 2012:-

“ To instruct the Director of Enterprise, Planning and Infrastructure to support the request by the International School of Aberdeen and install a safe pedestrian crossing similar to other crossings on North Deeside Road at the entrance of the school. At present there is no safe crossing and pupils and community members must negotiate a very busy road”

The Committee resolved:-

to defer consideration of this item until the next meeting of the Committee in terms of Standing Order 21(4).

MOTION BY COUNCILLOR JENNIFER STEWART – ON-STREET SUNDAY PARKING CHARGES

7. In terms of Standing Order 21, the Committee had before it the following motion by Councillor Jennifer Stewart which had been referred from Council at its meeting on 22 August, 2012:-

“Council instructs officers to report on the feasibility and implications of removing/ending on street Sunday parking charges”

Councillor Jennifer Stewart was in attendance and formally moved her motion and explained the rationale behind her request.

The Committee resolved:-

that officers be instructed to submit a report to the Committee on this matter.

MOTION BY COUNCILLOR YUILL – PROVISION OF MATERIALS AND EQUIPMENT TO UNDERTAKE SNOW CLEARING

8. In terms of Standing Order 21, the Committee had before it the following motion by Councillor Yuill which had been referred from Council at its meeting on 22 August, 2012:-

“That this Council agree to instruct officers to report to the appropriate Committee on the feasibility of providing local communities with the materials and equipment necessary to undertake snow clearing and gritting and that this report should seek to include information about such schemes operating elsewhere in Scotland and the UK.”

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The Committee resolved:-

that officers be instructed to submit a report to the Committee on this matter.

MOTION BY COUNCILLORS YUILL, DELANEY AND MALONE – CHANGE TO THE REGULATORY REGIME FOR BUS SERVICES

9. In terms of Standing Order 21, the Committee had before it the following joint motion by Councillors Yuill, Delaney and Malone which had been referred from Council at its meeting on 22 August, 2012:-

“That this Council agrees to write to the Scottish Government and Aberdeen’s MSP’s calling for a change in the regulatory regime for bus operators providing scheduled public services to introduce a ‘public service obligation’ which would require any bus operator providing more than 50% of the bus services in a particular local authority area to provide a basic level of bus service (including in evenings and at weekends) to communities across that local authority area.”

The Committee resolved:-

that officers be instructed to submit a report to the Committee on this matter.

MOTION BY COUNCILLOR YUILL – BUS SERVICES TO AIRYHALL

10. In terms of Standing Order 21, the Committee had before it the following motion by Councillor Yuill which had been referred from Council at its meeting on 22 August, 2012:-

“That this Council (1) expresses concern at the impact the withdrawal of evening bus services to Airyhall will have on residents of that community, including increased social isolation for some; and (2) instructs officers to report to the relevant Committee on the feasibility and likely cost of introducing a City Council supported evening bus service to Airyhall and to further report on options for funding such a supported service.”

The Committee resolved:-

that officers be instructed to submit a report to the Committee on this matter.

MOTION BY COUNCILLOR GREIG – BUS SERVICES TO SEAFIELD AND CRAIGIEBUCKLER

11. In terms of Standing Order 21, the Committee had before it the following motion by Councillor Greig which had been referred from Council at its meeting on 22 August, 2012:-

“That the Council produces a report on identifying funding sources for the bus services which are scheduled for withdrawal from Seafield and Craigiebuckler.”

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The Committee resolved:-

that officers be instructed to submit a report to the Committee on this matter.

MOTION BY COUNCILLOR DELANEY – PARK AND RIDE SERVICES FROM KINGSWELLS

12. In terms of Standing Order 12(1), the Committee had before it a Notice of Motion by Councillor Delaney which had been referred from Council at its meeting on 22 August, 2012.

Councillor Delaney, seconded by Councillor Yuill formally moved his motion in the following terms:-

That this Council (1) deplores First Aberdeen's decision to terminate the Park and Ride service from Kingswells at Aberdeen Royal Infirmary; (2) instructs officers to seek a meeting with First Aberdeen to emphasise the need for effective Park and Ride services in order to reduce congestion caused by commuter traffic and their role in achieving this; and (3) instructs the Chief Executive to seek a meeting with the Transport Minister and Group Leaders to discuss recent reductions in bus frequencies across the city and First Aberdeen's failure to deliver on local and national transportation policies in Aberdeen.

The Convener, seconded by the Vice Convener moved as an amendment:-

That this Council (1) regrets First Aberdeen's decision to terminate the Park & Ride service from Kingswells at Aberdeen Royal Infirmary; (2) instructs officers to seek a meeting with First Aberdeen to emphasise the need for effective Park & Ride services in order to reduce congestion caused by commuter traffic and their role in achieving this; and (3) instructs the Chief Executive to seek a meeting with the Transport Minister and Group Leaders to discuss recent reductions in bus frequencies across the city.

On a division, there voted:- for the motion (2) – Councillors Delaney and Yuill; for the amendment (13) – the Convener, the Vice Convener and Councillors Allan, Boulton, Cameron, Corall, Cormie, Finlayson, Grant, Jaffrey, MacGregor, Jean Morrison MBE and Thomson.

The Committee resolved:-

to adopt the amendment outlined above.

2012/13 REVENUE BUDGET MONITORING – EPI/12/184

13. The Committee had before it a joint report by the Director of Enterprise, Planning and Infrastructure and the Head of Finance, which highlighted the current year revenue

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budget performance to date along with forecast outturns for the services which related to the Committee and advised on any areas of risk and management action.

The report recommended:-

that the Committee –

- (a) note this report on the performance to date and the forecast outturn and the information on risks and management action that is contained therein;
- (b) approve the virement proposals detailed within Appendix A;
- (c) instruct that officers continue to review budget performance and report on Service strategies as required to ensure a balanced budget; and
- (d) instructs officers to report, in due course, on the actual outturn compared to budget following completion of the 2012/13 financial statements.

The Committee resolved:-

- (i) to approve the recommendations; and
- (ii) that officers report back to the Committee on the feasibility of utilising the underspend within the budget for road, pavement and street lighting repairs.

CAPITAL MONITORING – ENTERPRISE, PLANNING AND INFRASTRUCTURE PROJECTS – EPI/12/175

14. The Committee had before it a joint report by the Director of Enterprise, Planning and Infrastructure and the Head of Finance, which provided an update on the capital spend to date for the Enterprise, Planning and Infrastructure projects included within the Non-Housing Capital Programme.

The report recommended:-

that the Committee note the current position.

The Committee resolved:-

to approve the recommendation.

TWINNING AND INTERNATIONAL PARTNERSHIPS – EPI/12/153

15. With reference to article 20 of the minute of its previous meeting of 31 May, 2012, the Committee had before it for consideration a report by the Director of Enterprise, Planning and Infrastructure which sought approval of applications for financial assistance from the International Twinning Budget 2012/13.

The report recommended:-

that the Committee approve the following applications –

- (a) £2,500 towards the participation of four delegates from Aberdeen to go to Regensburg as part of their Photo Symposium 2012, 27th to 30th September 2012;

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- (b) £2,200 maximum contribution towards the visit of four delegates from Clermont-Ferrand to Aberdeen with the purpose of extending and building on business, economic, educational and sport links, 22nd-25th October 2012;
- (c) £1,925 maximum towards the visit of a group of Head Teachers and the Head of Education at Aberdeen City to Stavanger Education Department to initiate discussion on twinning projects between the two cities, 26th to 27th September 2012;
- (d) £630 grant towards the visit of the International Partnerships Officer to Gomel to represent Aberdeen City Council, accompany 21st Aberdeen Scouts and to attend meetings at Gomel City Council, 8th-15th October 2012;
- (e) £1,500 towards the participation of Aberdeen Kilt Kickers in the Stavanger annual meeting of social dancers, 28th-30th September 2012;
- (f) £5,200 towards the participation of the junior group of the HipHop School in Aberdeen, accompanied by the International Partnerships Officer, in the 'Trans'Urbaines' Festival in Clermont-Ferrand, 5th to 9th November 2012;
- (g) £400 maximum contribution towards the visit of the International Partnerships Officer to Stavanger Council, October 2012; and
- (h) £2,055 towards the visit of young people from the YMCA group in Aberdeen to Regensburg, 18th to 22nd October 2012.

The Committee resolved:-

to approve the recommendations.

CITY EVENTS PROGRAMME 2013-2014 – EPI/12/146

16. The Committee had before it for consideration a report by the Director of Enterprise, Planning and Infrastructure which sought approval of the 2013-2014 City Events programme and associated budgets which were outlined in appendices 1 and 2 of the report.

The report recommended:-

that the Committee –

- (a) note the content of this report;
- (b) approve the proposed City Events programme for 2013-2014;
- (c) approve the proposed budget to deliver the programme for 2013-2014;
- (d) refer the report to the Finance and Resources Committee to consider the request for monies from the Common Good Fund.

The Committee resolved:-

- (i) to approve the recommendations;
- (ii) that officers report back to the Committee in early 2013 providing information in relation to the Festive Communities Grant, specifically identifying the benefits of the grant, outlining previous and present uptake figures and geographical spread, and advising what the grant monies had been spent on previously; and

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- (iii) that officers review the arrangements for the Santa Parade and Christmas Lights Switch-on event, specifically with an option that they be held jointly, and report back to the Committee in early 2013 in this regard.

COMMUNITY DIGITAL MEDIA CHANNEL – EPI/12/170

17. With reference to article 17 of the minute of meeting of 18th January 2011, the Committee had before it for consideration a report by the Director of Enterprise, Planning and Infrastructure relating to the status of the Community Digital Media Channel.

The report recommended:-

that the Committee support the decision to withdraw from taking a leading role in developing a Community Media Channel at this time and instead to support the efforts of Station House Media Unit to progress future work in this development area.

The Committee resolved:-

to approve the recommendation.

ABERDEEN CITY EMPLOYABILITY FORUM – EPI/12/169

18. The Convener advised that this report had been withdrawn from consideration until the next meeting of the Committee.

RETAIL ROCKS UPDATE – EPI/12/171

19. With reference to article 31 of the minute of meeting of 18th January 2011, the Committee had before it for consideration a report by the Director of Enterprise, Planning and Infrastructure which provided a current update on the Retail Rocks Project in Torry and offered suggestions for future linked activities. The report also presented an overview of vacant retail units across the city and explored the potential of replicating Retail Rocks in other city neighbourhoods.

The report recommended:-

that the Committee –

- a) accept this update as an accurate overview of the pilot project to date;
- b) note that no grant funding currently exists to replicate the Retail Rocks Project on the same basis as the existing project;
- c) note that initial investigations have not identified any obvious areas of the City where a subsequent Retail Rocks type project could be successful; and
- d) allocate a budget of £5,000 from within the existing Employability, Skills and Community Enterprise Budget to support the further research that would be required to determine whether other funding mechanisms can be developed, and

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other suitable areas of the city can be identified, to replace or realise a similar Retail Rocks type initiative within the city.

The Convener, seconded by the Vice Convener, moved:-
that the recommendations as contained within the report be approved.

Councillor Corall, seconded by Councillor Cameron moved as an amendment:-
that recommendations (a) and (b) be approved; and that officers be instructed to investigate and report on the possibility of extending Retail Rocks into the George Street area, including the financial implications in anticipation of the submission of a bid to the 13/14 budget process; and to allocate a budget of £5,000 from within the existing Employability, Skills and Community Enterprise Budget to support this research.

On a division, there voted:- for the motion (8) – the Convener, the Vice Convener and Councillors Allan, Boulton, Finlayson, Grant, Jean Morrison and Thomson; for the amendment (7) – Councillors Cameron, Corall, Cormie, Delaney, Jaffrey, MacGregor and Yuill.

The Committee resolved:-
to adopt the motion.

MANAGEMENT OF EVENTS IN PARKS AND OPEN SPACES – EPI/12/145

20. The Committee had before it for consideration a report by the Director of Enterprise, Planning and Infrastructure relating to the management of events within Council owned parks and open spaces.

The report recommended:-
that the Committee –

- (a) note the current arrangements in place within City Events to manage Aberdeen's Parks and Open Spaces;
- (b) approve the current pricing structure in place and acknowledge the agreed working relationship between City Events and Environmental Services that ensures that Aberdeen's Parks and Open Spaces are maintained to the highest standard for ongoing event use;
- (c) agree that the Castlegate is the only recognised location within Aberdeen that can be used for political party and pressure group events; and
- (d) note the ever increasing disruption to Aberdeen's programme of events due to unauthorised Traveller encampments.

The Committee resolved:-
(i) to approve recommendations (a) and (b) of the report as outlined above;

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- (ii) to amend and approve recommendation (c) as follows “to agree that the Castlegate is the historic location within Aberdeen that can be used for political party and similar events”;
- (iii) to request the Housing and Environment Committee to review and update the Council’s Parks and Open Spaces Management Rules;
- (iv) to note the ever increasing disruption to Aberdeen’s programme of events due to unauthorised Traveller encampments and instructs officers to discuss with Grampian Police how a more robust approach could be adopted to dealing with such unauthorised encampments; and
- (v) to request that officers report back to the appropriate Committee on how future events of the type described are to be managed in Park and Ride sites.

DECLARATION OF INTEREST

The Convener and Councillor Corall declared a non-pecuniary interest in the subject matter of the following article by virtue of them both being Board Members of Visit Aberdeen. They did not feel it necessary to withdraw from the meeting during consideration thereof.

VISIT ABERDEEN BUSINESS PLAN – EPI/12/150

21. With reference to article 24 of the minute of meeting of 13th September 2011, the Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which provided information in relation to the establishment of Visit Aberdeen.

The report recommended:-

that the Committee –

- (a) note the current position with regard to the establishment of Visit Aberdeen and the production of their business plan; and
- (b) request officers to submit a paper to the next meeting of the Committee, confirming the support needed from the Council to enable Visit Aberdeen to undertake the activities, and deliver the outputs, set out in their business plan.

The Committee resolved:-

to approve the recommendations.

ABERDEEN HOGMANAY CELEBRATIONS – EPI/12/180

22. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which provided details of the current position with Aberdeen’s Hogmanay 2012 celebrations and sought approval for recommendations aimed at providing the city with a new Hogmanay event in 2013.

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The report recommended:-

that the Committee –

- (a) recognise the city's desire to include a Hogmanay event in its Festivals and Events programme in support of Aberdeen's bid for UK City of Culture;
- (b) agree that due to a lack of financial support and time available to organise an appropriate event there will be no Hogmanay celebration in 2012;
- (c) support the development of a new family event at Hogmanay in Aberdeen in 2013; and
- (d) note that officers will submit a report to Council before the end of February 2013, assessing the potential for a 2013 event and requesting approval of an appropriate budget to deliver a safe, inclusive and enjoyable family event in 2013.

The Committee resolved:-

- (i) to approve recommendations (a), (c) and (d) of the report as outlined above;
- (ii) instruct officers to identify funding streams, including external sponsorship, to support a Hogmanay event in 2012 and to report to the next meeting of the Council in this regard; and
- (iii) to thank the previous sponsor for their support of the 2011 Hogmanay Celebrations.

ABERDEEN THE SMARTER CITY: A STRATEGY FOR 2020 – EPI/12/164

23. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure relating to the Smart Cities concept and outlined the benefits of becoming a Smarter City.

The report recommended:-

that the Committee –

- (a) agree to develop a citywide Smarter Aberdeen Strategy and implementation plan which has sustainable development at its core, in line with the administrations vision Aberdeen – the Smarter City; and
- (b) instruct officers to report back to the Committee in August 2013 with the strategy and implementation plan for approval.

The Committee resolved:-

to approve the recommendations.

WEST HUXTERSTONE, KINGSWELLS MASTERPLAN – EPI/12/172

24. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure relating to the West Huxterstone, Kingswells Masterplan, prepared as a

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guide for the future development of land identified as Opportunity Site OP42 in the Aberdeen Local Development Plan.

The report recommended:-

that the Committee –

- (a) adopt the West Huxterstone, Kingswells Masterplan as interim planning advice; and
- (b) agree for officers to implement the process to ratify the Masterplan as Supplementary Guidance by the Scottish Government. This will include public consultation with results incorporated in the submission.

The Committee resolved:-

- (i) to delay consideration of the report until the next meeting of the Committee, during which time officers to consult with Kingswells Community Council on the Masterplan; and
- (ii) that officers be instructed to report back to the next meeting of the Committee on the masterplanning process and to also receive a presentation in this regard.

VARIOUS SMALL SCALE TRAFFIC MANAGEMENT AND DEVELOPMENT ASSOCIATED PROPOSALS (NEW WORKS) – EPI/12/144

25. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure relating to various small scale traffic management measures identified by officers, residents, local members, emergency services, etc and verified as necessary through surveys by officers. The report also outlined proposals associated with new developments as part of the development management process. Finally, the report included proposals for individual parking spaces for Blue Badge holders which now require to be progressed through the normal legal process for the required Traffic Regulation Order.

The report recommended:-

that the Committee –

- (a) approve the proposals in principle;
- (b) instruct the appropriate officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required as described in the report. If no significant objections were received, then progress with the public advertisement and report the results to a future meeting of this Committee;
- (c) instruct the appropriate officers to commence the combined statutory consultation for the traffic regulation order for the list of Blue Badge parking spaces and report back to a future meeting of this Committee.

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The Committee resolved:-

- (i) to approve the recommendations contained within the report with the exception of the proposals outlined for Cranford Road, in respect of which officers would consult with the local councillors and report back if necessary;
- (ii) in terms of the proposal for Foresterhill Road, to note in relation to Councillor Cormie's request that the speed limit extension be monitored, that as part of the statutory consultation process officers would receive feedback from partners including Grampian Police and NHS Grampian, and would report back to the Committee following the consultation period.

TRAFFIC ORDERS AT THE FINAL STAGE OF THE STATUTORY PROCESS – CG/12/082

26. The Committee had before it a report by the Director of Corporate Governance which advised of the outcome of the final statutory stage of the following two traffic orders and presented objections which had been received where relevant in each case:-

- (i) The Aberdeen City Council (A96/Old Meldrum Road/Mugiemoss Road, Aberdeen)(Shared Pedestrian/Cycle Path) (Amendment) Order 2012; and
- (ii) The Aberdeen City Council (Dubford/Greenbrae Area) (Various Footways)(Redetermination of Means of Exercise of Public Right of Passage) Order 2012.

The report recommended:-

that the Committee –

- (a) in relation to The Aberdeen City Council (A96/Old Meldrum Road/Mugiemoss Road, Aberdeen)(Shared Pedestrian/Cycle Path) (Amendment) Order 2012 to either:-
 - (1) approve the referral of the shared pedestrian/cycle path proposed order to the Scottish Ministers for determination; or
 - (2) accept that the objection is valid and instruct that the proposals be abandoned; and
- (b) approve The Aberdeen City Council (Dubford/Greenbrae Area) (Various Footways)(Redetermination of Means of Exercise of Public Right of Passage) Order 2012, and agree that this order be made as originally envisaged.

The Committee resolved:-

- (i) to approve recommendations (a)(1) and (b) of the report as outlined above; and
- (ii) that officers be instructed to report back in two years time on how these Traffic Orders have progressed.

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REVIEW OF THE NATIONAL SPEED LIMIT ON THE C128C ROAD BETWEEN ITS JUNCTION WITH THE A944 AT KINGSWELLS ROUNDABOUT AND THE EXISTING 40MPH RESTRICTION TO THE NORTH OF CULTS – EPI/12/161

27. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which (1) provided information relating to Councillor Boulton's earlier motion regarding the implementation of a 40mph speed limit on the currently derestricted section of the C128C road between its junction with the A944 at Kingswells Roundabout and the existing 40mph restriction to the north of Cults; and (2) outlined the effect that the installation of the Vehicle Activated Signs and the retexturing of the carriageway have made on vehicle speeds, accident numbers and severity on the C128C.

The report recommended:-

that the Committee –

- (a) note the content of this report and the results of the follow up surveys/analysis that have been carried out since the road safety measures were installed last year; and
- (b) agree that no further action should be taken with regard to the implementation of a lower speed limit.

Councillor Cameron, seconded by Councillor Corall moved:-

that the recommendations contained within the report be approved.

Councillor Boulton, seconded by Councillor Yuill moved as an amendment:-

to instruct officers to commence a statutory consultation process to reduce the speed limit on the road to 40mph; and to refer the estimated costs (£5,000) for implementation to the Finance and Resources Committee for consideration.

On a division, there voted:- for the motion (5) – Councillors Cameron, Corall, Cormie, Jaffrey and MacGregor; for the amendment (10) – the Convener, the Vice Convener and Councillors Allan, Boulton, Delaney, Finlayson, Grant, Jean Morrison, Thomson and Yuill.

The Committee resolved:-

to adopt the amendment.

ROADS WINTER SERVICES PLAN 2012/13 – EPI/12/183

28. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which provided details of the proposed Winter Services Plan and explained significant changes.

The report recommended:-

that the Committee –

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- (a) note the content of the report; and
- (b) agree the Roads Winter Service Plan for 2012-2013.

The Committee resolved:-

- (i) to approve the recommendations; and
- (ii) to thank City Wardens for their efforts and assistance to residents during previous winter periods.

EXTENSION OF PAY BY PHONE PARKING – EPI/12/185

29. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which sought approval to extend pay by phone parking so that it is available in all pay and display areas.

The report recommended:-

that the Committee –

- (a) approve extension of pay by phone parking so that it is available in all pay and display parking areas; and
- (b) to instruct officers to undertake the necessary legal procedures, in accordance with the Local Authorities Traffic Orders (Procedures) (Scotland) Regulations 1999 to make and promote the traffic regulation order required.

The Committee resolved:-

- (i) to approve the recommendations; and
- (ii) to instruct officers to report back on the feasibility of paying parking charges by credit/debit cards at parking machines.

ROAD ASSET MANAGEMENT PLAN – EPI/12/159

30. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which provided details of the first version of an Asset Management Plan for Roads Assets of the Council prepared as part of a family of Asset Management Plans covering the corporate assets of the Council.

The report recommended:-

that the Committee –

- (a) approve the Road Asset Management Plan;
- (b) agree to its continuing development as the basis for the stewardship of the Roads assets;
- (c) approve the continuing participation in a nationwide project to continue the development and implementation of such plans; and
- (d) note the financial implications of the Plan.

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The Committee resolved:-

to approve the recommendations.

FLOOD RISK MANAGEMENT (SCOTLAND) ACT 2009 – GOVERNANCE ARRANGEMENTS FOR THE NORTH EAST LOCAL PLAN DISTRICT – EPI/12/166

31. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which sought approval of governance arrangements for the North East Local Plan District (NELPD).

The report recommended:-

that the Committee –

- (a) agree the governance arrangements for the NELPD; and
- (b) appoint a member to the NELPD.

The Committee resolved:-

- (i) to approve the recommendations; and
- (ii) to appoint Councillor Alan Finlayson as the member to the NELPD.

PALMERSTON – REVISED TRAFFIC MANAGEMENT AND CONTROLLED PARKING ZONE PROPOSALS – EPI/12/178

32. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which outlined the results of the parking surveys and informal consultation by letter drop carried out on the streets within and surrounding the proposed Controlled Parking Zone within the North Dee area and of the initial design work which was undertaken.

The report recommended:-

that the Committee –

- (a) note the results of the informal consultation and parking survey exercises;
- (b) instruct officers to progress the preliminary design of the traffic management proposals and Controlled Parking Zone;
- (c) instruct officers to establish cost estimates for the traffic management proposals and Controlled Parking Zone and develop a business case for the Controlled Parking Zone; and
- (d) instruct the appropriate officers to report back on the findings of the above to a future Committee meeting.

The Committee resolved:-

to approve the recommendations.

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PERMIT MISUSE POLICY – EPI/12/151

33. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which advised on the development of a formal policy for dealing with misuse of all parking permits within the city, such as residents permits, business permits, contractors permits and Blue Badges.

The report recommended:-

that the Committee -

- (a) note that monitoring levels of misuse of residents permits and Blue Badge has commenced and will actively continue to the end of September to gather statistical data on the potential levels of abuse; and
- (b) instructs officers to report back to the Committee in November on the levels of misuse and with draft policies, procedures, actions and any financial implications arising from resources to address the misuse of parking permits and Blue Badges.

The Committee resolved:-

to approve the recommendations.

OFF-STREET MONTHLY PARKING PERMITS – EPI/12/152

34. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which provided information in relation to the levels of monthly parking permits being purchased within the off-street car parks.

The report recommended:-

that the Committee instruct officers to introduce a limit on the number of spaces available to permit holders (average 48% of full car park capacity) within the larger off-street car parks, to introduce off-street parking permits designated to a specific off-street car park and to monitor the operational performance of the car parks.

The Committee resolved:-

to approve the recommendation.

STRATEGIC AND LOCAL TRANSPORTATION PROJECTS – UPDATE REPORT – EPI/12/167

35. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure which outlined the progress to date of various strategic and local transportation projects within Aberdeen City and the wider area which flow from the development of the Regional Transport Strategy produced by Nestrans, and the Council's own Local Transport Strategy.

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The report recommended:-

that the Committee –

- (a) note the content of this report;
- (b) endorse development through Nestrans as outlined in section 5 of the report, including Board decisions and progress on the 2012/13 programme of works; and
- (c) agree a response to Nestrans on the draft Regional Transport Strategy Main Issues Report.

The Committee resolved:-

to approve the recommendations.

COMMITTEE BUSINESS STATEMENT

36. The Committee had before it a statement of pending and outstanding Committee Business, which had been prepared by the Head of Legal and Democratic Services.

The Committee resolved:-

- (i) to delete item 5 (Community Digital Media Channel), item 7 (VisitScotland Funding 2011/12 and Proposed Aberdeen Destination Marketing Organisation), item 9 (North Dee – Development Update) and item 12 (Speed Reduction at Countesswells) from the statement; and
- (ii) to otherwise note the updates contained within the list.

MOTIONS LIST

37. The Committee had before it a statement of outstanding motions under the Committee's remit which had been prepared by the Head of Legal and Democratic Services.

The Committee resolved:-

to note the updates contained within the list.

In accordance with the decision recorded under article 2 of this minute, the following item of business was considered with the press and public excluded.

BUS SHELTER ADVERTISING CONTRACT

38. The Committee had before it a report by the Director of Enterprise, Planning and Infrastructure relating to the status of the Bus Shelter Advertising Contract for Aberdeen City.

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The report recommended:-

that the Committee –

- (a) instruct officers to enter negotiations with Clear Channel with the aim of agreeing a reduced term for the Bus Shelter Advertising Contract;
- (b) instruct officers to negotiate appropriate terms for a formalised extension to the minute of variation in respect of the Variation in order for the Contract and Variation to expire at the same time;
- (c) invoke Standing Order 1(6) (a) of the Standing Orders relating to Contracts and Procurements in order to exempt this contract negotiation from Part A of the Standing Orders; and
- (d) instruct officers to report back on the outcome of the above negotiations and with a plan for retendering the Contract and Variation.

The Committee resolved:-

- (i) to approve the recommendations; and
- (ii) to instruct officers to undertake a systematic review of all commercial contracts relating to Enterprise, Planning & Infrastructure to ascertain and record termination arrangements and take appropriate action to safeguard the Council's position.

- **COUNCILLOR CROCKETT, Convener.**

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NORTH EAST SCOTLAND FISHERIES DEVELOPMENT PARTNERSHIP

WEDNESDAY, 21 NOVEMBER, 2012 AT 2.15 P.M.

Advance notice is given that a meeting of the NORTH EAST SCOTLAND FISHERIES DEVELOPMENT PARTNERSHIP will be held in COMMITTEE ROOM NO. 5, WOODHILL HOUSE, ABERDEEN, on WEDNESDAY, 21 NOVEMBER, 2012 at 2.15 P.M.

Christie Gore

16 October, 2012

Director of Corporate Services
Aberdeenshire Council
on behalf of North East Scotland
Fisheries Development Partnership

To: Aberdeenshire Council: Councillors A M Allan, P Argyle, M Kitts-Hayes, P Johnston and S Smith.

Aberdeen City Council: To Be Confirmed.

Moray Council: Councillors G McDonald and D Stewart.

Aberdeen Fish Producers Organisation

Aberdeen Harbour Board:

Peterhead Port Authority:

Peterhead and Fraserburgh Fish Processors Assoc

Scottish Enterprise:

HIE Moray:

British Boatbuilders Association:

Banff & Buchan College of Further Education:

Marine Scotland Science

Scottish Pelagic Producers Association

Lunar Fish Producers Organisation

Moray Firth Inshore Fisheries Group

North Sea Commission / NSRAC:

North East of Scotland Fishermen's Organisation:

North East Scotland Fishermen's Training Association

Scottish Fishermen's Federation:

Scottish Pelagic Fishermen's Association:

Scottish White Fish Producers' Association Ltd:

Seafish Industry Authority:

Seafood Scotland:

D Anderson

C Parker

J Wallace

W Clark

A Beddie

M Holmes

W Farquhar

D Cook

N Bailey

I McFadden

G Ingram

N Lake

A D Hawkins

R Stevenson

J Watt

B Armstrong

I Gatt and A Wiseman

M Park

J Harman

I Land and L Woodhatch

MSPs: Brian Adam, Richard Baker, Nigel Don, Alex Johnstone, Richard Lochhead, Lewis Macdonald, Mark McDonald, Alison McInnes, Jenny Marra, Nanette Milne, Dennis Robertson, Shona Robison, Alex Salmond, Kevin Stewart, Stewart Stevenson and Maureen Watt.

MEPs: Ian Hudghton, David Martin, Struan Stevenson, Catherine Stihler, Alyn Smith and George Lyon.

MPs: Malcolm Bruce, Sir Bob Smith and Eilidh Whiteford.

Contact Person:-

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ABERDEEN CITY COUNCIL

COMMITTEE :	Environment, Planning and Infrastructure
DATE:	6th November 2012
DIRECTOR:	Gordon McIntosh
TITLE OF REPORT:	Notice of Motion from Councillor Malone – “To instruct the Director of Enterprise, Planning and Infrastructure to support the request by the International School of Aberdeen and install a safe pedestrian crossing similar to other crossings on North Deeside Road at the entrance of the School. At present there is no safe crossing and pupils and community members must negotiate a very busy road.”
REPORT NUMBER:	EPI/12/162

1. PURPOSE OF REPORT

This is a background report which responds to Councillor Malone’s Notice of Motion “to instruct the Director of Enterprise, Planning and Infrastructure to support the request by the International School of Aberdeen and install a safe pedestrian crossing similar to other crossings on North Deeside Road at the entrance of the School. At present there is no safe crossing and pupils and community members must negotiate a very busy road”.

Following the deferral of this item by the committee on the 11th September further discussions have been held with the school and an update is provided.

2. RECOMMENDATION(S)

There are no recommendations at this time.

3. FINANCIAL IMPLICATIONS

There are no financial implications arising from this report. The International School of Aberdeen has informally intimated that they are willing to finance any proposal accepted by Aberdeen City Council.

4. SERVICE & COMMUNITY IMPACT

- 4.1 The content of the report meets with the local Community Plan objectives to continually improve road safety and maximize accessibility for pedestrians and all modes of transport.
- 4.2 The proposals are in line with the Council's Transportation Strategy to improve safety for all road users by continuing to reduce the number of casualties in traffic collisions.

5. OTHER IMPLICATIONS

None arising from this report

6. REPORT

Background

- 6.1 At its meeting on 13th June 2012 full Council considered a motion raised by Councillor Malone "to instruct the Director of Enterprise, Planning and Infrastructure to support the request by the International School of Aberdeen (ISA) and install a safe pedestrian crossing similar to other crossings on North Deeside Road at the entrance of the School. At present there is no safe crossing and pupils and community members must negotiate a very busy road"
- 6.2 Council officers have been in discussions with the International School of Aberdeen for the previous 18 months to bring forward a scheme to enhance pedestrian safety at this location, and these discussions are still in progress.
- 6.3 This issue originates from the Planning Application for the relocation of the school.
- 6.4 A Transportation Statement was submitted in support of the application with regard to the impact the development was likely to have on the local roads network.
- 6.5 Within this statement it was indicated that, at that time, the vast majority of the pupils were taken to the school by buses provided by the school that the majority of the remainder travelled by car. In short only 1% of pupils cycled/walked to school and when school staff were included the figure rose to 2% of daily visits.
- 6.6 It was indicated that this bus service would continue after the move and that the travel split would remain.

- 6.7** The proposal in the Planning Application was approved on the information submitted and did not include a requirement for a pedestrian facility. Had a pedestrian facility been highlighted by the schools consultants as a requirement at the planning stage then the roads mitigation necessary to support the application would have reflected this along with the conditions associated with a crossing facility.
- 6.8** Correspondence from the School's engineering/planning consultant restarted in August 2010 shortly after the new school opened, requesting that ACC consider the introduction of a pedestrian facility.
- 6.9** Initial surveys were carried out to determine the numbers of people walking and cycling to the School, and also to record the level of community usage. These surveys indicated a level well below that which would be necessary to support the introduction of a crossing facility.
- 6.10** When considering the survey results a meeting was held with the School Management, Head Teacher, their Planning/Engineering Consultant and the Council's Traffic Management Team to discuss the various options that had been suggested by the school and to rule out those that were not possible. The discussions also considered the means by which to take forward those options that had potential.
- 6.11** Various measures were discussed at length and these included, reducing the speed limit on North Deeside Road, Pelican/Puffin crossings, Zebra Crossing, Pedestrian Refuge Islands and School Crossing Patrollers.
- 6.12** Of these options only the Pedestrian Refuge Islands and installing a School Crossing Patroller were considered viable by the Traffic Management Team. It was suggested that the ISA take forward these two options for further investigation. The school does not wish to pursue the option of a School Crossing Patroller.
- 6.13** Proposals for retro-fitting a pedestrian refuge island at either side of the School entrance were submitted. However, on both occasions the design did not meet the recommended standards and guidance on best practice documents contained within the "Design Manual for Roads Bridges" and the recommendations in "The Design of Pedestrian Crossings" and "Cycling by Design".

Update

- 6.14** A meeting 26th September was attended by Councillor Malone, representatives of the International School Aberdeen and officers from the City Council to review crossing options.

- 6.15** The outcome of this meeting was that a facility could be installed subject to a departure from standards that officers felt could be accommodated in the constrained circumstances.
- 6.16** The standards contained within the design guidance documents indicate that separate independent running lanes be provided for both motorised vehicles and cyclists with a central island capable of accommodating cyclists and pedestrians. Whilst provision to the absolute standards is achievable within a green field environment it is necessary to consider the constraints of the existing environment and in this particular case the urban environment
- 6.17** The design of a scheme to meet the recommended design standards would include the widening of the North Deeside Road by approximately 1.2 m requiring existing walls and mature trees to be taken down and replaced. The cost of the widening the road and impact on the built environment would very high when compared to the level of provision and would not be in scale. It is therefore considered that provision a pedestrian island at this location to meet the recommended design standards would not be a cost effective or affordable option.
- 6.18** A departure from the recommended standards that will provide a pedestrian crossing facility has been considered and can be accommodated within the existing road boundary without any significant additional risk to other road users and whilst maintaining visibility and sight stopping distances.
- 6.19** A layout for a pedestrian island that meets the minimum design standards for a pedestrian crossing and which is similar to existing crossing facilities on the North Deeside Road is possible within the current road boundaries. However it will be noted that in order to accommodate the pedestrian crossing island it will not be possible to provide a dedicated cycle lane through the location of the island but will revert to a standard shared carriageway over this short section.
- 6.20** We currently await an updated design from the school's consultants, taking account of the agreed criteria and specifications, but are confident that an acceptable design will be forthcoming.

7. REPORT AUTHOR DETAILS

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8. BACKGROUND PAPERS

N/A

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	6 th November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Development of a Youth Employment Strategy
REPORT NUMBER:	EPI/12/238

1. PURPOSE OF REPORT

This report has been produced in response to a motion raised by Councillor Townson on the 13th June 2012 to develop a Youth Employment Strategy.

The report outlines the activities already taking place in relation to the production of a youth employment strategy and action plan based on the Scottish Government Opportunities for All Programme. It aims to reassure elected members that this work is already being progressed as a priority for the community planning partners and the council. It clarifies who is currently leading this work in the city and seeks approval for this work to be progressed through the Integrated Children's services Successful Transitions Outcomes Group.

This report will also highlight the number of individuals within the 16 – 19 segment that are not currently in Education, Employment or Training.

3. FINANCIAL IMPLICATIONS

The work to date in mapping the current activities available for those in the target segment which are not currently engaged, is being undertaken by an officer in Education, Culture and Sport and does not require additional financial input.

All identified services to date are funded by external funders and should require no additional financial input from Aberdeen City Council.

4. OTHER IMPLICATIONS

There may be an additional resource requirement to ensure that partners maintain updated records on the system. This would be sought from within Education, Culture and Sport and Skills Development Scotland.

5. BACKGROUND/MAIN ISSUES

Rising unemployment within the 16 – 19 year old age group is of key concern nationally and a number of initiatives have been launched by both the Scottish and UK governments to address this issue.

The unemployment rates for the 16 – 19 year olds within the city whilst not giving reason to be complacent are significantly lower than other areas of the country.

Skills Development Scotland's data for September 2012 highlight that there are currently **270** Job seeker allowance claimants within the 16 – 19 year category. This represents **0.17%** of the working age population of Aberdeen, which has an average unemployment rate of **2.2%**

Of those 270, there are **155** that have been claiming for 13 weeks or less.

In comparison the city of Dundee with an average unemployment rate of **6.5 %** and a population of 144,300 has **645** Job seeker allowance claimants within the 16 – 19 year category, representing **0.68%** of the working age population, of which **320** have been claiming for 13 weeks or less.

On a UK wide basis with a slightly different measuring mechanism there are **1.02 million** individuals within the 16 – 24 year old segment claiming Job seeker allowance.

Current situation - Internal

Officers within Education, Culture and Sport currently co-ordinate and take a lead role in supporting the multi agency Successful Transitions to Adulthood Outcome Group. The key objectives of this multi agency group are to ensure appropriate provision is available to minimise the number of school leavers not in employment education or training, and to ensure all partners are fully informed on all local and national initiatives to ensure individuals within this target segment can access all appropriate support. This group leads the implementation of the national Opportunities for All and the Youth Employment Strategy in the city.

The lead officer for this group has also mapped out all the appropriate local provision, updates regularly and shares with partners, ensuring that all partners that engage with this client group have an overview of current provision and can refer appropriately.

Additionally officers within Education, Culture and Sport manage the delivery of the Activity Agreements, which provide a package of tailored activity and learning to re-engage those young people furthest from the labour market

External

There are a number of external initiatives that are managed by Skills Development Scotland on behalf of the Scottish Government.

Opportunities for All

The Scottish Government has made a commitment to offer every 16 – 19 year old a place in education or training through the “Opportunities for All” initiative.

Training for Work

Training for Work provides vocational training support to people aged 18 or over who have been continuously unemployed for at least 13 weeks and are actively looking for work.

Get Ready for Work

Get ready for Work arranges skills training and work placements for people aged 16 – 19 who are finding it difficult to access training, learning and employment.

Additionally Skills Development Scotland maintain a database of all School leavers, which highlights those that are not currently in employment, education or training. This information is used to ensure all individuals are contacted and made aware of the services available through Skills Development Scotland and its delivery partners to reduce the time that School leavers are disengaged. They also provide support in the form of “My world of Work” coaches who will give one to one support to those furthest from the labour market.

My world of work

My world of work is an online toolkit that offers support and information in relation to job seeking, local employment opportunities, sector specific information and skill development tools within the employability arena.

Going Forward

The Scottish Government launched Scotland’s Youth Employment Strategy in 2012 and also appointed Angela Constance MSP, as Minister for Youth Employment. This strategy has been widely adopted and sets out delivery expectations for Local authorities to address the issues of Youth Unemployment on a local level, and has made commitment to additional funding to deliver new and increase the delivery on existing initiatives e.g. Modern Apprenticeships.

The Scottish Government has requested Skills Development Scotland through local community planning to develop a “Local Youth Employment Plan”

“Youth Employment Plans should bring together key Community Planning partners with a contribution to make to this agenda, and encourage service alignment locally.

Skills Development Scotland takes a lead role in developing Youth Employment Plans with each local authority area as the basis for agreeing the nature and size of the priority cohort locally and to develop a shared pipeline approach to manage the process. The development of records of the type of each offer made will also be considered”

The initial meeting in early October with Skills Development Scotland highlighted that officers within Education, Culture and Sport through the Successful Transitions to Adulthood Outcome Group, had already made significant progress through their mapping activity to meeting the needs of the minister, and were commended on their work. Further work will be initiated over the coming weeks to ensure a process is in place to allow this information to be regularly updated, and currency maintained, this will highlight which initiatives have been most

successful and any areas of duplication or over provision. A fully populated map of activity will assist support workers, guidance staff and advisors to direct clients appropriately to a programme that is most suited to their needs and therefore most likely to be successful.

In addition to this Aberdeen City Council and its Community Planning partners are committed to delivering activities and developing appropriate initiatives as per the Scottish Government, Youth Employment Strategy through the Successful Transitions Outcome group.

6. IMPACT

Single Outcome agreement (SOA) National Outcomes:

1. We realise our full economic potential with more and better employment opportunities for our people
2. We are better educated, more skilled and more successful
3. People of all ages take an active part in their own learning to achieve full potential. Learning and training is appropriate and accessible to learner's needs

Aberdeen - the Smarter City

We will ensure all citizens are encouraged and supported appropriately to make their full contribution.

We will help every citizen to improve their knowledge and skills, together contributing to our success as a learning city

We will enhance the physical and emotional wellbeing of all our citizens by offering support and activities which promote independence, resilience, confidence and self-esteem.

We will work with our partners to seek to reduce the levels of inequality in the city.

We will provide a high quality education service within our schools and communities which will improve attainment and life chances of our children and young people to achieve their full potential in education, employment or training.

Working with our third, public and private sector partners, we will provide opportunities for lifelong learning which will develop knowledge, skills and attributes of our citizens to enable them to meet the changing demands of the 21st century.

Again, working with partners, we will create a City of Learning which will empower individuals to fulfill their potential and contribute to the economic, social and cultural wellbeing of our communities.

7. BACKGROUND PAPERS

Scottish Government's Youth Employment Strategy

Skills Development Scotland's Youth employment Action Plan guidance

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	The Glens and Deeside Way
REPORT NUMBER:	EPI/12/232

1. PURPOSE OF REPORT

This report is in response to Councillor Ian Yuill's Motion to this Committee on 13th September 2011 about the creation of a new long distance path route between Aberdeen and Perthshire. (At that time it was suggested that it could be called the Pictish Way).

2. RECOMMENDATION(S)

1. Note the content of this report.
2. Support the principle of the long distance route but take no further action until all authorities along the route are engaged in the project.

3. FINANCIAL IMPLICATIONS

None.

4. OTHER IMPLICATIONS

There are no other implications.

5. BACKGROUND/MAIN ISSUES

- 5.1 At its meeting of 13th September 2011 this Committee approved Councillor Ian Yuill's Motion *"amongst other things, to request officers to participate in the Royal Deeside, Angus and Perthshire Way Steering Group which was already discussing the establishment of the "Pictish Way", and that officers report back to the next meeting of the Enterprise, Planning and Infrastructure Committee regarding the terms of the motion, and in particular detailing the outstanding aspects*

required to establish and signpost the proposed "Pictish Way", as well as the financial cost of achieving this".

- 5.2 An interim report was presented to the Committee at its 31 January meeting. This provided an update explaining that Angus Council intended to commission a feasibility study that would consider if the proposed route in its entirety was achievable, whether there is demand for it and providing details of the works required to bring it up to a fit for purpose standard.
- 5.3 Following completion of the feasibility study a meeting of the Steering Group, renamed the Pictish Way Steering Group was held in Forfar on the 25th of August. This was attended by the Senior Planner from the Environmental Policy Team and Councillor Yuill.
- 5.4 The principal issues that arose from consideration of the feasibility study were the resource implications for the local authorities taking on the route development and the continuing maintenance of the gap sections between established paths or tracks. The whole route would need to be in place and be brought up to a fit for purpose standard before it could be promoted and land owner negotiations would be needed prior to this. Within Aberdeen City there is a relatively short length of the route and this is already in place as the Deeside Way. Other local authorities' areas have longer lengths of the route and gaps that would be more costly to resolve. The resource implications for these authorities would be much greater. At the last two Steering Group meetings there have been no representatives from Aberdeenshire Council or the Cairngorms National Park Authority. Large sections of the route pass through their areas and therefore their support for the project would be crucial.
- 5.5 There would then be significant financial and legal (liability and landowner negotiation) issues with the route being developed jointly by local authorities and the Cairngorm National Park Authority. However, there are examples of long distance routes (e.g. the Rob Roy Way) having been developed 'virtually' and promoted as a concept online by private individuals. Interest and use (under access rights provided by the Land Reform (Scotland) Act) grows gradually, and more official route development can follow as and when possible. The potential economic benefits of the route are recognized and there is broad support from the Steering Group but the shorter term resource and legal implications suggest that a more practical first step would be the development of the 'virtual' route. A member of the Steering Group has undertaken to develop a website describing and illustrating the route.
- 5.6 There was also discussion on the name of the route. There already is a Pictish Trail in the Highlands and there was agreement that Glens and Deeside Way would be a more appropriate title. The precise start and finish of the route is not specified but it is quite clear that there is the opportunity to link Aberdeen (via the Deeside Way) with Pitlochry.

6. IMPACT

The Single Outcome Agreement's Wealthier and Fairer outcome is most directly relevant to this report, as the long distance path could attract leisure tourists to the city, although none of the actions listed in the SOA relate to such a project. This also relates to the Vibrant, Dynamic and Forward Looking objective of Aberdeen being an even more attractive place in which to do business. This could support the EP&I service priority within the 5 Year Business Plan of encouraging future economic and business development in the city.

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Delivering Transformational Change in Asset Management & Operations Services
REPORT NUMBER:	EPI/12/243

1. PURPOSE OF REPORT

This report provided the Committee with indicative details of the approach being taken within the Asset Management and Operations Services within Enterprise, Planning and Infrastructure to enable the services to deliver the level of efficiencies identified within the Council's Business Plan.

2. RECOMMENDATION(S)

It is recommended that the Committee:

- a) note the findings from the soft market testing and soundings undertaken by the services to date
- b) agree that the capability exists within the in-house workforce to achieve similar levels of efficiencies through transformational changes and appropriate investment in services
- c) note the range of change options identified within the report
- d) instruct officers to report back in six months to update the committee on progress

3. FINANCIAL IMPLICATIONS

The 2011/12 budget decisions identified a number of options for delivery including a saving of £4.481M annually from 2013/14 onwards which is for 'Partnering of AM&O Services'. The 2012/13 General Fund revenue budget makes provision of £0.5M to enable the delivery of these savings in future years.

4. OTHER IMPLICATIONS

A number of the options require changes to working practices, changes to job content or indeed changes to working hours in order to deliver the level of saving/efficiency identified. In each case there will be

engagement with staff groups involved and ongoing dialogue with trade unions in order to ensure that the reasoning behind change is clear and the benefits are delivered. Services with a key role to play in the change process will be participating in the governance of the transformation of the services e.g. Education in relation to soft Facilities Management

5. BACKGROUND/MAIN ISSUES

Asset Management and Operations (AM&O) provide a strategic and operational function for the management and maintenance of the Council's physical assets i.e. properties, road infrastructure, vehicles and plant.

The various teams are led by a wide range of professional and specialist service managers supporting frontline operations primarily in property, facilities management, roads and vehicle maintenance.

The 2011/12 budget decisions identified a number of options for delivery including a saving of £4.481M annually from 2013/14 onwards which is for 'Partnering of AM&O Services. The 2012/13 revenue budget makes provision of £0.5M to enable the delivery of these savings in future years.

Soft Market Testing has identified that potential service providers would generate an average savings across each function of around 20% (for more detail see Supplementary Report). As this is a gross saving comparisons and takes no consideration of costs the Council would have to encounter to manage any external provider net saving levels have been estimated. The levels of efficiencies are reflective of the approach and level of saving the in-house service would make to transform the service.

The market soundings identified specific actions that the private sector could generate savings from. These are Staff Transfer/Turnover, Maximising Attendance, Use of Supply Chain, Access to Wider Resources/Contracts, Innovation and Optimising External Income Sources. The comparison between internal and external service providers points to significant similarities of approach to cost reduction and service quality. It also has to be recognised that some work is already outsourced for workload/benchmarking purposes and in the circumstances managers are familiar with the local market, costs and capabilities of external service providers.

One of the key factors affecting the in-house service cost is the hourly rate of pay of many staff groups. Decisions made as part of the annual budget process to increase the lowest levels of pay for staff have increased service delivery costs against a backdrop of lower costs and minimum wage conditions in the private sector.

Since Financial Year 2010/11 a significant number of savings have been generated across the range of services within AM&O. A robust Capital Programme fee income from project design has now been established. In addition the ongoing rationalisation of corporate accommodation is assisting in bring down revenue spending in this area. Through continued innovation we are reducing our energy costs that provides the additional benefits of paying lower carbon reduction commitment costs.

The table below summarises the split of savings contributing to the overall target within option EPI_AMO01.

Service Heading	Saving/Efficiency	Estimated Saving £000	Total Savings by Service
Facilities Management			1128
Catering	Income generation / new services	310	
	Working Patterns/productivity	70	
Cleaning	Working Patterns	100	
	Management Structure	150	
Janitorial	Income from rents	164	
	Redesign Out of Hours Service / Roles	125	
School Crossing Patrols	Working Patterns	50	
Hard FM	Absorbed Cost Pressures	159	
Fleet	Redesign Service Delivery	166	600
	Vehicle Replacement Costs	434	
Parking	Consistency of Charging	200	200
Asset Management	Fee Income	1000	1980
	Corporate Estate Review	980	
Roads	Absorbed Cost Pressures	200	500
	New Income sources	125	
	Working Patterns	100	
	Process Re-engineering	75	
Total			4408

These figures have been confirmed by Finance colleagues as fitting to adjusted levels of spending in 2013/14.

Undoubtedly action needs to be taken to improve the efficiency of some services, especially those where soft market testing suggests opportunities exist.

Every opportunity is being identified to optimise income and where processes involve other services to involve them in any review.

At present a number of projects are under way to deliver efficiencies. Two specific tasks are being undertaken that will establish the most

cost effective and practical way forward for the future use of our property assets, these being the reviews of corporate office accommodation and depots. Services have set out in their asset management plans future needs in these areas and given the cross cutting nature of the services involved it is important to undertake a thorough option appraisal. Unfortunately such reviews and their outcomes will require further planning and resourcing and so a realistic timescale for delivery will be approaching 2014/15 before any meaningful savings can be delivered.

6. IMPACT

Aberdeen – The Smarter City identifies as a high priority that the Council will provide and promote a sustainable transport system, including cycling, which reduces our carbon emissions.

The Single Outcome Agreement identifies as a National Outcome 15, “Our public services are high quality, continually improving, efficient and responsive to local people’s needs.” One of the actions identified is that partners will “Modernise service delivery and ensure there is appropriate investment in staff training, equipment and accommodation to deliver this”.

7. BACKGROUND PAPERS

Internal reports produced summarising the Soft Market Testing Returns for services (Fleet, Roads and Streetscene, Catering).

Report by Ernst and Young re Option Appraisal for Management of Investment Properties.

The information contained in these reports is confidential in nature.

9. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Capital Monitoring – Enterprise, Planning & Infrastructure Projects
REPORT NUMBER:	EPI/12/211

1. PURPOSE OF REPORT

To advise the Committee of the capital spend to date for the Enterprise, Planning & Infrastructure projects included within the Non-Housing Capital Programme.

2. RECOMMENDATION(S)

The Committee note the current position.

3. FINANCIAL IMPLICATIONS

The monies required to fund these projects are achieved through external borrowing, capital receipts and grant income. These projects are all accommodated within the Non-Housing Capital Programme. Any underspend, carry forward or overspend will have implications for the programme. There are no issues at present that would result in such implications.

4. OTHER IMPLICATIONS

There are no other implications at this time but as projects progress or indeed fail to progress then other implications may arise and will be reported at an appropriate Committee.

5. BACKGROUND / MAIN ISSUES

As reported at the Finance & Resources Committee in June 2012 the overall responsibility for the monitoring / management of the Capital Programme lies with the Head of Asset Management & Operations. The Planning & Monitoring Officer within Asset Management & Operations is in regular contact with the Service Representative and the Capital Accountant, reporting in the first instance to the Corporate Asset Group. This ensures that the spend figures are always up to date and accurate.

Enterprise, Planning & Infrastructure has a total of 13 projects, totaling £22.688 million allocated to it from the Non-Housing Capital Programme. The projects and total budget committed to each project included in the programme are:-

- 1) Corp Property Condition & Suitability Programme
£7.71 million
- 2) Cycling Walking Safer Streets Grant
£252,000
- 3) Access From the North
£430,000
- 4) Western Peripheral Route
£4.1 million
- 5) Corporate Office Accommodation
£1 million
- 6) NESTRANS - Capital Grant
£960,000
- 7) 3R's Furniture, Fittings & Equipment and Other Works
£50,000
- 8) Planned Renewal & Replacement of Road Infrastructure
£3.748 million
- 9) Land Acquisition – Contingency
£650,000
- 10) Frederick Street Relocations - Crombie Road
£43,000
- 11) Frederick Street Multi Storey Car Park
£2.345 million
- 12) Hydrogen Buses
£400,000
- 13) City Broadband
£1 million

Spend for all projects to the end of September is £5.1 million, 22% of the total budget. Discussions so far have identified potential 2012/13 underspend in the Hydrogen Buses and City Broadband projects. Spend profiles provided by the project's budget holder show it is anticipated that any underspend this financial year will be picked up in

the following financial year(s). The three year capital business plan ensures that a project will not underspend within the three year period.

Appendix A provides a breakdown of expenditure to date and relevant supporting information as necessary.

An update on the capital position will be reported to this Committee on 22 January 2013.

6. IMPACT


Corporate - The capital programme encompasses projects which link to the Community Plan, Single Outcome Agreement, Corporate and Individual Service Plans and Vibrant, Dynamic & Forward Looking.

Public - This report will be of interest to the public as it outlines the Council's capital spending to date on Enterprise, Planning & Infrastructure projects.

7. BACKGROUND PAPERS

Non-Housing Capital Programme 2012/13 – Capital Monitoring Report approved at Finance & Resources Committee on 19 June 2012

8. REPORT AUTHOR DETAILS

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Appendix A: Spend to date

Project Description	Budget Holder	Total Budget 12/13	Budget 13/14	Budget 14/15	Spend to end Sep.
		£'000	£'000	£'000	£'000
Corp Property Condition & Suitability Programme	Neil Esslemont	7,710	7,400	7,400	1,652
Cycling Walking Safer Streets Grant	Doug Ritchie	252	232	341	38
Access From the North	Tom Rogers	430	7,010	6,400	63
Western Peripheral Route	John Wilson	4,100	1,000	600	0
Corporate Office Accommodation	Hugh Murdoch	1,000	0	0	53
NESTRANS - Capital Grant	Joanna Murray	960	882	1,295	480
3R's Furniture, Fittings & Equipment and Other Works	Graeme Craig	50	0	0	15
Planned Renewal & Replacement of Road Infrastructure	Mike Cheyne	3,748	3,768	3,659	1,640
Land Acquisition - Contingency	Stephen Booth	650	0	0	1
Frederick Street Relocations - Crombie Road	Jim Forbes	43	0	0	254
Frederick Street Multi Storey Car Park	Ian Taylor	2,345	0	0	881
Hydrogen Buses	Jan Falconer	400	400	400	0
City Broadband	Jan Falconer	1,000	1,000	0	25
Totals		22,688	21,692	20,095	5,100

- Over half of the budget for Planned Renewal & Replacement of Road Infrastructure has been spent, Works are ongoing and the full budget is profiled to be spent this financial year.
- The Condition & Suitability budget continues to be spent. Further commitments totalling £3 million are currently being undertaken. The full project budget is currently profiled to be spent this financial year.
- Given the recent court decision to reject a further appeal against the Western Peripheral Route, the budget holder of this project will proceed with a detailed spend profile as soon as possible. A project plan and expected out turn for 2012/13 and the next two financial years will be reported to the next meeting of this committee if available.
- The Corporate Office Accommodation budget has been split into 5 sub headings:
 - Marischal College Improvements
 - Town House: Replacement Sound System
 - Town House: Refurbishment & Upgraded Security
 - Frederick Street: Ground Floor & External Envelope Repairs
 - Upgrading Offices

Due to the complexity and nature of the projects defined it is confirmed that most of this budget will slip to 2013 / 14. However, it is intended to

have all contracts legally committed this financial year. To date, £53,000 has been spent on office moves and improvements to Marischal College.

- The Frederick Street Relocations – Crombie Road project involved the relocation of the Education, Culture & Sport Reserve Collection to vacant Council owned industrial units at Crombie Road. Much of the required refurbishment to this unit was from the dilapidations budget, paid for by the outgoing tenant. This “transfer” is still to be processed and will help bring the total expenditure back into line.
- The City Broadband capital project budget of £2 million has been reprofiled. It is believed that the only spend for this project will be to purchase expert advice in order to undertake an accelerated procurement process, at a cost of £42,000 with the remaining £1.9 million profiled for 2013/14.
- Similarly, the Hydrogen Buses project has been reprofiled and now anticipates the full spend will be made in 2013/2014: £800,000 for the 4 buses in December 2013 and £1.2 million for the remaining buses in April 2014.
- Issues relating to Frederick Street Multi Storey Car Park reported to the previous EP&I Committee have now been resolved and expenditure is continuing according to the project plan. The car park facility is on target to open on 19 November 2012.
- £70,000 has been spent from the Land Acquisition – Contingency budget but has yet to show in the ledger. This will show in October’s ledger. This acquisition was 46.28 acres of land at Mill of Dyce, Dyce.
- The NESTRANS Capital grant spend is following the original spend profile. £480,000 in September will be followed by the same amount again in February.

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	2012/13 Revenue Budget Monitoring
REPORT NUMBER:	EPI/12/201

1. PURPOSE OF REPORT

- 1.1 The purpose of this report is to:
- i) bring to Committee members' notice the current year revenue budget performance to date along with forecast outturns for the Services which relate to this Committee;
 - ii) advise on any areas of risk and management action.

2. RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
- i) note this report on the performance to date and the forecast outturn and the information on risks and management action that is contained herein;
 - ii) approve the virement proposals detailed within Appendix A;
 - iii) instruct that officers continue to review budget performance and report on Service strategies as required to ensure a balanced budget;
 - iv) instructs officers to report, in due course, on the actual outturn compared to budget following completion of the 2012/13 financial statements.

3. FINANCIAL IMPLICATIONS

- 3.1. The total Enterprise, Planning and Infrastructure budget currently amounts to £52.0M net expenditure.
- 3.2. Based upon present forecasts it is anticipated that the outturn for the Directorate will be £1.2M below budget. This position will be reflected in the overall financial monitoring for the Council when it is reported to Finance and Resources Committee at the end of this committee cycle.
- 3.3. Further details of the financial implications are set out in section 5 and in the appendix to this report.

4. OTHER IMPLICATIONS

4.1 None.

5. BACKGROUND/MAIN ISSUES

5.1 This report informs Members of the current year revenue budget performance for the Directorate to Period 5 (end of August 2012) and provides a high level summary for the consideration of Members. It also outlines whether or not there are any cost pressures that are immediately identifiable from the expenditure incurred to date and actions being undertaken to manage these.

5.2 The Directorate report and associated notes are attached at Appendix A.

Financial Position and Risks Assessment

The overall position for the Directorate at Period 5 is a net underspend of £1.4M. The forecast outturn for the full year is a net underspend of £1.2M. The reason for this movement is that a number of recharges to the Directorate do not take place until the year-end.

The outturn is based on a detailed review of progress towards achieving the income and expenditure budgets of each service and takes account of any factors which are expected to influence them. Detailed below are the main variances that are forecast to apply for the full year:

- Staff costs across a number of services are forecast to be £960K below budget due the ongoing management of vacancies.
- Fee income for the Non-Housing Design Team is forecast to exceed budget by £300K as the Service's workload for the current year is expected to increase in line with the Non-Housing Capital Plan.
- Fleet Management costs are forecast to be £130K below budget with increased short-term hires costs being offset by savings in vehicle lease costs.
- The cost of school catering provisions is forecast to be £70K above budget due to continuing pressure on food prices.
- Recharges for services to non-General Fund accounts are forecast to exceed budget by £500K. None of these recharges are new but some were not included in the budget and others

were reviewed in 2011/12 and revised charges for 2012/13 are anticipated as a result.

- Advertising income is forecast to be £200K below budget. The service will not achieve the additional income required by PBB option EBD03a.
- Other shortfalls in income totaling £100K are forecast across a number of services, primarily in Marketing Services and in exhibitions income.
- The outturns detailed in Appendix A incorporate the main risks identified by the Service to date. The most significant additional risk faced by the Service relates to the severity of the winter and its impact on roads maintenance budgets. This could add up to £100K per week of severe weather and would reduce the Directorate's overall favourable variance.

6. IMPACT

Corporate - as a recognized top priority, the Council must take the necessary measures to balance its budget. Therefore Committees and Services are required to work within a financial constraint. Every effort is being focused on delivering services more efficiently and effectively.

Public – this report is likely to be of public interest due to the size of the budgets involved and the nature of the services provided by Enterprise, Planning and Infrastructure, a number of which are front line services delivered directly to citizens and the business community within the city.

7. BACKGROUND PAPERS

Financial ledger data extracted for the period.

8. REPORT AUTHOR DETAILS

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**ABERDEEN CITY COUNCIL
REVENUE MONITORING 2012 / 2013**

DIRECTORATE : ENTERPRISE, PLANNING AND INFRASTRUCTURE

AS AT	31 August 2012	ANNUAL BUDGET	YEAR TO DATE			OUTTURN		
			PLANNED	ACTUAL	VARIANCE	FORECAST TOTALS	FORECAST VARIANCE	
ACCOUNTING PERIOD 5		£'000	£'000	£'000	£'000	£'000	£'000	%
HEAD OF ASSET MANAGEMENT AND OPERATIONS		39,657	13,252	11,908	(1,344)	38,440	(1,217)	(3.1%)
ECONOMIC DEVELOPMENT PROJECT DIRECTOR		4,401	1,840	1,903	64	4,722	320	7.3%
HEAD OF PLANNING AND SUSTAINABLE DEVELOPMENT		7,049	2,453	2,509	56	6,979	(69)	(1.0%)
OPERATIONAL SUPPORT MANAGER		935	318	173	(145)	725	(209)	(22.4%)
TOTAL BUDGET		52,042	17,862	16,493	(1,369)	50,866	(1,176)	-2.3%

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ABERDEEN CITY COUNCIL
REVENUE MONITORING 2012 / 2013

DIRECTORATE : ENTERPRISE, PLANNING AND INFRASTRUCTURE

AS AT	31 August 2012	ANNUAL BUDGET	YEAR TO DATE			OUTTURN			CHANGE FROM LAST REPORT
			PLANNED	ACTUAL	VARIANCE	FORECAST TOTALS	FORECAST VARIANCE		
ACCOUNTING PERIOD 5		£'000	£'000	£'000	£'000	£'000	£'000	%	£'000
STAFF COSTS	28,701	11,655	10,954	(702)	27,737	(964)	(3.4)%	620	
PROPERTY COSTS	10,548	5,887	5,395	(492)	10,268	(279)	(2.6)%	(200)	
ADMINISTRATION COSTS	1,503	444	595	151	1,624	121	8.1%	(514)	
TRANSPORT COSTS	5,157	2,149	2,083	(65)	5,073	(84)	(1.6)%	(58)	
SUPPLIES & SERVICES	14,263	5,943	6,671	728	14,568	305	2.1%	641	
TRANSFER PAYMENTS	3,043	1,268	1,168	(100)	2,972	(70)	(2.3)%	(42)	
CAPITAL FINANCING COSTS	11,703	21	42	21	11,715	11	0.1%	17	
GROSS EXPENDITURE	74,918	27,366	26,907	(459)	73,957	(960)	(1.3)%	464	
LESS: INCOME									
GOVERNMENT GRANTS	(2,936)	(1,197)	(1,852)	(655)	(2,289)	647	(22.0)%	59	
OTHER GRANTS	(851)	(354)	(224)	130	(598)	253	(29.7)%	(68)	
INTEREST	(17)	(7)	(2)	5	(7)	10	(59.8)%	0	
RECHARGES	(6,303)	(2,626)	(3,156)	(530)	(7,531)	(1,228)	19.5%	(829)	
OTHER INCOME	(12,768)	(5,320)	(5,179)	141	(12,666)	102	(0.8)%	(608)	
TOTAL INCOME	(22,875)	(9,504)	(10,414)	(909)	(23,091)	(215)	0.9%	(1,446)	
NET EXPENDITURE	52,042	17,862	16,493	(1,369)	50,866	(1,176)	(2.3)%	(982)	

VIREMENT PROPOSALS

Several. See details within individual Head of Service summaries.

REVENUE MONITORING VARIANCE NOTES	FORECAST VARIANCE £'000	CHANGE £'000
Employee Costs		
Vacancies have been identified and are being managed across all services.	(964)	620
Property Costs		
Savings are forecast in energy and repairs costs. Janitorial rents have been budgeted for but no costs are being incurred.	(279)	(200)
Administration Costs		
The postages budget is forecast to be overspent pending reorganisation of the postages budgets corporately.	121	(514)
Transport Costs		
Vehicle leasing costs will be below budget while external hires and repairs are forecast to exceed budget.	(84)	(58)
Supplies & Services		
School catering food costs are forecast to be higher than budget, in line with current trends. Supplies and services within Roads Services are above budget in line with the level of workload.	305	641
Transfer payments		
The forecast cost of contributions to a number of external bodies has been reduced on the basis of the actual level of payments made in 2011/12	(70)	(42)
Capital Financing		
No significant variance from budget is forecast at this stage.	11	17
Government Grants		
The budget for the Hytrec project was not matched correctly against expenditure and has since been reallocated. Recoveries for the AWPR project are forecast to be less than budget in line with the level of expenditure incurred.	647	59
Other Grants & Contributions		
Sponsorship and advertising income is forecast to be below budget.	253	(68)
Interest		
No significant variance from budget is forecast at this stage.	10	0
Recharges		
Accommodation recharges to non-General Fund accounts for Marischal College and the other main sites is forecast to be above budget. The Architectural Design team is forecasting income in excess of budget in line with their increased workload associated with the capital plan.	(1,228)	(829)
Other Income		
Building application fee income is forecast to exceed budget. A review of budgets has identified a number of recoveries that are unlikely to be made and the outturn has been updated to reflect this.	102	(608)
	(1,176)	(982)

DIRECTORATE : ENTERPRISE, PLANNING AND INFRASTRUCTURE
HEAD OF ASSET MANAGEMENT AND OPERATIONS

AS AT	31 August 2012	ANNUAL BUDGET	YEAR TO DATE			OUTTURN			CHANGE FROM LAST REPORT
			PLANNED	ACTUAL	VARIANCE	FORECAST TOTALS	FORECAST VARIANCE		
		£'000	£'000	£'000	£'000	£'000	%	£'000	
ACCOUNTING PERIOD 5									
STAFF COSTS		19,332	8,055	7,449	(606)	18,875	(457)	(2.4)%	148
PROPERTY COSTS		10,232	5,694	5,207	(487)	9,946	(286)	(2.8)%	(202)
ADMINISTRATION COSTS		871	265	417	152	1,018	147	16.9%	151
TRANSPORT COSTS		1,927	803	741	(62)	1,857	(71)	(3.7)%	(71)
SUPPLIES & SERVICES		10,450	4,354	4,293	(61)	10,760	310	3.0%	70
TRANSFER PAYMENTS		25	10	21	10	25	0	0.0%	0
CAPITAL FINANCING COSTS		11,102	21	42	21	11,102	0	0.0%	0
GROSS EXPENDITURE		53,938	19,202	18,169	(1,034)	53,582	(356)	(0.7)%	96
LESS: INCOME									
GOVERNMENT GRANTS		(1,050)	(438)	(617)	(179)	(1,056)	(6)	0.6%	0
OTHER GRANTS & CONTRIBUTIONS		(140)	(59)	(35)	24	(7)	133	(95.0)%	133
INTEREST		0	0	0	0	0	0	0.0%	0
RECHARGES		(5,802)	(2,418)	(2,855)	(437)	(6,737)	(935)	16.1%	(935)
OTHER INCOME		(7,288)	(3,037)	(2,755)	282	(7,342)	(54)	0.7%	(272)
TOTAL INCOME		(14,281)	(5,950)	(6,261)	(311)	(15,142)	(861)	6.0%	(1,074)
NET EXPENDITURE		39,657	13,252	11,908	(1,344)	38,440	(1,217)	(3.1)%	(977)

VIREMENT PROPOSALS

None this cycle

REVENUE MONITORING VARIANCE NOTES

Employee Costs

Vacancies have been identified and are being managed across the service.

FORECAST VARIANCE	CHANGE
£'000	£'000

(457)	148
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Property Costs

Marischal College energy costs are forecast to be £70K below budget. Property repairs for Marischal College and Kittybrewster are forecast to be £80K below budget. The budget to cover the cost of rents for janitors homes is no longer required, saving £130K.

(286)	(202)
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Administration Costs

The main item of overspend is forecast to be postages at £120K. It was expected that this budget would have been consolidated across the organisation but work on this is still ongoing.

147	151
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Transport Costs

Underspends are forecast in vehicle leasing costs (£230K). The cost of external hires is forecast to be £100K over budget and the cost of vehicle repairs is forecast to be £50K over budget.

(71)	(71)
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Supplies and Services

The cost of catering provisions is expected to be £70K above budget in line with current trends in food costs. The cost of fleet spares is forecast to be £30K above budget. The cost of supplies and services across a number of Roads Services functions is forecast to be £220K above budget, based on current levels of activity.

310	70
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Capital Financing Costs

No significant variance from budget is forecast at this stage.

0	0
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Income

Fee income for Non-Housing Design team is expected to be £300K above budget based on a forecast of increased workload associated with the current year's non-housing capital plan. A review of the allocation of accommodation costs carried out as part of the 2011/12 year-end exercise identified several areas where allocations to services outwith the General Fund had to be increased. The forecast incorporates these revised allocations and it is expected that an additional £500K will be recharged for the current year. The level of recoveries made by Roads Services is expected to exceed budget by £60K, reflecting an increased workload.

(861)	(1,074)
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(1,217)	(977)
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DIRECTORATE : ENTERPRISE, PLANNING AND INFRASTRUCTURE
HEAD OF PLANNING AND SUSTAINABLE DEVELOPMENT

AS AT	31 August 2012	ANNUAL BUDGET	YEAR TO DATE			OUTTURN			CHANGE FROM LAST REPORT
			PLANNED	ACTUAL	VARIANCE	FORECAST TOTALS	FORECAST VARIANCE		
		£'000	£'000	£'000	£'000	£'000	£'000	%	£'000
ACCOUNTING PERIOD 5									
STAFF COSTS		6,319	2,401	2,426	24	6,053	(266)	(4.2)%	509
PROPERTY COSTS		307	184	179	(5)	312	5	1.8%	0
ADMINISTRATION COSTS		345	60	46	(13)	235	(110)	(31.9)%	(726)
TRANSPORT COSTS		3,149	1,312	1,308	(4)	3,149	0	0.0%	1
SUPPLIES & SERVICES		2,581	1,075	1,918	842	2,629	48	1.9%	541
TRANSFER PAYMENTS		767	320	312	(8)	745	(22)	(2.9)%	(12)
CAPITAL FINANCING COSTS		590	0	0	0	590	(0)	(0.0)%	0
GROSS EXPENDITURE		14,058	5,352	6,188	836	13,713	(345)	(2.5)%	313
LESS: INCOME									
GOVERNMENT GRANTS		(1,871)	(759)	(1,301)	(541)	(1,393)	478	(25.6)%	2
OTHER GRANTS & CONTRIBUTIONS		(270)	(112)	(60)	52	(274)	(4)	1.4%	0
INTEREST		0	0	0	0	0	0	0.0%	0
RECHARGES		(157)	(66)	(138)	(73)	(329)	(172)	109.1%	0
OTHER INCOME		(4,710)	(1,963)	(2,180)	(217)	(4,738)	(28)	0.6%	(444)
TOTAL INCOME		(7,009)	(2,900)	(3,679)	(779)	(6,734)	275	(3.9)%	(441)
NET EXPENDITURE		7,049	2,453	2,509	56	6,979	(69)	(1.0)%	(128)

VIREMENT PROPOSALS

None this cycle.

REVENUE MONITORING VARIANCE NOTES

Employee Costs

Vacancies have been identified and are being managed across the service.

FORECAST VARIANCE	CHANGE
£'000	£'000

(266)	509
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Property Costs

No significant variance from budget is forecast at this stage.

5	0
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Administration Costs

Transport Strategy & Projects has a budget under this heading of £110K while the actual spend is forecast to be in consultants fees under Supplies & Services (see below).

(110)	(726)
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Transport Costs

No significant variance from budget is forecast at this stage.

0	1
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Supplies and Services

The cost of consultants used in Transport Strategy & Projects and in Roads Authority Works is forecast to be £150K above budget although in both cases, there is adequate budget under Administration Costs to cover the value of these works and a corresponding favourable variance. Savings are forecast in Development Management consultancy fees (£40K) and in Development Plans (30K). A further £30K of savings is forecast in materials costs across a number of services.

48	541
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Transfer Payments

Contributions to external bodies is forecast to be £20K below budget based on the actual level of contributions made in 2011/12.

(22)	(12)
------	------

Capital Financing Costs

No significant variance from budget is forecast at this stage.

(0)	0
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Income

The level of recovery necessary for the AWPR project is forecast to be £200K below budget. There is currently no income forecast against the SURF project whereas a £70K budget has been set for this. There are corresponding savings forecast in staff costs and administration costs and forecast outturns have since been re-aligned to eliminate these variances.

275	(441)
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DIRECTORATE : ENTERPRISE, PLANNING AND INFRASTRUCTURE
ECONOMIC DEVELOPMENT PROJECT DIRECTOR

AS AT	31 August 2012	ANNUAL BUDGET	YEAR TO DATE			OUTTURN			CHANGE FROM LAST REPORT
			PLANNED	ACTUAL	VARIANCE	FORECAST TOTALS	FORECAST VARIANCE		
ACCOUNTING PERIOD 5		£'000	£'000	£'000	£'000	£'000	£'000	%	£'000
STAFF COSTS	1,987	828	759	(69)	1,972	(15)	(0.7)%	(31)	
PROPERTY COSTS	9	8	9	0	10	1	16.5%	2	
ADMINISTRATION COSTS	247	103	120	17	301	54	21.9%	61	
TRANSPORT COSTS	75	31	32	1	62	(13)	(17.0)%	12	
SUPPLIES & SERVICES	1,213	506	459	(47)	1,165	(48)	(4.0)%	29	
TRANSFER PAYMENTS	2,250	938	836	(102)	2,202	(48)	(2.1)%	(30)	
CAPITAL FINANCING COSTS	12	0	0	0	23	11	94.6%	17	
GROSS EXPENDITURE	5,793	2,413	2,214	(199)	5,736	(57)	(1.0)%	60	
LESS: INCOME									
GOVERNMENT GRANTS	(15)	0	65	65	160	175	(1147.4)%	57	
OTHER GRANTS & CONTRIBUTIONS	(440)	(183)	(130)	53	(317)	123	(28.0)%	(201)	
INTEREST	(17)	(7)	(2)	5	(7)	10	(59.8)%	0	
RECHARGES	(149)	(62)	0	62	(264)	(115)	77.1%	(84)	
OTHER INCOME	(770)	(321)	(244)	77	(586)	183	(23.8)%	108	
TOTAL INCOME	(1,391)	(573)	(311)	263	(1,015)	377	(27.1)%	(121)	
NET EXPENDITURE	4,401	1,840	1,903	64	4,722	320	7.3%	(61)	

VIREMENT PROPOSALS

None this cycle.

REVENUE MONITORING VARIANCE NOTES

Employee Costs

Vacancies have been identified and are being managed across the service.

FORECAST VARIANCE	CHANGE
£'000	£'000
(15)	(31)

Property Costs

No significant variance from budget is forecast at this stage.

1	2
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Administration Costs

Sponsorship costs within Central Marketing are forecast to be £40K above budget. Subscription costs are forecast to be £20K above budget.

54	61
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Transport Costs

Travel expense costs associated with international trade and investment activities are forecast to be £20K below budget.

(13)	12
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Supplies and Services

Equipment purchases within Marketing & Design are forecast to be £30K below budget. Event costs within European Funding and Projects are forecast to be £20K below budget.

(48)	29
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Transfer Payments

Contributions to external bodies is forecast to be £30K below budget based on the actual level of contributions made in 2011/12.

(48)	(30)
------	------

Capital Financing Costs

Budgets have been re-aligned to reflect better the organizational structure of the service and some forecasts have not been updated. Corrections have now been made to eliminate this variance.

11	17
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Income

Sponsorship income within the Central Marketing service is forecast to be £130K below budget. Recoveries and advertising revenue within the Marketing and Design service are forecast to be £60K below budget. £160K of the remaining variance is in the Hytrec project and is due to a mis-match in the allocation of budget and expenditure, which has since been corrected.

377	(121)
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320	(61)
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ABERDEEN CITY COUNCIL
REVENUE MONITORING 2012 / 2013

DIRECTORATE : ENTERPRISE, PLANNING AND INFRASTRUCTURE
OPERATIONAL SUPPORT MANAGER

AS AT	31 August 2012	ANNUAL BUDGET	YEAR TO DATE			OUTTURN			CHANGE FROM LAST REPORT
			PLANNED	ACTUAL	VARIANCE	FORECAST TOTALS	FORECAST VARIANCE		
		£'000	£'000	£'000	£'000	£'000	%	£'000	
ACCOUNTING PERIOD 5									
STAFF COSTS	1,063	371	320	(51)	837	(227)	(21.3)%	(6)	
PROPERTY COSTS	0	0	0	0	0	0	0.0%	0	
ADMINISTRATION COSTS	40	17	12	(5)	71	30	74.1%	0	
TRANSPORT COSTS	6	3	2	(0)	5	(1)	(18.9)%	0	
SUPPLIES & SERVICES	19	8	1	(7)	13	(5)	(29.0)%	0	
TRANSFER PAYMENTS	0	0	0	0	0	0	0.0%	0	
CAPITAL FINANCING COSTS	0	0	0	0	0	0	0.0%	0	
GROSS EXPENDITURE	1,129	399	336	(63)	925	(203)	(18.0)%	(5)	
LESS: INCOME									
GOVERNMENT GRANTS	0	0	0	0	0	0	0.0%	0	
OTHER GRANTS & CONTRIBUTIONS	0	0	0	0	0	0	0.0%	0	
INTEREST	0	0	0	0	0	0	0.0%	0	
RECHARGES	(194)	(81)	(163)	(82)	(200)	(6)	3.2%	190	
OTHER INCOME	0	0	0	0	0	0	0.0%	0	
TOTAL INCOME	(194)	(81)	(163)	(82)	(200)	(6)	3.2%	190	
NET EXPENDITURE	935	318	173	(145)	725	(209)	(22.4)%	185	

VIREMENT PROPOSALS

None this cycle

REVENUE MONITORING VARIANCE NOTES

Employee Costs

The budget includes a £190K provision for living wage costs which will not be required and which has since been removed from the budget. There is an additional provision of £30K for redundancy payments which is forecast not to be needed either.

FORECAST VARIANCE £'000 CHANGE £'000
(227) (6)

Property Costs

No significant variance from budget is forecast at this stage.

0 0

Administration Costs

Adverse variances are forecast in storage and archiving (£20K) and in copying and subscriptions (£10K). In both cases this is based on a comparison of actual spend in 2011/12 with the budget provision for 2012/13.

30 0

Transport Costs

No significant variance from budget is forecast at this stage.

(1) 0

Supplies and Services

Various small savings are forecast in a number of areas.

(5) 0

Income

No significant variance from budget is forecast at this stage.

(6) 190

(209) 185

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning & Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Visit Aberdeen
REPORT NUMBER:	EPI/12/236

1. PURPOSE OF REPORT

To advise Committee of the current position regarding the establishment of Visit Aberdeen, a Aberdeen Destination Marketing Organisation (DMO) for the city and to detail the funding requirements to build on past investment to further drive the promotion of Aberdeen as a destination for business and leisure tourism.

2. RECOMMENDATION(S)

1. To note the current position with regard to the establishment of Visit Aberdeen and the production of their business plan.
2. To acknowledge the support needed from Aberdeen City Council to enable Visit Aberdeen to undertake the activities, and deliver the outputs, set out in their business plan.
3. To agree the allocation of £200,000 towards Visit Aberdeen's destination marketing activities from 6 November 2012 to 31 March 2013.
4. To agree the allocation of £520,000 towards future destination marketing activities from 1 April 2013 to 31 March 2014, then annually for a further four years subject to the council's normal budgetary process.
5. That Visit Aberdeen provides ACC with an annual Service Level Agreement and an updated Business Plan detailing their planned activities, plus regular progress reports, to enable the agreed monies to be released on a quarterly basis.

3. FINANCIAL IMPLICATIONS

Within its 2012-13 budget, Economic & Business Development currently has £520,000 allocated for activities designed to promote Aberdeen as a destination for business and leisure tourism, as follows:

Business Tourism Services (formally provided by Aberdeen Convention Bureau)	£213,750
Visitor Information Centre	£100,000
Local and International Marketing Activity	£ 71,250
Regional Promotion (in conjunction with ACSEF)	£ 85,000
Conference Subvention Support	£ 50,000
Total	£520,000

Recent realignment of the City Promotion service budget, in parallel with the service's recent re-organisation and the transfer of resources previously used to purchase VisitScotland services, will ensure that the above resources can safely be allocated to the DMO's establishment in the future without detriment to existing services.

Costs associated with the realisation of Visit Aberdeen will be met from within the existing City Promotion budget within Economic & Business Development and it is fully anticipated that the use of these funds will leverage significant additional resources from partner agencies and stakeholders from year's three to five of Visit Aberdeen's operation.

4. OTHER IMPLICATIONS

Following discussions with Visit Aberdeen and the staff within City Promotions and City Events, it has been agreed that all existing staff within the Economic & Business Development Service responsible for place marketing, inward investment and events would remain and that resources within the existing team would be allocated to support the activities of Visit Aberdeen going forward, as outlined in their summary business plan (see Appendix 1).

5. BACKGROUND/MAIN ISSUES

EP&I Committee on 13 September 2011 agreed to authorise:

(a) Aberdeen City Council officers to work with its partners at Visit Scotland, ACSEF, Aberdeen and Grampian Chamber of Commerce and private sector tourism businesses to establish a DMO Steering Group that would at no additional cost to Aberdeen City Council:

- establish a private sector led DMO
- appoint the DMO's Board of Directors, by 30 November 2011

- produce an action plan for the period 30 November 2011 to 30 September 2012;
 - implement the DMO's initial action plan;
 - appoint a Chief Executive, this was achieved in August 2012
 - supervise the production of a detailed, Five Year DMO business plan, by the new DMO Chief Executive, before 30 September 2012;
- (b) the submission of a further report to Enterprise, Planning and Infrastructure Committee in November 2012, requesting ongoing council support for the Aberdeen DMO for the next 3-5 years, based on existing resources (i.e. no new resources), subject to the DMO Chief Executive and Board:-
- (1) producing an appropriate, detailed and financially sustainable 3-5 year Business Plan; and
 - (2) securing significant private sector cash contributions or other non-council income streams, equivalent to at least 50% of the DMO's total annual operating costs over the 3-5 year business plan period.

Since the above recommendations were approved, officers have been working with the Board of Visit Aberdeen and the newly appointed Chief Executive to establish the organisation and produce a detailed business plan.

In summary, since September 2011, Visit Aberdeen has:

- Formed Visit Aberdeen Ltd, a company Ltd by Guarantee
- Appointed an Interim Independent Chair, six private sector board members representing the funding partners and/or appointed through public advertisement and three ACC elected members
- Appointed, after market tender, bankers, legal advisers and finance advisors/auditors.
- Appointed a CEO to take up post on 14 August 2012.
- Held various stakeholder engagement events, involving Board Members, Council officers and other stakeholders involved in the marketing of Aberdeen as a visitor destination (Aberdeen Airport, VisitScotland, Aberdeen Hotels Association etc.), as part of the business planning process.
- Produced a draft business plan for discussion with their Board and key funding partners

The draft business plan confirms that Visit Aberdeen's main aim is to be:

“The best city destination for their target markets”

This aim is consistent with Visit Aberdeen's intention to identify target markets that could potentially regard Aberdeen as an excellent visitor

destination and then convince people in these target markets that Aberdeen is the best destination available to them.

Visit Aberdeen's draft business plan identifies five key objectives that, if achieved, will realize this aim:

- Objective One: Market Aberdeen with Energy
- Objective Two: Provide great reasons to visit
- Objective Three: Deliver exceptional results for investors by realizing the full tourism potential of Aberdeen
- Objective Four: Tourism is everybody's business
- Objective Five: Run a tight ship

The draft business plan provides a significant amount of detail as to how these objectives will be achieved, how success will be measured, and describes the organisation structure, budget and funding needed to undertake this activity.

The Board of Visit Aberdeen have approved the draft business plan and a summary of this is attached in Appendix 1. Committee are asked to review this and accept it provides a valid basis for providing Visit Aberdeen with the requested grant funding.

6. BACKGROUND PAPERS

Appendix 1 - Visit Aberdeen's Business Plan Executive Summary.

7. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning and Infrastructure Committee
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Aberdeen Digital Connectivity Update Report
REPORT NUMBER:	EPI/12/208

1. PURPOSE OF REPORT

This report provides:-

- an update on UK and Scottish Government activities and
- an outline of ACC's Activities to date.

RECOMMENDATION(S)

That the Committee:

- notes the details within this report,
- endorses the bid to the Department of Culture Media and Sport's (DCMS) 2nd round of Urban Broadband Funding (UBF),
- instructs that officers, that should the bid be successful, undertake the processes required to implement the plan within this bid, taking into account any changes that are required by the funder;
- instructs officers to procure the necessary additional technical advice at an estimated £330k, in order to prepare for the procurement of private sector development partners to implement the city wireless and open access broadband projects within this plan,

2. FINANCIAL IMPLICATIONS

As a result of the Council's 2012/13 capital programme, provision has been made to invest up to £2 million in Open Access and City Wireless Digital Programme over the next two financial years. The scheme identifies 4 projects. The most significant of these projects is the Open Access Fibre Network to which Aberdeen City Council's (ACC) full £2 million will be used in addition to a further £2 million from Aberdeenshire Council. This comes to a total of £4 million for this project. Within the UBF Bid ACC has applied for a further £3 million from the DCMS for this. This will create a significant stake-holding, along with any existing asset to which ACC can make an 'in-kind' contribution in seeking a Private Sector Partner. The Private Sector Development partners will benefit with access to the growth and development

sectors in the city, with the ability to utilise ACC's assets and existing infrastructure. The other three projects within the bid require only an in-kind contribution from ACC. The bid has commercially sensitive information but is available for Elected Members to view on request to the author of this report.

4. SERVICE & COMMUNITY IMPACT

An Equalities Human Rights Impact Assessment was undertaken as part of the report to this committee on 13 September 2011. These significant infrastructure projects have the potential to provide benefit in the delivery of frontline services which reliant on digital connectivity in their delivery. This covers services in social care, education, community wardens work, environmental health, building services and planning. The contents of the report relate directly to delivering on key National Outcomes of the Single Outcome Agreement:

National Outcome 1 – We live in a Scotland that is the most attractive place for doing business in Europe

National Outcome 2 – We realise our full economic potential with more and better employment opportunities for our people.

National Outcome 3 - We are better educated, more skilled and more successful, renowned for our research and innovation.

Delivering Next Generation Access (NGA) with associated affordability levels for disadvantaged communities (geographical and societal) will support and enable community engagement to improve life chances and opportunities. This also supports the administration's Smarter Aberdeen Policy Statement as NGA.

Aberdeen City and Shire Economic Futures (ACSEF) identified a number of areas within the Economic Action Plan where NGA can play a critical role in transport, global connectivity, attracting and developing skilled people and company headquarters.

The delivery of next generation broadband and wireless connectivity is consistent with ACC's administration policy – "Aberdeen a Smarter City". It supports ACC's aspiration for Aberdeen to be a Smarter City. The widespread availability of NGA will allow public services to be more cost effectively delivered – this will become increasingly important as Council budgets reduce in real terms. A good example of this has been the savings made by building services who already use Consillium's Total mobile application and have made cost savings in the region of £1.5 million. ACC's enforcement officers are also using mobile technology but due to the poor connection across the city, are unable to utilise the full benefits of real time data transfer. ACC invests in the region of £1 million per annum in information technology with more services opting to use it so as to provide effective and efficient services.

5. OTHER IMPLICATIONS

The European Commission currently has Draft EU Guidelines for applying State Aid rules in relation to the rapid deployment of broadband networks. Digital Connectivity Procurements have significant State Aid implications. The UBF bid required all bidders to provide a State Aid opinion for their options. ACC obtained expert advice from TaylorWessing, the advisors to Birmingham City Council who has already obtained State Aid clearance from the European Commission.

The plan would see ACC partnering with third parties to deliver services across the city and the potential that ACC utilise current property and asset portfolios to assist this delivery. This plan may also provide an opportunity to identify ongoing revenue income. Which could be reinvested into infrastructure, access to areas which would be otherwise uneconomic etc.

If the programme is approved for further development any implications arising will be reported to committee as part of ongoing process.

6. REPORT

6.1 Background

On 9 November 2010 this Committee instructed officers to develop a programme to enable the delivery of improved broadband connectivity to domestic and commercial users in Aberdeen. This was undertaken by Aberdeen City and Shire Economic Future (ACSEF), Aberdeen City Council (ACC) and Aberdeenshire Council (AC). As a result of this on 11 September 2012, the Enterprise Planning and Infrastructure Committee agreed to support efforts to lobby Scottish Government for funding, appoint an advisor to assist officers to implement the recommendations with in the 'Maximising Digital Connectivity'; and to support submission of an initial bid for up to £2 million of capital funding, from Aberdeen City Council's 2012/13 capital programme.

The proposal presented to this committee on 11 September 2012 identified three key projects, namely;

- (1) **An open access fibre** network built adjacent to the future peripheral road around the City to connect key business areas such as Westhill and Dyce and the residential suburbs like Kingswells and Dyce with the potential extension to the Shire from Westhill and along the Energetica corridor. Due to commercial sensitivity the cost are unable to be published.
- (2) **A rural access strategy** to ensure that there is 100% availability of broadband services in the rural areas and significantly increased access speeds. AC are working with Scottish Government on this with the costs being part of the current proposed Scottish Government procurement process.
- (3) **Deployment of next generation wireless technology** in Aberdeen's core city area, to be a concessionary scheme which would be cost neutral to the ACC..

It was believed that Projects (1) and (2) could obtain Central Government funding through British Digital UK programmes being managed in Scotland by the Scottish Government and could also potentially be suitable for European funding programmes.

Project (3) may be deliverable without public subsidy.

6.2 Update

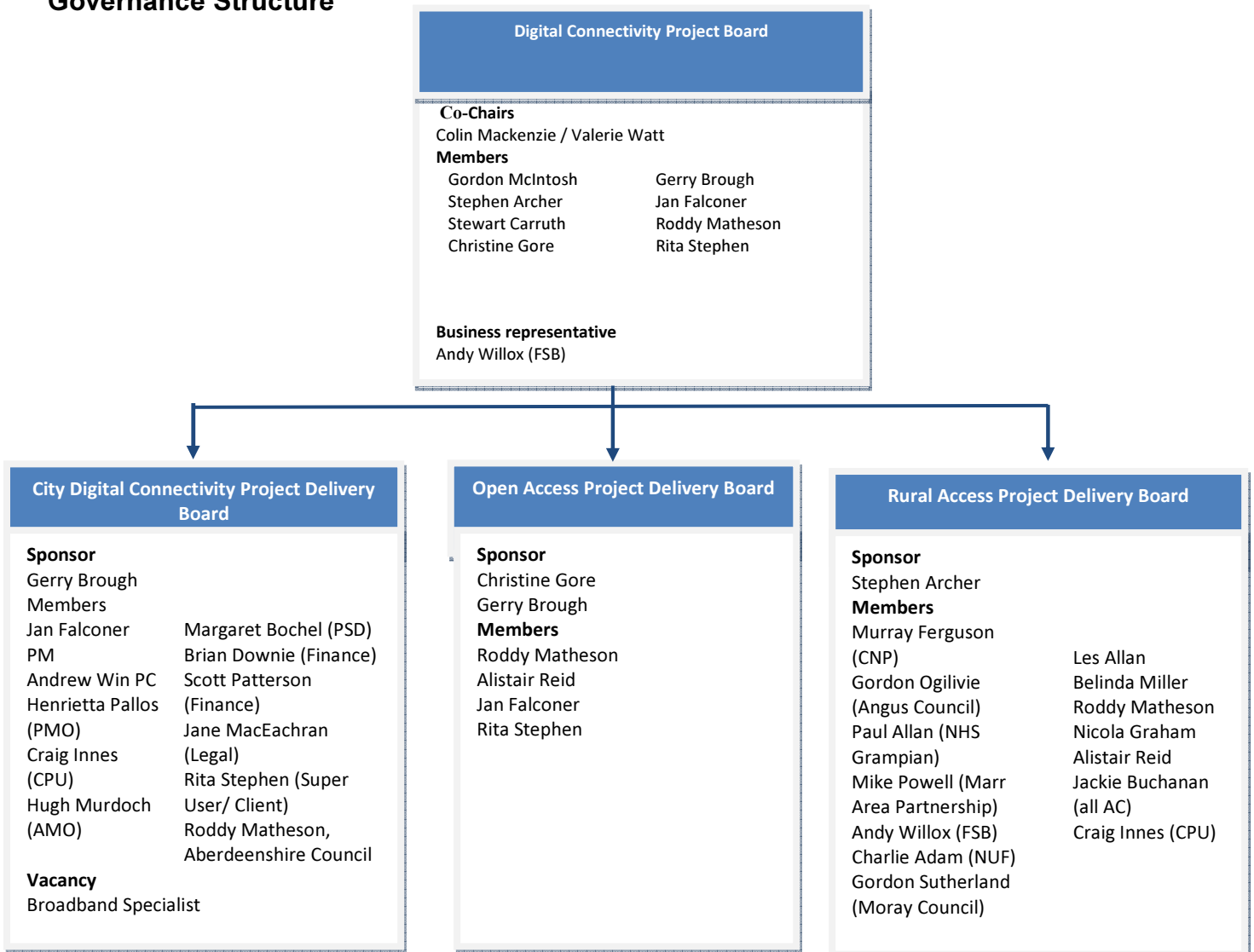
Since the Committee granted approval for this initiative a number of actions have been undertaken. ACSEF funded a Project implementation plan for City next generation wireless. This was used to assist in bringing together a capital bid to cover the full required investment for Open Access Fibre and City Wireless Digital Connectivity which is part of ACC's five year

business plan. It was agreed on 9 February 2012 by Council that £2 million will be part of the Council's three year capital programme.

Joint Governance Structures, as shown overleaf, with the ACC and Aberdeen City Council have been established. The Digital Connectivity Board is a joint board between ACC and AC. It is co-chaired by its Chief Executives. Its membership consists of the two Councils' Directors of Enterprise, Directors of Corporate Governance, Project Sponsors and Project Managers, Super-users - ACSEF representative (as a public-private partnership) and a business representative from the Federation of Small Business. The remit of this board is to receive reports and oversee all Digital Connectivity projects across the North East of Scotland. The board meets bi-monthly and all meetings are documented. The Project's Terms of Reference have been developed to give a focus of the expectation and requirements of this Board.

ACC has a City Digital Connectivity Project Delivery Board which is tasked to deliver the City wireless project but this is being extended to include broadband connectivity in the City's contiguous area including Westhill. The Delivery Board consists of the Project Sponsor, Project Manager, Project Co-ordinator, Head of Procurement, Head of Asset Management, Head of Planning, Legal Advisor, Financial Advisor, Programme Management Office (PMO) representative, Super-user (ACSEF), Advisor from Aberdeenshire Council and Broadband Specialist. It meets monthly and its meetings are documented.

Governance Structure



The Scottish Government in the meantime produced its 'Scotland's Digital Future Action Plan. It consists of four Programmes. Programme One is called 'Achieving a Step Change to 2015'. Its aim is to address the digital divide, specifically for those areas where the market will currently not go, to ensure a step change in speeds by 2015. The outcome we are seeking is a significant uplift in speeds for everyone by 2015, with speeds of 40 – 80Mbps for 85% to 90% of premises. Scottish Government had planned to produce a Procurement Strategy by March 2012, commence procurement in September 2012 so as to award a contract by the first half 2013.

The Scottish Government is producing a procurement strategy where it aims to use the UK Government's procurement process in order to gain State Aid approval. As this is yet to be achieved the procurement has been delayed. Though exempt from this ACC continues to work with Scottish Government.

BT announced early this year it would upgrade two of its 13 exchanges that serve the city (nine in the City and four in the Shire). A further four exchanges were due to be upgraded by September 2012 and one more by September 2013. Whilst these exchanges are concentrated around the City Centre, BT is a commercial enterprise which will only undertake work on cabinets that are commercially viable. ACC's modelling shows that this will only upgrade 66% of premises in Aberdeen (this excludes new planned developments.) whilst the Scottish Government's modelling indicates that 70% of premises will be covered. ACC is exempt from Programme One funding as the market will meet the city's' needs. Aberdeenshire is included in Programme One and has the Rural Broadband Project Delivery Board whose membership consists of a range of stakeholders and AC officers. This Board oversees the Rural Broadband work being undertaken as Phase one of the Scottish Government's Digital Action Plan. It is also part of the Aberdeen City and Shire joint Digital Connectivity Board.

The Scottish Government's Digital Action Plan Programme Two aims to achieve world class digital connectivity by 2020. Whilst there is no timing for this programme, Scottish Government is undertaking a Public Consultation for the procurement for the rest of Scotland to which they have identified £100 million of public funding to which the Scottish Government will expect local authority contributions which have yet to be determined. ACC has also been working with the Scottish Cities Alliance (Aberdeen, Dundee, Edinburgh, Glasgow, Perth and Stirling) on a Cities Digital Project which aims to focus on bringing together work with mobile operators, smart ticketing, wireless technology and transport and intra-city collaboration in digital connectivity. Cities are sharing 'lessons learned', along with shared resources and have a comprehensive stock-take on where Cities and regions are in the digital mix. This is an important element within the City and Shire's Digital Connectivity Strategy.

6.3 DCMS UBF Phase 2 Bid

A range of groups, included ACC, wrote and lobbied the UK government due to Aberdeen being excluded from the earlier Phase one of Urban Broadband

Funding for large cities. In June 2012 the Chancellor George Osborne announced a £50 million fund for which 27 smaller cities including Aberdeen, Dundee and Perth could compete, with the closing date being 17 September 2012. The City Digital Delivery Board produced ACCs bid to the Department of Culture, Media and Sport's UBF Phase 2 bid with technical advice from Mott MacDonald and specialist State Aid advice which was submitted to DCMS on 14 September 2012.

The bid covers the contiguous area of Aberdeen City and includes Westhill. The technical strategy to deliver enhanced digital connectivity across Aberdeen will be addressed in four parallel work streams:-

- **Open Access Fibre Network** - a network built around the key developments areas in the City including the new growth areas defined by LDP12 and existing key business hubs such as Aberdeen Airport and the Bridge of Don. This also includes 20,000 homes and 196 hectares of employment & industrial land. It also includes 3,000 houses and 10 hectares of employment land in Countesswells and up to 8,000 homes in Elsick. These are areas in the City and Shire that form part of the contiguous area. Access to the major business parks in the neighbouring Shire (notably Westhill). Both Councils are contributing to this project. Both ACC and AC will contribute to this.
- **City Wifi Scheme** – to provide next generation wireless network deployed across the City to give enhanced broadband access capability for both residents and businesses. The access speeds obtained will be a function of the number of base stations deployed delivering download speeds of greater than 20Mbps. A deployment of this nature will provide citizens with an alternative to the ADSL services typically used in the City that suffer from contention and hence limited performance. It would also provide a commercial incentive to upgrade the City's exchanges. Implementation of WiFi hotspots across the City would provide benefits to businesses and consumers – providing remote access to key applications and flexible working. It is also likely to be attractive to visitors – enabling tourists to access local information and travel data. This is a concessionary scheme and would only requiring the use of Council assets as an investment stake.
- **Voucher Scheme**- available to SMEs including Non-Government Organisations, Charitable organisations etc to provide a grant up to £500 to contribute towards capital works to enable digital connectivity
- **White area in-fill**¹- use UBF funding to gap fund the elimination of such white areas (subject to state aid approval). This will be undertaken either through a separate ACC procurement or as part of wider national procurements currently being undertaken by the Scottish Government. Should this procurement fail to eventuate ACC will make efforts to meet the DCMS's requirements within the given timeframes.

¹ An area where there is an absence of affordable services at 2Mbit/s or above could be a useful retail indicator of a lack of effective competition and/or lack of economic viability for basic broadband networks, which will be surrounded by grey and black areas. Grey areas In a grey area NGA networks may exist or be planned, but such network or networks have not been constructed in an open way such that competition in high speed broadband services can be assured and there is no proven c commitment by any operator to deploy an inherently open NGA network in the coming 5 years. A black area has access to NGA. The in-fill is providing NGA.

Should ACC be successful in its bid it is envisaged that ACC will undertake an accelerated procurement process commencing in early December 2012. This is because the UBF funds must be spent by December 2014. As ACC's bid is still in a competitive process, we are unable to disclose the total amount that has been bid. Should the bid be unsuccessful it is still viable to pursue the Open Access Fibre Network and City Wifi Scheme. ACC already has a procurement brief for a City Wifi Scheme, produced by Mott MacDonald. The Open Access Fibre Network has £4 million of funds from ACC and AC and it is possible to undertake the necessary procurement process so as to gain a development partner to undertake the work that is required. This plan not only considers the key development areas but will address the areas that will struggle to gain connectivity. The estimates the cost of specialist Procurement and legal advice be in the region of £330,000. There is budget allocated for this cost which will be spread across both this and next financial year.

Whilst the White area in-fill may not be possible in the short-term, this may eventually be addressed by the market. The voucher system may also be difficult to achieve but collaboratively with the Scottish City Alliance, ACC is working on Demand stimulation initiatives which can be replicated in the City.

7. REPORT AUTHOR DETAILS

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8. BACKGROUND PAPERS

The Scottish Government (2012) Infrastructure Action Plan - Step Change 2015
Rest of Scotland Procurement Public Consultation
The Scottish Government (2012) Scotland's Digital Future First Annual Progress Report and Update – 2012
European Commission, (2012) *Draft EU Guidelines for the application of state aid rules in relation to the rapid deployment of broadband networks*
The House of Lords Committee Communications Session 2012-2013 (2012) *Broadband for all— an alternative vision*
The Scottish Government (2012) *Scotland's Digital Future - Infrastructure Action Plan*
ACSEF Management Team Paper (2011) *Maximising Digital Connectivity – Next steps and Action Plan*
The Scottish Government (2011) *Scotland's Digital Future A Strategy for Scotland*, <http://www.scotland.gov.uk/Resource/Doc/981/0114237.pdf>
DCMS (December 2010) *Britain's Superfast Broadband Future*

ABERDEEN CITY COUNCIL

COMMITTEE Enterprise, Planning and Infrastructure

DATE 6th November 2012

DIRECTOR Gordon McIntosh

TITLE OF REPORT International Partnerships & Twinning Applications

REPORT NUMBER: EPI/12/197

1. PURPOSE OF REPORT

The purpose of this report is to bring before Committee applications for financial assistance from the International Twinning Budget 2012-2013 and to make recommendations.

2. RECOMMENDATION(S)

That the Enterprise, Planning and Infrastructure Committee recommend the approval of the following grants from the International Twinning Budget 2012-2013:

- i) £1,790 towards the participation of two entertainers from Stavanger, Clermont-Ferrand and Regensburg in Aberdeen City's Christmas Light Switch-On Parade 24th to 26th November 2012.
- ii) £3,410 maximum contribution towards the participation of traditional Norwegian singers from Stavanger to enhance the Christmas Tree Light Switch-On Ceremony, Aberdeen, 29th November to 1st December 2012.
- iii) £2,400 contribution towards the visit of three Aberdeen City Council Officers to the Festival du Court Métrage, Clermont-Ferrand, 1st to 8th February 2013.
- iv) £3,960 towards the participation of an Aberdeen delegation in the Social Initiative Market in Regensburg, 4th to 11th December 2012.
- v) £6,800 contribution towards the inclusion of two musical elements from Regensburg and Stavanger twin cities in the Aberdeen Jazz Festival 2013, 15th to 17th March 2013.

3. FINANCIAL IMPLICATIONS

Full council approved the International Twinning Budget for 2012-2013 of £137,350. The approved budget from the Common Good Fund, provides £102,000 of monies towards the support of twinning projects and/or visits.

	Balance of Budget Available	£65,860
i)	Contribution towards the participation of two entertainers from Stavanger, Clermont-Ferrand and Regensburg in Aberdeen City's Christmas Light Switch-On Parade 24 th to 26 th November 2012.	£1,790
ii)	Contribution towards the participation of traditional Norwegian singers from Stavanger to enhance the Christmas Tree Light Switch-On Ceremony, Aberdeen, 29 th November to 1 st December 2012.	£3,410
iii)	Contribution towards the visit of three Aberdeen City Council Officers to the Festival du Court Métrage, Clermont-Ferrand, 1 st to 8 th February 2013.	£2,400
iv)	Contribution towards the participation of an Aberdeen Delegation in the Social Initiative Market in Regensburg, 4 th to 11 th December 2012.	£3,960
v)	Contribution towards the inclusion of two musical elements from Regensburg and Stavanger twin cities in the Aberdeen Jazz Festival 2013, 15 th to 17 th March 2013.	£6,800
	TOTAL	£18,360
	Amount remaining in budget if recommendations approved:	£47,500

4. OTHER IMPLICATIONS

The health and safety implications of all visits are taken into account during planning, with any exceptional implications addressed at the time of application.

The International Partnerships Officer regularly reviews the travel advice provided by the Foreign and Commonwealth Office and is in close contact with counterpart officers in the twin cities to ensure information provided to participants is accurate and up-to-date.

Where individuals/groups from Aberdeen are travelling overseas, comprehensive pre-visit briefings are offered by the International

Partnerships Officer and all have the opportunity to ask questions and request support.

When participating in overseas visits, Council employees are covered by the Council's insurance policy while non-Council employees are instructed by the International Partnerships Officer to obtain comprehensive travel insurance.

Long established twinning links will not be maintained without regular reciprocal visits and ongoing projects being initiated by communities, groups and individuals and there is the potential for the city's international image to be damaged if we don't continue to support established relationships.

Without support from the twinning budget, exchanges and visits of this nature could not take place and people in Aberdeen would lose a wide range of opportunities to engage with the international community. In addition, incoming visitors to the city on twin city projects support the local economy through their participation at events and hospitality and retail expenditure while visiting the city.

The International Partnerships Officer will provide a strong link between Aberdeen and our twin cities who also provide organisational support, advice and guidance when required.

They will also make recommendations on funding available to the group and offer support to foster a relationship with the city so that it is long lasting and beneficial to the people of Aberdeen without requiring on-going financial support from the public purse.

All groups participating in twin city activities are encouraged to develop sustainable relationships with their partner groups. Links between most of the twin cities of Aberdeen are active, and links which have not recently been engaged have been contacted to re-establish communications. It is important that all visits and projects through twinning are long-standing and fruitful to ensure their benefit to the local people of Aberdeen, to establish and promote positively the overseas image of Aberdeen and for best practice to be shared between cities.

5. BACKGROUND/MAIN ISSUES

- i) **Contribution towards the participation of two entertainers from Regensburg, Clermont-Ferrand and Stavanger to participate in the Aberdeen City's Christmas Light Switch-On Parade, 24th to 26th November 2012 .**

The annual Christmas Light Switch-On Parade is a popular and well-attended event within the Winter Festival programme. Last year, over 25,000 people came to Aberdeen city centre to see the festive lights

being switched-on. The event normally consists of local groups and entertainers from across the UK. This will be the first time an international element will be added to the festival.

The City Events & Twinning team is looking into ways to incorporate the unique offerings from our twin cities to enhance and value-add to Aberdeen's existing annual events programme. This involvement offers an excellent opportunity to demonstrate to Aberdeen's population and overseas visitors our twin city connections and that Aberdeen strives to include an international dimension in its events.

After discussions with organisers of the parade, it was decided two entertainers from Stavanger, Clermont-Ferrand and Regensburg will participate in the parade. Each of the entertainers will wear the traditional costume of their country and perform traditional folk dances for the crowds. This addition to the parade will add a new dynamic to this family orientated event which is key fixture in the Winter Festival programme.

The twin cities participation in the parade will add a new element to this even changing event and will further promote the international links Aberdeen is proud of. . It will also highlight cultural differences between Aberdeen and its twin cities and the traditional ways in which we celebrate the festive season. Aberdeen is dedicated to demonstrating the importance of our twin cities by building and developing new projects which will benefit the local population and those of our overseas partners. It will further promote Aberdeen as a welcoming and open city which values its long standing relationships.. Our invitation to the twin cities has already initiated reciprocal invitations for Aberdeen performers to participate in events such as Stavanger's National Day in May 2013. This will continue to promote Aberdeen City as an exciting place to visit, full of tradition, culture and heritage.

The contribution asks for a maximum £1,790 contribution towards the participation of the six entertainers to cover the costs of accommodation, meals and transport. The twin cities would cover the cost of air flights for their participants to and from Aberdeen.

ii) Contribution towards the participation of traditional Norwegian singers from Stavanger at the Christmas Tree Light Switch-On Ceremony, Aberdeen, 29th November to 1st December 2012.

Another key event during the Winter Festival programme is the Christmas Tree Light Switch-On Ceremony. Each year the tree is generously gifted to Aberdeen from the City of Stavanger. This year, the dates of the ceremony have been changed in order to allow the Mayor of Stavanger to attend. Given the involvement of the Mayor of Stavanger and representatives from the Norwegian community in the tree ceremony, Stavanger Kommune has encouraged closer collaboration and suggested the inclusion of a cultural element traditional to Norway in the Christmas Tree Light Switch-On Ceremony.

Stavanger Kommune have proposed a group of traditional Norwegian singers perform at the Christmas Tree Light Switch-On Ceremony to promote the Aberdeen and Stavanger connection and demonstrate the strong links between the two cities. Some of the younger singers will be dressed in traditional costume of Norway, and the singers will perform a mixture of Norwegian and Scottish songs. Their performances will reflect the traditional customs of Norway and fulfill the request from Stavanger Kommune. The citizens of Aberdeen will be invited to attend the ceremony to enjoy the choir as they sing popular Scottish songs.

During the day of Thursday 29th November, various business meetings will be undertaken. The link between Stavanger and Aberdeen has in the past mainly focused on the business and economic links. Stavanger's participation in the Christmas Tree Ceremony will promote both cities abilities to combine business, culture and tradition. It will also demonstrate to Stavanger that Aberdeen is able to effectively respond to requests and continue to have successful working relationships which are sustainable and provide mutual benefit.

Also on the 29th November,, the singers will perform in various city centre locations in Aberdeen to highlight the Christmas Tree Switch-On Event, thus encouraging a larger audience to attend the event. After the ceremony they will return to the city centre malls to perform again. The evening of Thursday 29th November has been designated by the BID as a 'Shopping Extravaganza' so the Norwegian performances will add to the whole retail experience.

The Norwegian choir will also perform at the Nativity Scene and the Winter Gardens in Duthie Park on Friday 30th November to enhance the Winter Festival programme and increase the entertainment available to the local communities. The choir will then perform in Aberdeen Art Gallery on Saturday 1st December as the Gallery is holding a special event for the children. Their performance will be an excellent opportunity to showcase to the children and their families Aberdeen's connections with Stavanger, increase their understanding of different cultures and the number of family orientated events in Aberdeen.

Stavanger's participation in the Winter Festival is aimed at enhancing the city events programme and providing the public with international, high quality performances from our twin city. For both the Light Switch-On Parade and the Christmas Tree Ceremony, feedback will be sought from the event organisers and the twin cities in order to determine the success and value added to the events. The feedback will be used to determine to pursue twin cities participation in forthcoming years.

Most of the singers have stated that when their performances finish in Aberdeen that they will be staying on in Aberdeen in order to see more

of the city and enjoy the retail opportunities available. Any of the singers who remain in Aberdeen for their own leisure purposes will pay their own hotel costs etc.

The application requests a total of £3,410 to contribute towards the cost of accommodation, subsistence, travel and a contingency to meet any unanticipated costs. The participants will be covering some of the cost of accommodation and Stavanger Kommune is paying the cost of the flights for the singers and for the total cost of the visit of the Mayor of Stavanger.

iii) Contribution towards the visit of three Officers to the Festival du Court Métrage, 1st to 8th February 2013.

The Court du Métrage in Clermont-Ferrand is an international short film festival and in 2013 it will be the 35th annual festival to be held. The Festival provides an annual meeting place and exhibition space for various international and national organisations, agencies, individuals, societies and industries to showcase their activities, present their work, network and prepare for their next film or video productions.

It is proposed that three Aberdeen City Council Officers attend the Clermont-Ferrand Festival du Court Métrage during the 2013 exhibition in order to determine how artists can submit a film to the Festival and what requirements there are to the film production. The visit will also establish the appropriate partners in Clermont-Ferrand and other industry professionals with whom Aberdeen should work with in order to successfully submit any short films. The links that would be forged during attendance will ensure that Aberdeen will have the opportunity to showcase short films filmed and produced in Aberdeen at the 2014 Court Métrage. The year 2014 is particularly important to have work produced in Aberdeen shown at this Festival since it will be Homecoming year and Commonwealth Games in Glasgow. With much of the international focus on Glasgow, the International Festival in Clermont-Ferrand allows Aberdeen to promote its local film-making talent and cultural vibrancy.

The main aim of the visit is to showcase homegrown talent on an international platform, but the Court du Métrage is also an excellent way of promoting Aberdeen City as a destination due to the wide, international audience and could provide incentives for film-makers to film their productions in Aberdeen or in the surrounding areas.

Citymoves Aberdeen has already expressed interest in submitting a video to the Court du Métrage to portray the local dance talent and culture in Aberdeen. The visit will allow for the knowledge and expertise to be learnt in order to explain how local groups in Aberdeen can submit a video or film to demonstrate Aberdeen is a community based, talented and full of culture and artistic talent. Additionally, Aberdeen

College provides a course in 'Film School'. Local Aberdeen students would have the opportunity to include their work within the Festival du Court Métrage. Arts Development would also work with the most vulnerable and disadvantaged citizens of Aberdeen. If the funding is approved the links will help the team to work with these groups to develop, produce and showcase their work.

Aberdeen City Council has strong arts and cultural links with Regensburg, but has in the past struggled to find suitable cultural partners in Clermont-Ferrand in which to engage in this internationally recognised Festival. The International Partnerships Officer, through contacts in Clermont-Ferrand, has successfully managed to establish contact with the organisers of the Festival who want to welcome Aberdeen to their event and undertake meetings to discuss deeper involvement and participation for 2014.

The application requests a maximum of £2,400 to cover the costs of flights and some subsistence for the Officers whilst in Clermont-Ferrand. Clermont-Ferrand will cover the cost of accommodation for the delegates as part of the normal twinning agreement for visiting groups. Following the visit, applications will be made to Creative Scotland to secure future funding to support ongoing development.

iv) Contribution towards the participation of Aberdeen Delegation in the Social Initiative Market, Regensburg between the 4th and 11th December 2012.

The Arts Development team at Aberdeen City Council was invited to populate a stall at the Regensburg Social Initiative Market, during their recent visit to Regensburg for the Photo Symposium. It is anticipated between four to six adults will travel to Regensburg in order to participate in the Market. The delegation from Aberdeen will compose of the Aberdeen artists selling their products at the Market, as well as Aberdeen City Council Officers who will provide support and assistance to the artists and can promote the Aberdeen city as vibrant place to visit, work and live whilst in Regensburg. A delegation of between four to six people is also required as the Market in Regensburg operates between 1000 and 2100 therefore the delegation will take it in rota shifts to operate their stall, ensure that the appropriate working breaks can be taken as identified in the risk assessment and also to provide training sessions.

The group will offer training sessions to groups involved with the Social Initiatives Market to help promote the arts as a tool for improving social inclusion. The work made for this project with Regensburg will be made available to local outlets in Aberdeen to promote the work of the Arts Development team and raise awareness of the strong twinning links with Regensburg.

For 2012, a stall will be held in Regensburg aided by Aberdeen City Council Art's Development team promoting the social enterprise project currently being undertaken by the Arts Development team. This year the team and artists will work with the Art Development team volunteers, the 100 Word Project and the adult justice service to create small works to take to the market and sell to raise funds to sustain projects in Aberdeen.

Aberdeen continues to be invited by Regensburg to participate in their Christmas Markets which is a credit to Aberdeen City Council for their innovative projects being undertaken in the local community and being used as a model of forward thinking.

The people involved in designing, producing and packaging the artwork will be provided with an international platform for the sale of their work. The quality of the work shown at the market in Regensburg will promote the creative talents of the makers and the work Aberdeen City Council delivers to enable social inclusion. It will further promote Aberdeen as a city who actively engages its citizens in creativity and a supports a culturally rich environment in which to live, visit, and learn.

The application requests a total of £3,960 towards the cost of air travel, local transport and some subsistence. A break-down of costs is given below.

Expenditure	
Flights	£2,400
Transport	£300
Subsistence	£1,260
Hire of Stall	£400
Accommodation	£4,200
Staff costs, materials	£8,000
Total	£16,560
Income	
Estimated Fund-Raising	£400
Contribution by participants	£12,200
Total	£12,200
Expenditure-Income	£16,560-£12,200
Total Applying For:	£3,960

v) **Contribution towards the inclusion of two musical elements from Regensburg and Stavanger twin cities in the Aberdeen Jazz Festival 2013, 15th to 17th March 2013. .**

Jazz Aberdeen is a not for profit organisation which was started by the Aberdeen Jazz Group. Since its first annual Jazz Festival in 2003, the festival has helped Aberdeen in promoting jazz as it involves both local

and international artists, therefore attracting a large crowd of jazz fans to Aberdeen. The Festival also hosts various workshops for amateur jazz players and schools, ensuring that local people can benefit from the event.

During the Aberdeen Jazz Festival 2012 two of Aberdeen's twin cities, Regensburg and Stavanger participated and were highly successful during the event and received excellent media coverage with the band from Regensburg described as '*punchy and dynamic*'. As a result of the performance of the band from Regensburg, a new band formed consisting of jazz musicians from both Aberdeen and Regensburg. Both twin cities participation encouraged a younger audience to attend the Festival, particularly to witness the young jazz band Skadne Krek from Stavanger.

The twin city links with Stavanger were strengthened by inviting a jazz club promoter from Stavanger who has since invited jazz musicians from Aberdeen to perform in Stavanger. Information on the Aberdeen Jazz Festival is sent to jazz bookers in Stavanger and it was found that people travelled from Stavanger to Aberdeen to attend the Festival and enjoyed retail and other leisure pursuits during their stay. It is evidence that the Aberdeen Jazz Festival, through the twin city links, is successful in attracting an international audience and internationally promoting the Aberdeen city, its culture and music.

The Aberdeen Jazz Festival 2013 organisers wish to continue to promote the jazz musician exchanges between the twin cities, develop and enhance their twin city project work to the maximum benefit of the local communities and attract a larger audience to the Festival. The organisers of the Festival are applying for funding towards two elements of twin city inclusion in the 2013 Festival.

The first element is applying for funding to help towards commissioning a new project of collaboration featuring the Aberdeen and Regensburg musicians. Regensburg will send a professional jazz band to Aberdeen, including the international player Walter Lang, in order to compose and rehearse new material with Aberdeen musicians. This new project will then be premiered at the Aberdeen Jazz Festival 2013. The joint Aberdeen and Regensburg band will also perform at the Regensburg Jazz Festival in 2013 on a reciprocal visit.

The second request for support contributes towards bringing a new dimension to the Aberdeen Jazz Festival by including the Scottish National Youth Jazz Orchestra workshop in Aberdeen. Music students who are currently at University from either Stavanger or Clermont-Ferrand will provide music tuition at this workshop, offering the opportunity for youth jazz musicians to meet with their twin city partners, learn from the partners, share techniques and gain insight to a different approach to jazz music and culture. By involving the younger people into the Aberdeen Jazz Festival, and adding an

international dimension from the twin cities will contribute towards sustaining the legacy of the long-standing jazz links Aberdeen has with Regensburg and Stavanger.

Aberdeen Jazz Festival has already proved that it is successful with the promotion of the Aberdeen jazz scene internationally as fans from both Regensburg and Stavanger alike flew to Aberdeen in order to attend to the Festival and Aberdeen musicians have been invited to their jazz events. In order for the Festival to maintain its continued success, it is important to maintain and develop new projects with the twin cities, to attract and promote the music and culture in Aberdeen and highlighting that the city continues to provide high quality entertainment for the local population and its visitors. The Festival will be enhanced from last year by providing development and educational opportunities for the youth audience in Aberdeen.

The application requests a maximum of £6,800 towards accommodation, transport and additional fess. The musicians have accepted a lower fee in order to reduce the total amount applied for. The twin cities will cover the cost of air travel for the musicians travelling to Aberdeen. A break-down of the costs has been given.

Element One Expenditure	
Flights	£1,400
Hotels	£1,260
Transport	£380
Subsistence	£480
Artist Fees	£3,600
Jazz Aberdeen	£450
Marketing	£450
Office & Promotion Expenses	£1,200
Total	£9,220
Element Two Expenditure	
Flights	£1,500
Accommodation	£600
Subsistence	£400
Fees	£1,000
Transport	£180
Office & Promotion Expenses	£900
Total	£4,580
Element One + Element Two	£13,800
Income	
Contribution from participants	£6,100
Other sources of income	£900
Total	£7,000
Expenditure-Income	£13,800-£7,000
Total Applying For:	£6,800

6. IMPACT

Aberdeen – the Smarter City

- We will promote Aberdeen as a great place to live, bring up a family, do business and visit.
- We will ensure that Union Street regains its position as the heart of the city and move cultural activity centre-stage through re-invigorated cultural leadership.

Smarter Living (Quality of Life)

- We aspire to be recognised as a City of Culture, a place of excellence for culture and arts by promoting Aberdeen as a cultural centre hosting high quality diverse cultural events for the whole community and beyond.

Smarter Economy (Competitiveness)

- We will work with partners to promote the city as a place to invest, live, work and export from.

Community Plan

- Work with other organisations, agencies and groups, including Visit Scotland and Visit Aberdeen, to encourage tourism and the provision of facilities for tourists.

Promoting and supporting international exchanges, involving various communities of interest, significantly contributes to Aberdeen City Council Vision as outlined above and to the actions of the Single Outcome Agreement. The outlined twinning projects also align with the cultural strategy vision as it allows for improved communication of cultural opportunities, helps with changing the perception of the city, to develop a stronger cultural identity and to have increased levels of effective partnership working.

Aberdeen's twin city partnership initiatives:

- Foster international understanding and friendship and an appreciation of cultural diversity
- Provide Aberdeen's citizens with an awareness of, and insight into, international issues and perspectives and their impact on Aberdeen and Scotland
- Promote the image and raise the profile of the city overseas; and

- Provide a forum for the exchange of knowledge, expertise, skills, ideas and best practice in any given field.

Opportunities for groups, communities and organisations to access twin city projects and exchanges and for possible sources of funding are advertised through the Council's website, press releases, school circulars where appropriate and the Aberdeen Council of Voluntary Organisations e-bulletin.

Human Rights, Equalities and Diversity: Aberdeen City Council has in place a range of statutory and discretionary plans, schemes and policies to promote equality. Officers endeavour to target groups and communities which have not previously had experience of international visits or exchanges, or have been under-represented in twin city activities. Applicants complete an equal opportunities monitoring form as part of the application process.

7. BACKGROUND PAPERS
None used

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE Enterprise, Planning and Infrastructure

DATE 6th November 2012

DIRECTOR Gordon McIntosh

TITLE OF REPORT Support for local employability training providers

REPORT NUMBER: EPI/12/239

1. PURPOSE OF REPORT

This report aims to highlight the needs and the potential benefits of supporting local employability training providers.

2. RECOMMENDATION(S)

1. Officers within Employability, Skills and Community Enterprise further investigate and consult on the optimum means of supporting local employability training providers.

2. Officers submit a further report to committee once further investigation and consultation has been completed.

3. FINANCIAL IMPLICATIONS

Further investigation and consultation into the development of a support mechanism for local employability training providers will have no additional financial resource implications on the service

4. OTHER IMPLICATIONS

If officers participate in this piece of work, it will mean that there is adequate and appropriate training provision available on a local level which may have a positive impact on the ability of local businesses to attract appropriate skills to support their needs, and a potential decrease in numbers of long term dis-engaged people within our communities.

5. BACKGROUND/MAIN ISSUES

With employability high on both the political and economic development agendas it is imperative that Aberdeen City Council ensures that we have robust employability support provision within the city that links

with local employer and client needs and can address all aspects of disengagement.

Current Situation

Currently there are a number of local specialist employability service providers that work in isolation across the city. Initial feedback from meetings with companies has identified that there are no clear communication channels between these companies, linkages or communication channels into and from the community planning framework are poor, which often excludes the evaluation of information obtained from working with local clients and businesses when strategic decisions are made. Initial discussions with local employability training providers also highlighted a willingness to work closer together, to address the ever changing local employability and engagement landscape and strengthen their position when competing against national providers. The need to strengthen local provision is further enforced with the introduction of Welfare reform. What they sought was an appropriate platform to bring their organisations together to aid meaningful exchange and development.

Increasingly, employability contracts are awarded on a national basis, which requires high level financial robustness and a track record of large scale delivery, which eliminates many local providers from the tendering process and also eliminates appropriate provision based on local needs and knowledge.

This system also encourages competition amongst local providers and this produces a barrier to partnership working and co-operative engagement within the employability providers community. This can lead to duplication of and gaps in service provision, and a general lack of awareness of the service provision landscape.

It must also be noted that more and more contracts are awarded on an outcome related basis, with payment not being made till many months after outcomes are achieved, which puts severe financial strain on the organisations.

These administrative issues all have an impact on the breadth, depth and sustainability of services and this has an ultimate negative impact on the number of disengaged individuals within the city. This is a particularly significant issue, since the current economic recession means that the level of disengagement within the youth population is already rising on both a local and national level.

Furthermore, local businesses across all sectors are having increasing difficulties in recruiting appropriate staff, which will negatively impact on service delivery, growth, sustainability and ultimately business location in Aberdeen.

Going Forward

Officers within the ESCE service will further consult with other external agencies, local employability training providers and members of Aberdeen Works to highlight the concerns raised by local businesses and training providers to identify potential solutions that could create a platform for local employability training providers to work closer together.

The formation of an appropriate platform should also ensure that a clear pipeline of employability support services is developed, which will engage with all sections of the population from schoolchildren through to those approaching retirement. This will ensure that local needs on an individual and business level are met, and that a diverse range of employability options are promoted.

The formation of any support platform should not duplicate the work of any existing forums or partnerships.

Potential Benefits

The potential benefits of establishing an appropriate platform for local employability training providers include:

- Sharing of information
- Gaps in provision can be identified and addressed
- Access to wider funding streams – explore funding opportunities and scope for collaborative bids
- Shared knowledge of local provision
- Improved pipeline progression
- Wider skills and knowledge base
- Facilitate increased partnership working
- Central communication point
- Development of cradle to grave provision
- Coordination of relationships
- Improved alignment to business needs
- Improved relationships with business community

Initial discussions have already taken place with a number of employability training providers to identify if there was a willingness to participate if an appropriate platform were to be established. (Aberdeen Foyer, Pathways, SHMU, Enterprise North East, Opito, Cornerstone, GREC) and positive responses were received from all the above organisations. If approval to initiate further investigation and consultation is given then other local employability training providers will also be consulted. Officers from Education Culture and Sport will also be included in this consultation.

Reporting process

All information gained during the consultation process will be shared with our external partners and Aberdeen Works, prior to appropriate proposals being submitted and a future report being submitted to Enterprise, Planning and Infrastructure committee.

6. IMPACT

The project contributes significantly and directly to the achievement of

Single Outcome agreement (SOA) National Outcomes:

1. We realise our full economic potential with more and better employment opportunities for our people
2. We are better educated, more skilled and more successful

Aberdeen the smarter City

1. We will enhance the physical and emotional wellbeing of all our citizens by offering support and activities which promote independence, resilience, confidence and self-esteem
2. Working with our third, public and private sector partners, we will provide opportunities for lifelong learning which will develop knowledge, skills and attributes of our citizens to enable them to meet the changing demands of the 21st century.
3. Again, working with partners, we will create a City of Learning which will empower individuals to fulfill their potential and contribute to the economic, social and cultural wellbeing of our communities.
4. We will aim to have a workforce across the city which has the skills and knowledge to sustain, grow and diversify the city economy.

Equality and Human Rights implications

A full equality and human rights impact assessment was not required, as this paper only seeks approval to investigate and further consult. A full EHRI will be undertaken once potential solutions are in place..

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	West Huxterstone, Kingswells Masterplan
REPORT NUMBER:	EPI/12/222

1 PURPOSE OF REPORT

1.1 This report outlines the West Huxterstone, Kingswells Masterplan, prepared as a guide for the future development of land identified as Opportunity Site OP42 in the Aberdeen Local Development Plan (2012) and report on the results of public consultation.

1.2 A summary document of the West Huxterstone, Kingswells Masterplan has been appended to this Committee Report. The West Huxterstone, Kingswells Masterplan for site OP42 (September 2012) can be viewed by accessing the following link:

www.aberdeencity.gov.uk/masterplanning

1.3 A hard copy of the Masterplan is available in the Member's Library, in the Planning and Sustainable Development service at Ground Floor North, Marischal College, or by contacting the Masterplanning, Design and Conservation team.

2 RECOMMENDATION(S)

2.1 It is recommended that the Committee:

(a) Adopt the West Huxterstone, Kingswells Masterplan as Supplementary Guidance subject to endorsement by the Scottish Government.

2.2 Definition

'Supplementary Guidance' (SG) – this is adopted and issued by a planning authority in connection with a Local Development Plan, as a result any such guidance will form part of the Development Plan. Before adoption, the SG must be publicised and a period for representations to be made specified, this includes the targeting of key consultees and stakeholders who may want the opportunity to comment. Following the specified consultation period and as a result of comments received, relevant changes will be made to the final

document before reporting back to committee and subsequent submission to Scottish Ministers. After 28 days have elapsed, the authority may then adopt the guidance unless Scottish Ministers have directed otherwise.

3 FINANCIAL IMPLICATIONS

- 3.1 There are no direct financial implications arising from this report outwith normal officer time to evaluate the Masterplan. The developers have met the cost of preparation of the Masterplan including all consultation and engagement. The proposals will result in efficiencies in the determination of future planning applications related to the Masterplan, leading to a reduction in Council staff time to assess future detailed proposals.

4 OTHER IMPLICATIONS

- 4.1 There are no known property, legal or equipment implications arising from this report.
- 4.2 The Masterplan reduces the risk of piecemeal and inappropriate development within the site, by setting out the policies, opportunities and constraints up front.
- 4.3 Approving the Development Framework and Masterplan will contribute to efficiencies in determining future planning applications. By encouraging acceptable forms and uses of new development to be agreed publicly, it allows more informed decision making to be made earlier, saving time and resources for staff, applicants and the public.
- 4.4 In accordance with the Aberdeen Local Development Plan, Appendix 4, Action Programme and the Infrastructure and Developer Contributions Manual, the Masterplan references how the development will address requirements for cumulative and local transport infrastructure; public transport along with walking and cycling.
- 4.5 A Strategic Environmental Assessment has been undertaken on the West Huxterstone, Kingswells site as part of the preparation for the Aberdeen Local Development Plan.

5 BACKGROUND/MAIN ISSUES

- 5.1 The Masterplan has been produced by Ryden on behalf of Stewart Milne Homes and Graham Homes and aims to produce a high quality and deliverable village expansion to the south east of Kingswells. A multi disciplinary project team has contributed to the preparation of the masterplan, including Ryden (planning), Nicoll Russell Studios (architects) and Fairhursts (transportation).

Site Description

- 5.2 The site is located to the north of the A944, an arterial route into the City from the west. The site is generally low lying and rises from south to north. The site is bounded to the east by green belt, to the west by Fairley Road, the Den Burn to the south and the Lang Stracht to the north.

Policy

- 5.3 The West Huxterstone, Kingswells Masterplan is intended to act as interim planning advice, pending the implementation of the process to ratify the Masterplan as Supplementary Guidance by the Scottish Government.
- 5.4 The Masterplan has been prepared following the Council's adopted Aberdeen Masterplanning Process and contributes to the Scottish Government and Aberdeen Local Development Plan policy aims of raising design quality in new development, through the 6 key qualities of creating successful places, namely – distinctive, welcoming, safe and pleasant, adaptable, easy to get to and move around, and resource efficient.
- 5.5 The site is allocated for 120 houses in the Aberdeen Local Development Plan (2012). The development is proposed over the period 2007-2016. A contribution of 25% affordable housing is also required within this site.
- 5.6 An assessment of the environmental impact of the Masterplan was made alongside the Aberdeen Local Development Plan. Since the adoption of the Local Development Plan there has been additional consultation undertaken, additional detail added and amendments made. To ensure that this is properly documented a Screening Report has been prepared and submitted to the SEA Gateway and the Consultation Authorities. All the Consultation Authorities have agreed that the Masterplan is unlikely to have significant environmental effects. If the recommendations are agreed a formal determination will be issued and advertised.

Design Principles

- 5.7 The Masterplan sets out a vision for the site as being an expansion to Kingswells. West Huxterstone, Kingswells will be a place in which the houses, streets and outdoor spaces all combine to reflect the high standards and aspirations of not only the developers but also the Council and the community of Kingswells.
- 5.8 The Masterplan provides a spatial framework, discusses landscape, connectivity and transport proposals and sets out a phasing scheme, all summarised in the appendix to this report.

5.9 The approach to development at West Huxterstone, Kingswells is based on:

- Creating a unique environment which complements the existing character of the local area;
- Integration with the existing neighbourhoods of Kingswells which are linked by a strategic landscape network;
- Avoid skyline development by ensuring that buildings fit within a detailed landscape framework;
- Safeguard and enhance the ecological habitat of the Den Burn; and
- Connect the site in terms of pedestrian, cycle and vehicular traffic with new pathways between the housing and surrounding recreational areas.

5.10 The location and topography of the site play an important role in the form, design and character of the development. The site is split into four character areas:

- Rural Lang Stracht;
- Central;
- The Denburn; and
- Fairley Road.

These are described in the appended summary document.

5.11 Housing –25% affordable housing will be provided in accordance with the Aberdeen Local Development Plan. Affordable housing will be integrated with private development areas and will reflect housing need. The proposed mix of housing is described in the summary.

Infrastructure

5.12 Walking and Cycling – The Masterplan acknowledges that an east/west link connecting the employment area to Kingswells and beyond to Northfield as well as links and extensions to the Westhill/ Aberdeen cycle route will be provided.

5.13 Public Transport – There may be a need for extensions to existing services along with opportunities to integrate the Park and Ride services.

5.14 Roads – There is a requirement for a new major junction from OP40 West Hatton and Home Farm access to A944 and new road access within junction at Kingswells perimeter road. There is also a need to upgrade the junctions at A944/ B9119 on approach to Westhill and A944 Kingswells roundabout. The developer is in discussion with ACC and appropriate contributions will be agreed through the Transport Assessment process.

- 5.15 Health – the Masterplan acknowledges that there is a requirement for financial contributions to provide one additional GP and two additional dental chairs to serve both the Kingswells and Maidencraig developments. This will be agreed through planning gain and a Section 75 legal agreement.
- 5.16 Education – Developers are currently in discussion with ACC regarding capacity at Kingswells Primary School. Appropriate contributions will be agreed at the planning application stage.

Public Consultation

- 5.17 Throughout the process the project team have worked with the local authority, statutory consultees and the local community.
- 5.18 Design for the site has evolved with continued input from the community and key stakeholders. Input from the community has informed the proposals. Two community events took place during the Masterplanning Process on 5 March 2012 and 22 May 2012. Please see Appendix 1 for a detailed summary of the issues raised and addressed at this stage.
- 5.19 The public consultation arising from the Committee's decision at their meeting on 11 September 2012 ran from 17 September to 15 October 2012. The following statutory consultees were included
- Kingswells Community Council;
 - Aberdeen City and Shire Strategic Planning Authority;
 - Aberdeenshire Council Planning Authority;
 - Forestry Commission Scotland;
 - Aberdeen City and Shire Economic Future (ACSEF);
 - Scottish Water;
 - Scottish Environment Protection Agency (SEPA);
 - Scottish Natural Heritage (Grampian);
 - Historic Scotland;
 - Scottish Enterprise (Grampian);
 - Transport Scotland;
 - NHS Grampian;
 - NESTRANS; and
 - Aberdeen City and Shire's Planning Gain team.
- 5.20 Consultation responses were received from Historic Scotland, SEPA, Aberdeen City and Shire's Planning Gain team, Kingswells Community Council (KCC), two individuals who are members of the Kingswells community and are also members of the Kingswells Primary School Parent Council and seven individuals from the Kingswells community.
- 5.21 The main issues arising from consultation include:

- **Education**

Concerns were raised by KCC, members of the Kingswells Primary School Parent Council and members of the Kingswells community regarding provision of primary education and it has been asked to relook the proposed phasing. The current Aberdeen City Council consultation on the School Estates Strategy will inform the exact education provision in the Kingswells area and developer contributions to education will be determined once the conclusions to the consultation have been formed.

The ratio used to determine an estimate of pupils per household was based on a house buyer's survey. The ratio will be looked at again in due course once the Council's research team has received the 2011 Census data.

- **Access and transport**

KCC and one individual object to any vehicular access to and from the Lang Stracht and think that two accesses into the development should be from Fairley Road. It is stated that the Lang Stracht will be a designated bus lane soon and they wish it to remain a free flowing bus route, an access to local residents only and kept relatively traffic free as it is a popular route for walkers, cyclists and horse riders. They also have concerns regarding visibility approaching the proposed access into the proposed development, the width and condition of the Lang Stracht and development detracting from the current rural setting.

It is acceptable to take access from the Lang Stracht. Visibility and junctions spacing requirements, along with land ownership issues have all meant that it has not been possible to gain two accesses from Fairley Road. The access onto the Lang Stracht and Fairley Road will be designed to comply with Aberdeen City Council's roads standards and upgrades to that road made where necessary. The access to and from the Lang Stracht is secondary with right in and left out turn only to prevent vehicles turning right onto the Lang Stracht. The majority of the traffic into the site is expected to be from Fairley Road.

It is therefore not anticipated that there will be a significant increase in traffic along the Lang Stracht. It is understood that the bus lane access will apply only to the A944 end of the Lang Stracht and have no impact on existing local access arrangements. The access proposed from the Lang Stracht will not affect the flow of buses. The Lang Stracht will remain as bus/emergency vehicle/cycle access only.

Some comments were received that highlighted that the roads around Kingswells are already very busy. It is stated that the additional 120 homes will have an impact on traffic and will have a detrimental effect on Kingswells. Fairhurst have been appointed as Transport Consultants and an initial assessment of transport in the area

demonstrates that the predicted peak period traffic movements from the development can be accommodated within available traffic capacity at local junctions and will have an imperceptible impact on the A944 junction roundabout. Development will be supported by the infrastructure required to sustain the development which will be identified through the Transport Assessment.

- **Internal road layout**

KCC object to the internal road layout because it reflects land ownership and there are concerns regarding piecemeal development. There are also concerns that the dead end road within the Stewart Milne land will lead to future development to the adjoining fields to the east. The roads standards and visibility requirements have dictated the location of the access roads. The layout of streets is informed by Designing Streets, a policy statement in Scotland for street design as well as the Kingswells Development Framework and the need to protect the GSN to the south. It marks a change in the emphasis of street design towards place-making. Some curves may be considered appropriate, however, Designing Streets states that layouts that use excessive or gratuitous curves should be avoided, as they are less efficient, reduce legibility and make access for pedestrians and cyclists less direct. The masterplan road layout proposed contains some curves as a result of the consultation exercise and it is considered that it creates a balance between the KCC and planning guidance. The masterplan does not consider the development of the land to the east of West Huxterstone and the suitability of this site is not an issue to be taken into consideration in the masterplan.

- **Green Space Network and the Denburn**

KCC has stated that the Green Space Network (GSN) must be protected and enhanced and that SUDS ponds should not be unattractive and lifeless dry basins. The GSN will be enhanced as well as protected, with additional landscaping provided as appropriate. A Landscape Consultant forms part of the design team who are developing proposals for the site. Footpaths suitable for the size of the site will be incorporated. The comment regarding the SUDS ponds is noted and engineers are looking at a solution.

- **Environment and Flooding**

SEPA have asked that the following issues are addressed prior to the submission of a planning application: foul and surface water drainage, pollution prevention and environmental management and protection of the water environment. Contact will be made with those responsible in due course during the planning application stage. The site will also be assessed for flood risk from all water sources as suggested by SEPA.

Another response highlighted that the south of the site is prone to flooding. GSN and the development of SUDS will help buffer the development from the burn.

- **The Lang Stracht as a street**

KCC and one individual object to the concept of making the Lang Stracht a street. Development on the Lang Stracht would be excluded from the rest of the development and would be out of keeping with the existing houses. There are also concerns expressed that this kind of development would promote future development across the Lang Stracht. In response, the requirement to make the Lang Stracht a Street arose from the Development Framework which requires that houses face onto the Lang Stracht. The masterplan is bound by the requirements of this document. The existing houses along the Lang Stracht currently face onto open fields and it is considered that this would be an attractive outlook for new residents of those properties. The masterplan does not consider the development of the land opposite and the suitability of this site is not an issue to be taken into consideration in the masterplan.

KCC also state that if the Lang Stracht were to be made a street it would need a footpath to extend to Fairley Road and this may cause some land ownership issues. The masterplan states that new pavements will be provided to connect to the existing network of paths and core paths.

- **House types**

KCC and one individual are concerned that more West One house types, starter homes, are proposed for the West Huxterstone site. In response, the development of West Huxterstone will contain a variety of house types, not just West One sized properties. This is to ensure a choice of house types across the development. House types will range from one bedroom up to five bedroom properties, thereby providing a range of dwellings to meet a variety of needs. The West One sized properties will make up the affordable element of the site, which is a requirement of the Local Development Plan.

With regards to KCC's disappointment that there are no plans for bungalows for elderly or disabled people it must be reiterated that the exact mix and type has yet to be determined and will be addressed with the detailed planning applications that will be received in due course.

- **Affordable housing**

KCC has stated that affordable housing appears to be concentrated in one area of the masterplan site and assume that they will be in the form of the blocks of eight homes used in the West One development. The masterplan states that it is anticipated that these will be provided

on site and integrated throughout the development. The type, style and exact location will be discussed with Aberdeen City Council during the planning application process.

- **Size of development**

KCC has raised concerns on the final number of units that will be developed on the site. The site is identified for 120 units and the layout is based on this number. An adjoining landowner has demonstrated interest in developing their land that lies within the boundary for OP42. At the moment there is no finalised proposal and indication of unit numbers. Exact numbers will be determined at the detailed planning application stage however over development of the site will be unacceptable.

- **Broadband and lack of facilities**

One individual raised concerns regarding slow broadband and the lack of food shopping facilities in the area. With regards to broadband, this is noted and will be investigated as part of the planning application process but is, strictly speaking, not a material planning consideration. Regarding shopping facilities, a residential development of this size would not generate the demand for more convenience or comparison shopping facilities therefore there is no need for this development to contribute to shopping facilities. The development is in close proximity to the existing neighbourhood centre which provides local shopping facilities.

Another respondent raised concerns regarding the GP facilities and question how they will cope with more families in the area. It has been identified that there is a requirement to extend Kingswells Health Centre to give space for one additional GP.

- **Consultation process**

KCC are unhappy with the consultation process because they interpreted the EP&I Committee decision from Tuesday 11 September 2012 differently from officers. KCC are also unhappy with the lack of confirmation that there was a public consultation, the duration of consultation and how the consultation was advertised on the Council's web pages.

Officers took from the Committee decision that they had been instructed to go out for public consultation and report back to the next EP&I Committee with the responses they received from the consultation.

Following this Committee decision, consultation papers were issued on 17 September. Kingswells Community Council was issued a letter, stating that the consultation had begun and detailing where the

document could be found on the public pages of our website, as part of this process.

Regarding the duration of the consultation a decision was made to consult for four weeks. There is nowhere in legislation that stipulates how long consultation should be for masterplanning documents therefore officers have to make a decision on what is appropriate. After discussion internally there was a decision that a four week consultation would be sufficient and this would also allow us to report to the next EP&I Committee.

Masterplanning never use the 'Current Consultations' page to advertise consultation and have always used the 'Masterplanning Public Consultation' page. This is something that we will raise with our IT department to see if a link from the current consultation page to our masterplanning page can be established to avoid any future confusion.

Officers received a number of responses from members of the public.

IMPACT

- 5.22 The Masterplan promotes access to walking and cycling networks.
- 5.23 The Masterplan contributes to the following Single Outcome Priorities: 1 – We live in Scotland that is the most attractive place for doing business in Europe; 2. We realise our full economic potential with more and better employment opportunities for our people; 10. We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.
- 5.24 The proposal contributes to “Aberdeen – A Smarter City” in terms of providing a clean, safe and attractive streetscape and promote bio-diversity and nature conservation, encouraging wider access to green space in our streets, parks and countryside, improving access to affordable housing in the private sectors by working with developers to maximize the effective use of developer contributions.
- 5.25 The proposal is consistent with the Council's Corporate Plan in particular delivering high levels of design from all development, maintaining an up-to-date planning framework, sustainable development and open space provision.
- 5.26 The proposal is consistent with the Planning and Sustainable Development Service Plan, in particular engaging the community in the planning process, and the delivery of Masterplans/Development Frameworks in line with the Aberdeen Masterplanning Process.

- 5.27 An Equalities and Human Rights Impact Assessment has been prepared. In summary, many of the aims of the proposal will have positive benefits, including;

Other (Housing) – Greater provision of affordable and family housing in Aberdeen as a result of development.

Other (Health) – Improved access useable open spaces for leisure pursuits and opportunities for walking/cycling/exercise.

6 BACKGROUND PAPERS

- 6.1 The West Huxterstone, Kingswells (August, 2012 issue) is a large document containing a lot of illustrative material and can be viewed by accessing the following link:

http://thezone/PI/SL_stratlead_reports.asp

- 6.2 Aberdeen Local Development Plan 2012

<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=42278&siD=9484>

- 6.3 Aberdeen Masterplanning Process 2012

<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31782&siD=14394>

7 REPORT AUTHOR DETAILS

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Appendix 1

Over the two consultation events 25 responses were received. Some of the comments highlighted support for the open space, new school, quality of place and design. Full details of the consultation are summarised on page 9 and detailed in Appendix 1 and Appendix 2 of the Masterplan. Comments raising opposition are summarised below with associated outcomes:

Comment	Response
School and Facilities	
Concerned about the school and numbers of pupils. Already a large waiting list for the nursery. Will there be provision made for a new school primary or secondary. Also concerned about the impact on other village facilities.	The Local Development Plan indicates that contributions to the health centre are required and the exact requirements will be discussed with Aberdeen City Council.
Concerned about the impact on public transport: perhaps bus service could be improved	Any improvements to the bus service would be identified though the Transport Assessment which is currently being undertaken. If improvements to the bus service are required, this will be discussed with Aberdeen City Council through the planning application process.
If SMG were to take the extra 22 houses which were allocated to West One (72 instead of 50) off the number of proposed houses for huxterstone, then the development numbers for Kingswells would be correct	The West One site was initially identified as a mainstream housing site with a range of house types and sizes. Due to the properties built on the site being all affordable housing, there was scope for a higher number of smaller houses. It is for this reason that the number of houses increased.
The development must integrate with the existing community and children from the development must go to school in Kingswells	Noted. Children from the development will go to Kingswells Primary. Given that the settlement doesn't have a secondary school, such pupils will have to go to a school outside of Kingswells. However, this is the current situation with secondary school age children in the village. The development will be fully integrated with the existing community and linkages provided to and from the site to allow this.
Roads	
We are concerned about the increase in traffic.	Fairhurst have been appointed as Transport Consultants and an initial assessment of transport in the area demonstrates that the predicted peak period traffic movements from the development can be accommodated within available traffic capacity at local junctions and will have an imperceptible impact on the A944 junction roundabout. Development will be supported by the infrastructure required to sustain the development which will be identified through the Transport Assessment.
Concern re: speed limit: can this be reduced to 20mph especially concerned about safety of children.	The speed limit in the surrounding streets is controlled by the Council and would be an issue for them to consider. This will be

	drawn to their attention.
There is no site plan to show the detailed layout of the planned buildings. No real opinion could be formed on traffic flow and impact.	The material on display at the exhibition was a "work in progress" and the layout is constantly evolving. An indicative road layout was identified and housing would be accessed from this and the open space. The masterplan sets the parameters for the preparation of a layout to accompany a planning application. As such, the layout(s) for the site will be available for comment at the planning application stage, once amendments have been made in response to the consultation events.
Concerned with the entrance onto the Lang Stracht and traffic on Fairley Road	The access onto the Lang Stracht and Fairley Road will be designed to comply with Aberdeen City Council's roads standards and upgrades to that road made where necessary. The Lang Stracht will be a secondary access with right in and left out turn only to prevent vehicles turning right onto the Lang Stracht. The principal point of access will be from Fairley Road.
Certain roads/roundabout issues to be addressed.	The roads/roundabouts to be addressed will be determined through the Transport Assessment and any mitigation measures required will be provided.
No further development until the lights at the roundabout is working from 7am to 7pm at the very least.	The Transport Assessment has not indicated that this is an issue to be mitigated. The timing of the lights would therefore be an issue for Aberdeen City Council to address.
Very concerned about level of houses and traffic issues ie access to and from Old Skene Road.	There is no direct access to the site from Old Skene Road. It is not anticipated that the development will increase traffic on Old Skene Road as traffic to the development will turn off at Fairley Road. The junction with the bypass will be assessed as part of the Transport Assessment and any mitigation measures required will be provided as part of the development.
Infrastructure needed first before development of housing.	Infrastructure required for the site will be provided in conjunction with the development.
No road network infrastructure. From discussions, the development will go ahead without improving the roads.	The Transport Assessment will identify any improvements to be made in the surrounding area and they will be provided as part of the development of the site.
No investing in reopening the old Lang Stracht road due to limited funding. Temporarily this is key for traffic flow.	Some investment is being made in the Lang Stracht, however, this will not include re-opening it to general traffic. There will be a secondary access to the site from the Lang Stracht and any improvements along this stretch provided as part of the development.
Access to development along Fairley Road, cars already park along this road thus further road congestion.	Due to the size of the development proposed, two points of access are required, with one being from Fairley Road. This will ensure adequate visibility is

	provided. It is not anticipated that this will increase the amount of cars parking along this road.
Presumably access to utilities on Fairley Road will be required. Can this road be resurfaced properly on completion of the work rather than "patched up"	This is noted and will be taken into consideration.
It would be good if the City would reopen the Lang Stracht Road – it should not be buses only	The re-opening of the Lang Stracht would be the decision of Aberdeen City Council Roads Department.
Concern that Lang Stracht might be re-opened to general traffic – this will impede buses (already slow) and encourage rat-running through Kingswells. This section of the Lang Stracht is now often used by walkers, joggers and cyclists.	The decision to re-open the Lang Stracht is a decision for Aberdeen City Council roads department. The re-opening of the road to general traffic would not be required for this development, although the current access arrangement from the Lang Stracht would be required.
Housing Numbers	
Concerned about actual number of houses to be built (will 120 become 140 due to a previous Stewart Milne development in Kingswells).	The site is identified for 120 units and the layout will be based on this number. The previous proposal was identified in the Local Plan as a mainstream housing site, which would have contained a range of dwelling types and sizes. However, it was altered to an affordable housing site which resulted in scope for a higher number of smaller units.
Development to the east	
Would also be against future development of any further field to the east On the eastern boundary of the site, no features should be included which will enable the building of a future access road into fields to the east of the current site.	There is no future development identified on land to the east within this masterplan. Any further development would require to go through the Local Development Plan process in the future and is therefore not guaranteed.
The eastern boundary should have a more solid line of trees than shown on the strategic landscaping extract on the exhibition boards.	This will be considered and landscaping provided as appropriate.
Masterplanning Process	
Much of the development is set in stone	The development is not set in stone, however, there has to be flexibility and understanding on both sides. Some of the requirements for the site have been set by the Kingswells Development Framework, and the developer has to work within these requirements.
The developer is not accurately reporting discussions with ACC	As far as the developers are aware, they have been honest regarding discussions with Aberdeen City Council. It is not the intention to provide misinformation to the Community Council.
The developer appears to be considering areas of development not included in the masterplan area	The masterplan does not include any land outwith the boundaries of the OP42 designation in the Local Development Plan. Stewart Milne Homes have always been honest about the potential for future development to the east, but have made it clear that this would have to be promoted

	through the next Local Development Plan and there are no guarantees that it would be identified for development by Aberdeen City Council.
Conflicting answers given to the same question eg. House types	The exact house types have yet to be decided. It was highlighted at the public exhibition that a range will be provided on the site. No further details are known at this time and the exact mix will be determined through the planning application process.
Ryden have provided no plans or maps to discuss prior to public consultations	The Community Council were invited to a councillor briefing before both of the public consultation events, to view the plans before the public. Up until this event, there were no plans to provide as they were being worked up right until the last minute. The Community Council were given links to the website so that plans could be viewed after the events.
Access	
<p>KCC has been informed that road access and layout are only at the very early stages and no stipulations have been set by ACC. KCC want two accesses to be from Fairley Road.</p> <p>KCC strongly object to vehicular access from the Lang Stracht as the Lang Stracht will be a designated 'bus lane'; visibility approaching the proposed access is poor; the Lang Stracht is unsuitable for traffic due to its narrow width and poor surface condition</p>	<p>ACC Roads have agreed that it is acceptable to take access from the Lang Stracht. Visibility and junction spacing requirements, along with land ownership issues have all meant that it has not been possible to gain two accesses from Fairley Road. The access to and from the Lang Stracht is secondary and the majority of the traffic into the site is expected to be from Fairley Road. It is therefore not anticipated that there will be a significant increase in traffic along the Lang Stracht.</p> <p>It is understood that the bus lane access will apply only to the A944 end of the Lang Stracht and have no impact on existing local access arrangements. The access proposed from the Lang Stracht will not affect the flow of buses.</p>
If access is provided along the Lang Stracht and a street is formed, this will have a huge impact on local residents and the way they use their homes. Would the owners be asked to remove their fence?	<p>The masterplan does not propose any changes to the existing properties and homeowners will not be asked to remove their fences.</p> <p>The requirement to have houses facing the Lang Stracht and therefore the formation of a street is a requirement of the Kingswells Development Framework that ACC prepared.</p>
Access via north of main roundabout is not good.	Roads issues are being fully investigated by the project team
Internal Road	
KCC strongly objects to the internal road layout. The "dead end" road leads to future development in the adjoining field and this is not acceptable. The obvious layout is a main loop with some minor roads crossing the top. The exact layout is difficult to determine if no information is provided on the house types.	<p>The northern access proposed by KCC is too close to the existing farm building. Visibility splays cannot be achieved and there is a requirement to be 60 metres south of this point. To provide another access south of that would not achieve the required visibility.</p> <p>The layout also does not meet guidance</p>

	<p>contained in Designing Streets which requires the avoidance of “layouts that use excessive or gratuitous curves” as they are less efficient, reduce legibility and make access for pedestrians and cyclists less direct. The masterplan road layout proposed contains some curves as a result of the consultation exercise and it is considered that it creates a balance between the KCC and planning guidance. The layout as shown by KCC also encroaches into the GSN which would be unacceptable and sits too close to existing development to the north east and north west. Again, this would not be acceptable to the Council.</p>
<p>The layout proposed appears to reflect the ownership of the two parties</p>	<p>The roads standards and visibility requirements have dictated the location of the access roads. The layout also meets the requirements of Designing Streets Policy, the Kingswells Development Framework and the need to protect the GSN to the south. The topography of the site has also been taken into consideration, as has the curves requested by KCC.</p>
<p>Forming Streets</p>	
<p>KCC strongly objects to the concept of making the Lang Stracht a Street. The houses facing the Lang Stracht would be excluded from the rest of the development and would be out of keeping with the existing houses.</p> <p>Proposed houses on the Lang Stracht would face onto open fields. The area opposite the development site was proposed at the Main Issues stage of the LDP and was deemed as unsuitable.</p>	<p>The requirement to make the Lang Stracht a Street arose from the Development Framework which requires that houses face onto the Lang Stracht. The masterplan is bound by the requirements of this document. The existing houses along the Lang Stracht currently face onto open fields and it is considered that this would be an attractive outlook for new residents of those properties.</p> <p>The masterplan does not consider the development of the land opposite and the suitability of this site is not an issue to be taken into consideration in the masterplan.</p>
<p>The layout of the streets should avoid straight rows of housing, and should include a mix of house types and sizes in each street. Curves are more interesting than straight lines and they should be used where possible.</p>	<p>The layout of streets is informed by Designing Streets, a policy statement in Scotland for street design. It marks a change in the emphasis of street design towards place-making. Some curves may be considered appropriate, however, Designing Streets states that layouts that use excessive or gratuitous curves should be avoided, as they are less efficient, reduce legibility and make access for pedestrians and cyclists less direct. A mix of house types and sizes in each street will be given consideration during the design and layout. The indicative road layout in the Masterplan introduces curves in the layout.</p>
<p>House Types</p>	
<p>KCC has concerns about the proposal for</p>	<p>The decision to buy a particular style of</p>

more West One house types.	home in a particular location is decision of the homeowner. Stewart Milne Homes are providing first time buyers the opportunity to purchase a property on two levels at a competitive price. Each purchaser is able to look at the show home before making their decision and is therefore made aware of the size and storage opportunities within the property. The development of Huxterstone will contain a variety of house types, not just West One sized properties. This is to ensure a choice of house types across the development. The West One sized properties will make up the affordable element of the site, which is a requirement of the Local Development Plan.
KCC is disappointed that there is no plan for bungalows for elderly or disabled people.	The exact mix and type has yet to be determined.
I am concerned that the proposed development might interfere with the amenity of Manse which is located on Lang Stracht to the NE corner of the site. Any development immediately adjacent to Manse should preferably be low rise – 1 storey.	The existing dwellings located within the boundary of the masterplan site have been considered in the preparation of the masterplan. Strategic landscaping will be located adjacent the Manse and other properties, to ensure their amenity is protected.
House styles should be in keeping with the area, with no gaudy colours. House heights should be kept low – one and a half storeys would be best, but with some housing suitable for the elderly ie. Bungalows with easy access and no stairs	This is noted. It can be confirmed that a range of house types and styles will be provided on the site. There are a range of house types in the vicinity, with the majority comprising two storeys. Similarly, the majority of houses on this site will be two storey, however, consideration will be given to one and a half storey properties. It is not anticipated that there will be any bungalows on the site. The mix of properties is given careful consideration at the outset and the mix based on demand.
The pitch of roofs should be similar to others in Kingswells (ie no flat roofs) with a mix of colours for roofing materials.	None of the properties will have flat roofs and a range of roof colours will be taken into consideration. Details will be discussed as part of any detailed planning application moving forward.
GSN/SUDs	
SUDs ponds should not be unattractive and lifeless dry basins	This is noted and engineers are looking at a solution
The GSN area should be enhanced as well as protected, with no excessively wide paths included.	The GSN will be enhanced as well as protected, with additional landscaping provided as appropriate. A Landscape Consultant forms part of the design team who are developing proposals for the site. Footpaths suitable for the size of the site will be incorporated.
Developer Contributions	
Developer contributions: pavement required on Fairley Road (right hand side) onto Kingswood Drive for residents to access village centre should be provided as part of developer contributions for the	The exact details of developer contributions will be discussed with Aberdeen City Council's Planning Gain Co-ordinator.

site; stonework to the village signed as per KCC plans could also be provided as part of developer contributions.	
Sustainable Energy	
Discussed suitability of the site for PV solar on roofs	The provision of PV solar on roofs would be a decision for the individual household to make. The developers will incorporate other sustainable principles in their proposals.
Houses should be well insulated and the south-facing aspect of the site should be exploited as far as possible by incorporating solar panels.	The houses will exceed current guidelines on insulation and the south facing aspect will be exploited as far as possible in their orientation. Implementation of carbon reducing technologies will be discussed with the Council at the appropriate time. The construction industry widely promote the "fabric first" approach which in the first instance looks at how the construction of the product creates less need for energy consumption before adding energy generating technologies. This primarily focuses on increased levels of insulation, air tightness and building orientation to maximise solar gain.
Other	
Kingswells is currently very poorly served by internet connection. What considerations/arrangement has been made in this development to ensure that the additional load on the already inadequate telephone internet service is not further compromised.	This is noted and will be investigated as part of the planning application process.
Need some play area as already no play areas at this end of Kingswells.	The layout of the site will include areas of open space in line with Aberdeen City Council's open space requirements. The type of open space to be provided will be in line with the LDP requirements.
Housing market falling due to saturation in demand, shops, schools etc improvements	The housing numbers are based on the housing requirements identified in the Aberdeen and Aberdeenshire Structure Plan. Aberdeen City Council are required to find appropriate locations for these housing numbers through the Local Development Plan and it is through this process that the site at Huxterstone has been identified.
It sounds like the old story of profiteers gaining while the local community has to suffer. A change of ethical standards would be greatly appreciated.	There are many positive points arising as a result of this development. The developers have appointed external consultants to investigate transport and landscaping matters in order to develop an attractive residential environment, with access to an improved green space network. Mitigation measures will be provided to address any adverse impacts arising.
The owners of Morven have a heritable and irredeemable servitude right from a sewer pipe going through the ground which we understand is for the proposed development. We understand that the pipe	This will be taken into consideration and fully investigated.

connects to the main sewer at a point on Fairley Road.	
Have a concern in connection with the proposed development regarding the mains water pressure in their property and they believe, generally in the Kingswells area.	Mains water pressure will be fully considered during drainage investigations for the site. It is not anticipated that the development of 120 homes will have a significant impact on this.
All homes should have gardens with adequate space for homeowners to enjoy their property and the views available from the site.	Where appropriate, homes will have gardens in order to enjoy the property. In terms of a view, it should be understood that, due to the layout and orientation of individual properties, not every property will have the same view. However, the orientation of properties will be carefully considered and will be maximised to take advantage of available views.
Adequate litter bins, dog bins and grit bins need to be included	This will be considered through planning gain negotiations.
There should be adequate car parking spaces for visitors	Parking will be provided in accordance with Aberdeen City Council's parking guidelines.
Written assurances are needed that a Residents Association will be set up.	Details of this will be agreed in due course with Aberdeen City Council.
KCC want to retain the drystone dyke, with a break to provide access for footpath/cycles, but not vehicles. Removal of the dyke destroys the rural character. Any stones from dykes within the development should be reused within the development to provide a sense of place, and rural character of the area.	Stone dykes will be retained as far as is practical. Access to vehicles is required from the Lang Stracht. Any stones from dykes within the development will be reused where appropriate.
Concerned that access/amenity should not be interfered with during building work etc	Unfortunately there may be some disruption during building work, but this will be controlled and minimised as much as possible.

Appendix 2 – Consultation Responses

Please see separate document.

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Summary Document

OP42: West Huxterstone Masterplan

Planning and Sustainable Development
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Marischal College
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August 2012

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Introduction to Masterplan

This document has been produced to provide a summary of the Huxterstone Masterplan document. It aims to highlight the key information and guidance contained in the document, however, for a more comprehensive understanding of the site, the process and the aims and outcomes of the Masterplan the main document is the main point of reference.

The site, identified in the Aberdeen Local Development Plan, lies approximately 4 miles to the west of Aberdeen City on the A944, an arterial route into the City. The land is in various ownerships, and extends to approximately 6 hectares (15 acres). It lies to the east and north of the old village of Kingswells which centred around Old Skene Road.

The Masterplan follows on from the Kingswells Development Framework, which was prepared by the Council in 2008. It explains how a residential development of around 120 units can be developed as an extension to Kingswells.

Figure 1: Kingswells Location Plan



It is intended that the document will form the basis of Supplementary Guidance (SG) to the Aberdeen Local Development Plan.

The design team has worked closely together to provide a well thought out response to the site. The Masterplan has been informed by a series of reports and technical studies. It has also benefited from community consultation and regular meetings with Aberdeen City Council.

The Masterplan document is set out in eleven sections, including:

1. Introduction
2. Context
3. Communication and Engagement
4. Identity
5. Transport and Accessibility
6. Planning and Design Principles
7. Infrastructure
8. Developer Contributions
9. Phasing and Delivery
10. Development Process
11. Further Information

This document gives a brief description of the key information.

Figure 2: Local Development Plan extract



Context

The Masterplan is a step towards the delivery of approximately 120 new homes in the village. Roads, drainage infrastructure and open space will also be provided to create a high quality residential environment.

The Masterplan aims to achieve the following:

- A high quality village extension that will create a unique environment that complements the existing character of the local area.
- Integration of the site with the existing neighbourhoods, which are all linked by a successful strategic landscape framework.
- The avoidance of skyline development by ensuring that buildings fit within a detailed landscape framework.

- The safeguarding and enhancement of the ecological habitat of the Den Burn.
- The connectivity of the site in terms of pedestrian and vehicular traffic with new pathways connecting the housing with recreational areas along the burn.

The masterplan considers the surrounding context, site features, existing landscape context, existing open space and the visibility of the site in determining the most appropriate means of developing the site for residential purposes.

There are a range of house types and styles in the area and there are no landscape features of any significant importance within the site. Stone dykes separate the fields and there are dykes to the north and east. Stone walls will be retained where appropriate and stone will be reused to build new stone walls throughout the development.

Figure 3: Topography

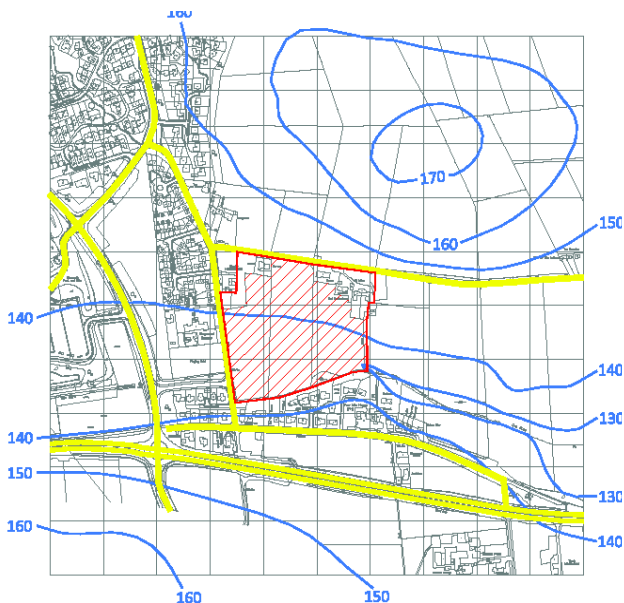
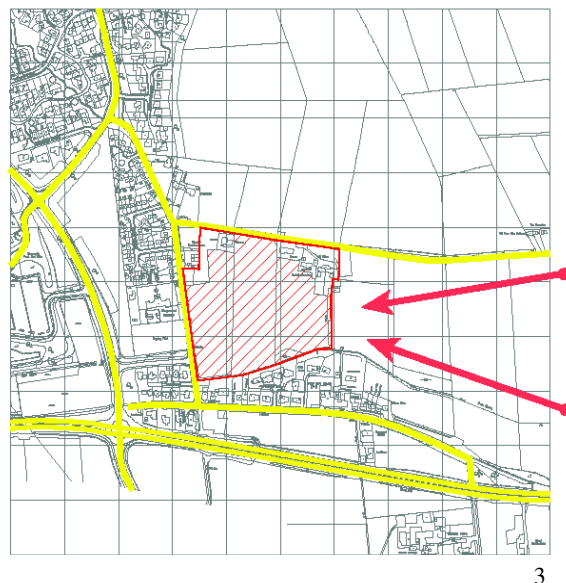


Figure 4: Site Visibility



The Den Burn lies to the south and south east and is an important natural feature what has been given due consideration in the design process, both in the landscaping proposals and in relation to the location of development.

The Den Burn corridor adjacent to the site will be retained and improved where appropriate. The introduction of landscaping and the SUDs basins will help to create open space which will enhance the wetland habitat. Footpaths will be provided to link the Den Burn to the surrounding area to improve access for all.

An Archaeology walk over study has been completed which concluded that there is no evidence of any Huxter Stones on the site, which was suggested in the Kingswells Development Framework. A further evaluation will be undertaken before development to ensure this is the case.

An Ecological Assessment was also undertaken which confirmed the site does not lie within any designed sites of interest. It is not listed under any statutory nature conservation, nor is it within any Council landscape or wildlife designation. The Den of Maidenraig Local Nature Reserve is located approximately 1km downstream of the masterplan site.

The Den Burn is identified as being of ecological interest. The land surrounding the burn will be retained and enhanced so that it is

inkeeping with the important linear habitat of the burn.

Environmental enhancements are proposed throughout the site, including the creation of new wooded areas and the improvement of the burn area, which is compliant with national and local plan policies. Net gains in biodiversity will arise as a result of these improvements, to the benefit of the wider area.

A Landscape and Visual Appraisal was also carried out which has informed the masterplan.

Communication and Engagement

A range of consultation exercises have been carried out in the development of the Masterplan.

This included a meeting with the Community Council; Two public consultation events, including a community council and councillor briefing prior to the public event; and a meeting with the adjacent landowner.

Issues highlighted at these events have been summarised and a response to each is contained in Appendix 1 and 2 of the Masterplan document.

Where possible, the wishes of the local community have been taken on board and incorporated into the design of the residential development. An explanation has been provided where this has not been possible.

Identity

Through the site analysis and public consultation, proposals were developed. A number of principles guided the Masterplan, consisting of the following:

- Traditional villages are successful due to their use of both green and “urban” spaces. Such spaces assist the legibility of their community due to the association of streets with a sense of place.
- The development of the site will be based on a series of green spaces which will be linked by a network of landscaping. The southern edge will enjoy clusters or strands of trees to create a more open “parkland” environment, whilst denser shelter belt planting will be utilised around the eastern perimeter of the site.
- Landscaping and open space requirements will be accommodated in the overall layout, within which the general philosophy will be to create “streets” and “places” rather than roads.
- The “streets” and “places” created will be aligned with groups of housing. Building lines are an important consideration in the designing of successful development and will ensure the creation of a “street”.

First Thoughts

From these principles emerged some first thoughts which were considered important in the development of the site, including:

- Retention of the Den Burn and Green Space Network.
- Creation of a new shelter belt to the east to provide screening and to soften the development.
- Enhancement of the buffers to the east.
- Provision of an appropriate density of housing whilst retaining quality public green space.
- A series of linked public spaces to allow green space to flow through the heart of the site.
- Easily accessible public green space to the south
- Principal vehicular access from Fairley Road with secondary access from the Land Stracht.
- Integration of the site within the context of Kingswells by reinforcing the building lines within the surrounding developments along the Lang Stracht. Development to face outwards at these points.
- Reinforcement of the rural character.
- Appropriately scaled and designed house types created using the established pattern of field boundaries

Transport and Accessibility

The Masterplan considers the existing access routes in the area in relation to the site, including pedestrian and cycle routes and core paths; public transport connections; road connections; and the connectivity of the site with the surrounding area.

The site will be fully connected with the surrounding area and this is crucial to the success of the site.

Two points of access are proposed in accordance with Council guidelines. The main access is from Fairley Road, with a secondary access from the Lang Stracht. Two points of access from Fairley Road were investigated, however, due to visibility requirements, junction spacing and configuration requirements, this was not possible. Access from the Lang Stracht will be a right in and left out configuration to minimise

movements eastwards along the Lang Stracht beyond the development access.

The access from Fairley Road continues into the site providing an opportunity for an east-west spine from which an arrangement of meandering secondary minor access roads run northwards designed to minimise gradients and cut and fill as much as possible.

Development will be supported by the infrastructure required to sustain the development identified as necessary through a Transport Assessment (TA). This will identify a Travel Plan Framework to allow the preparation of a Residential Travel Pack for approval by Aberdeen City Council prior to first occupancy of the site.

STF policy will apply. Details will be worked out at the planning application stage in addition to site specific transport mitigation.

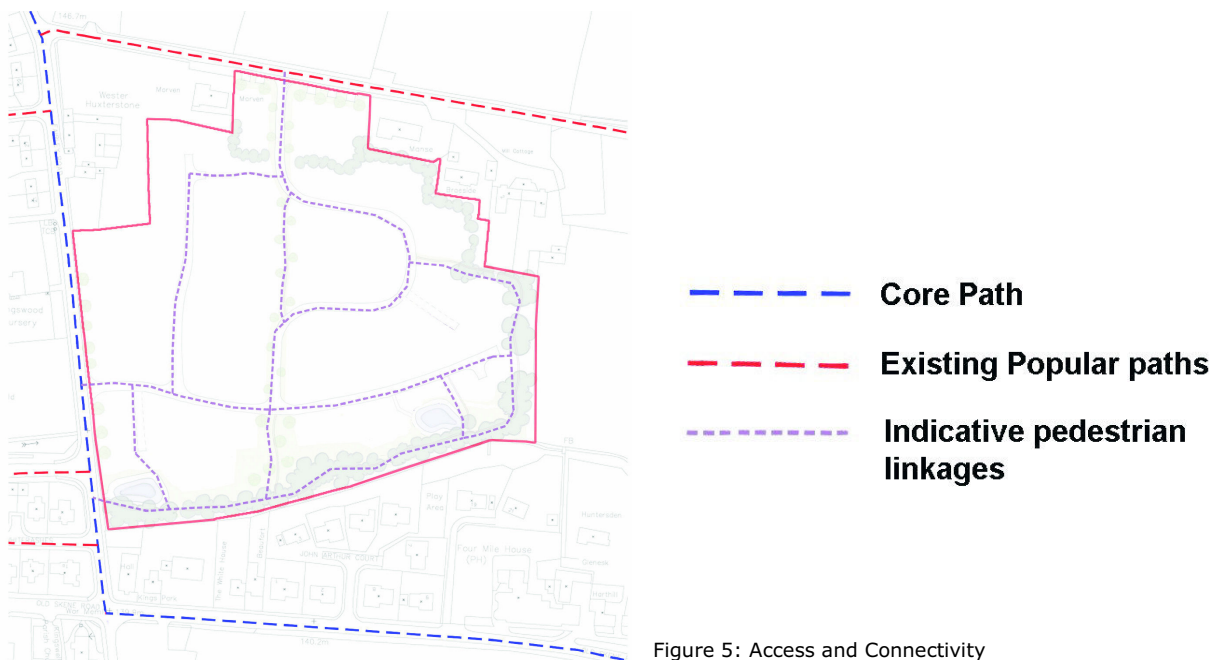


Figure 5: Access and Connectivity

Planning and Design Principles

The masterplan provides a context for development and outlines the planning and design principles that should be followed in any planning applications for the site.

The main issues considered were the Den Burn, the Green Space Network, topography of the site, existing properties to the north and existing stone walls.

One of the main aims was to integrate the site within the existing neighbourhoods through a successful strategic landscape network. This provides an attractive landscape setting, within which housing will be located, so that the site fits with the character of the wider area.

Given the visibility of the site from the east, a sensitive design solution, offering suitably located development and enhancement of strategic landscaping is proposed. Landscape wedges will cut into the site at a variety of points allowing pedestrian access from the site to land beyond. These spaces are complemented by pathways to create a recreational framework.

A new shelter belt is to be planted along the eastern boundary to soften views of the development when viewed from the east. Landscaping adjacent to existing properties to the north will ensure their amenity is protected.

The Green Space Network to the south is an important feature which will be protected and enhanced.

Areas of open space will be concentrated principally along the burn, which will maintain the local character of the area. Open space will be linked by a network of footpaths which will ensure the integration of the site with the wider Kingswells area.

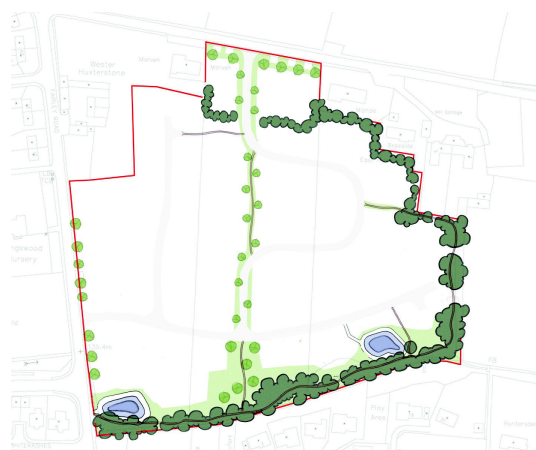
Street trees will soften the street scene and provide visual interest within the development.

The design and layout of the development will fit within the landscape framework and network of open space on the site and will avoid skyline development.

Housing will sit away from the Den Burn and Green Space Network to ensure their protection. Housing will follow the established pattern of field boundaries.

The Masterplan and Landscape Appraisal provide further details of the landscaping and open spaces proposed. A Design Statement will be submitted with any planning application(s) for the site to explain how the design and layout of the external spaces will contribute to a quality environment.

Figure 6: Landscaping



Design and Layout

The rising topography of the site will allow for the development to fit naturally into the landscape, avoiding housing intruding into the skyline.

Steps will also be taken, wherever possible, to minimise the extent of tabling. Each house will sit at its own appropriate level, which will enhance connectivity and create a more cohesive layout.

Development will consider Scottish Government Policy 'Designing Streets' and 'Designing Places'. Surfaces will be shared to promote pedestrian priority by reducing vehicle speeds.

The orientation of properties will be maximised to take advantage of available views. Housing on the perimeter will face the surrounding streets while housing in the heart will generally face south. Development will not turn its back on the Lang Stracht as required by the Kingswells Development Framework.

Development will therefore face outwards at these points to engender them with the architectural merits of a 'street'. This follows best practice, so that proposals engage with their surroundings, offering visual and physical connectivity to the existing village.

Four character areas are identified in the Masterplan, including:

- Rural Lang Stracht Character Area
- The Den Burn Character Area
- Central Character Area
- Fairley Road Character Area

The character areas are determined by the architectural styles of the different developers, although a palette of similar materials and finishes will be provide unity and coherence throughout the development.

The Masterplan provides full details of the character areas.

Figure 7: Character Areas



The Rural Lang Stracht Character Area

This area will be characterised by buildings of a similar footprint to those already found either side of the site on the Lang Stracht. Split level housing will be located in this area which will reduce roof heights and be in keeping with existing houses in the area.

Figure 8: Lang Stracht Perspective



The Central Character Area

This area is characterised by the pedestrian landscape link running north south through the centre of the site, the function of which is to provide a link from the Green Space Network to the south to the rest of the site and the settlement of Kingswells.

Figure 9: Landscape Link Perspective



It also provides recreational opportunities in the form of a walking route through the site. The wedge provides visual continuity from the Den Burn Character Area by continuing the green link. Dwellings overlook this area to increase visual supervision and safety.

The Denburn Character Area

This area is dominated by the Green Space Network to the south which provides the main focal point of the site in the form of open space along the Burn. The function of this space is to provide recreational opportunities in the form of footpaths along the burn, linking to footpaths around the site.

New landscaping will enhance this area and a play park will provide opportunities for children to enjoy this area.

Figure 10: Eastern Courtyard Perspective



Shared surfaces will reduce vehicle speeds and encourage pedestrian activity in the area. Houses to the east will be grouped around a courtyard.

Fairley Road Character Area

The houses facing Fairley Road will be of a similar typology of those found elsewhere along the street. They will formally face the street and will utilise a simple palette of materials and detailing complemented, where practical, by drystone dykes and formal tree planting forming their front gardens. The houses either side of the entrance will be selected to respond to 'gateway' opportunity.

Figure 11: Fairley Road Perspective



The layout will make efficient use of the land and housing will range in floorspace from 44 square metres to 140 square metres offering a variety of accommodation and styles, ranging from one to five bedroom properties.

Materials will vary and include smooth render, dry dash render and stonework. Roofs will consist of concrete tiles or slate effect tiles in differing colours.

The developers promote a "fabric first" approach which looks at how the construction of the properties can create less need for energy consumption before adding any energy generating technologies. This primarily focuses on increased levels of insulation, air tightness and building orientation to maximise solar gain.

The buildings will meet current standards of sustainability promoted by Aberdeen City Council.

An indicative design concept is contained in the masterplan as indicated below.

Figure 12: Indicative Design Concept



The indicative design concept is taken a step further in the preparation of an indicative block layout for the site.

Shared surfaces will reduce traffic speed and encourage pedestrian priority. The site is linked by a network of open space and footpaths and a play area is located within the open space to the south. This is explained further in the Masterplan document.

Figure 13: Indicative Block Layout



Infrastructure

A Drainage Impact Assessment and Sustainable Urban Drainage Strategy (SUDs) will be submitted with any planning application(s) for the site. A construction method statement will also be submitted in

relation to development activity on the site.

The Masterplan provides details of foul and surface water infrastructure for the site. Sewers will be designed and installed in accordance with best practice advice.

Developer Contributions

Aberdeen City Council through their Action Programme indicates the infrastructure requirements for the site. Details are provided in the Masterplan.

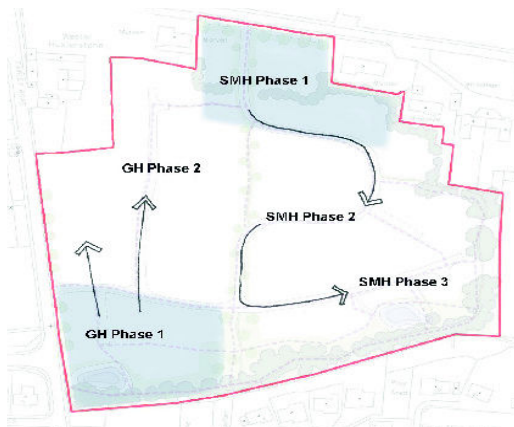
The amount and type of contributions will be commensurate with the scale and impact of development as required by Scottish Government Circular 1/2012: Planning Agreements.

The precise levels of infrastructure provision and developer contributions required for the development will be agreed with Aberdeen City Council via the planning gain officer in consultation with other statutory agencies.

Affordable housing will be provided in accordance with the approved Local Development Plan and will be integrated throughout the development.

Phasing and Delivery

Development will be provided in a number of phases and the Masterplan provides an indication of when they will be delivered.



Planning application(s) for the site will be accompanied by a Delivery Statement providing further details of how the proposed development and supporting infrastructure will be provided.

Development Process

Subject to approval, the Masterplan will become Supplementary Guidance and a material planning consideration in the determination of planning application(s) within the area covered by the Masterplan.

All planning applications will comply with the Town and Country Planning (Hierarchy of Developments)(Scotland) Regulations 2009 and where appropriate the Pre-Application Consultation (PAC) process outlined in the Planning etc (Scotland) Act 2006.

Planning applications, where required will be submitted with a PAC report detailing the public consultation carried out. This will provide evidence that there has been appropriate input from stakeholders, landowners, the local community and other community representatives in the preparation of the proposals. Where disagreement may have arisen which has not been resolved, this will be made clear.

Figure 14: Phasing

Further Information

For further information contact:

Planning and Sustainable
Development
Aberdeen City Council
Business Hub 4| Ground Floor
North
Marischal College
Broad Street
Aberdeen
AB10 1AB

The Masterplan was prepared in
association with Aberdeen City
Council by

Ryden

Ryden

Nicol
Studios

Russell



Fairhurst

FAIRHURST

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Our Ref: OP42, Huxterstone Kingswells

Direct Dial
Email



25 September 2012

Corporate Services

Legal and Governance

c/o Banchory Area Office

The Square

Banchory

AB31 5RW

Tel 01330 825518

Fax 01330 823972

www.aberdeenshire.gov.uk

Dear Sirs

**Consultation, OP42 West Huxterstone, Kingswells
Masterplan**

Any future applications will be considered under the Aberdeen City and Shire Structure Plan and the recently adopted Aberdeen Local Development Plan. The Structure Plan aims are to:

- Provide strong framework for investment decisions which help to grow and diversify the regional economy, supported by promoting the need to use resources more efficiently and effectively, and
- Take on the urgent challenges of sustainable development and climate change.

To support these aims, the plan also aims to:

- Make sure the area has enough people, homes and jobs to support the level of services and facilities needed to maintain and improve the quality of life,
- Protect and improve our valued assets and resources, including the built and natural environment and our cultural heritage,
- Help create sustainable communities, and the associated infrastructure, which meets the highest standards of urban and rural design and cater for the needs of the whole population,
- Make the most efficient use of the transport network, reducing the need for people to travel and making sure that walking, cycling and public transport are attractive choices.

The Aberdeen Local Development Plan policies which would apply are Policy I1 – Infrastructure Delivery and Developer Contributions and Policy H5 – Affordable Housing.

Policy I1 – Infrastructure Delivery and Developer Contributions, states that Development must be accompanied by the infrastructure, services and facilities required to support new or expanded communities and the scale and type of developments proposed. Where development either individually or cumulatively

will place additional demands on community facilities or infrastructure that would necessitate new facilities or exacerbate deficiencies in existing provision, the Council will require the developer to meet or contribute to the cost of providing or improving such infrastructure or facilities.

Infrastructure requirements relating to Masterplan Zone sites and other allocated sites outwith the Masterplan Zones are set out in Appendices 4 and 5. Actions for delivering such infrastructure are described in the Local Development Plan Action Programme. Infrastructure requirements and the level of developer contributions for other development will be assessed using the criteria set out in the Infrastructure and Developer Contributions Manual. The precise level of infrastructure requirements and developer contributions will need to be agreed with the Council, and other statutory agencies. The level of provision or contribution required will relate to the development proposed either directly or to the cumulative impact of development in the area and be commensurate to its scale and impact.

Masterplans will be expected to reflect the infrastructure requirements and developer contributions identified and should include a Delivery Statement setting out details of how the proposed development, and supporting infrastructure, will be delivered.

New infrastructure will either be provided by the developer or through financial contributions.

Policy H5 - Affordable Housing states that:

Housing developments of five units or more are required to contribute no less than 25% of the total number of units as affordable housing. Further guidance on the provision of affordable housing from new developments is available in Supplementary Guidance on Affordable Housing.

Copies of the policy are available on the Aberdeen City website at www.aberdeencity.gov.uk

Affordable Housing

Policy H5 seeks a minimum of 25% of any development of 5 or more units be provided as affordable housing.

Possible categories of Affordable Housing

1. Social rented

2. Intermediate housing:

Shared ownership

Shared equity

Discounted low cost sale (Low Cost Home Ownership)

Housing without subsidy (low cost entry level)

Mid Market Rented Accommodation

3. Other Options

At this point in time these are the only models for delivery of affordable housing that have been identified. However, this does not rule out the opportunity for new models for affordable housing delivery to be developed and meet the affordable housing requirement.

Further discussion with Graeme Stuart at the Housing Service is encouraged, Graeme can be contacted at: GSTUART@aberdeencity.gov.uk

Infrastructure contributions

Education

Officers from the Education Culture & Sport Service, Planning Gain Team and the Local Development Plan Team undertook a joint assessment of the impact of proposed development on the provision of education services, and the need for new and extended schools. This work took account of the 2009 School Roll Forecasts, published in May 2010, which includes the most up to date information on current school capacities and the likely spare capacity available in the future.

The actual impact on future development will depend on:

Update of the school role forecasts;

Revision of the school catchment areas; and

Any changes to the provision of educational services.

Infrastructure requirements for education provision is based on the anticipated number of pupils from a proposed development and their effect on the Primary and Secondary school rolls serving that development averaged out over a 5 year period from the anticipated development start date. The number of pupils generated by a development is based on the rates of 0.25 Primary school children per standard house unit and 0.175 Secondary school children per standard house units. One bedroom dwelling units are not assessed for education contributions. The required level of contribution is then determined from the 'rate per pupil' required to provide the following:

Relocatable accommodation should be provided where the school roll is expected to, or already, exceeds the capacity of the school and is forecast to fall back to an under capacity position during the forecast period, using the following figures:

Primary – Temporary accommodation = £7,000 per pupil generated

Secondary – Temporary accommodation = £9,000 per pupil generated OR

New build accommodation should be provided where the school roll forecast trend is for it to grow beyond its current capacity or it is already above capacity and the additional pupils from the development will add to this over capacity problem.

Primary – Permanent extension or new build school = £23,000 per pupil generated.

Secondary – Permanent extension or new build school = £44,000 per pupil generated.

Education will advise on this issue and further discussion with Derek Samson at the Education, Culture and Sport Service is encouraged, Derek can be contacted at: dsamson@aberdeencity.gov.uk.

Community Facilities

New development will generate a requirement for the provision of community facilities. This is calculated on the requirement of 0.69 m² of community facility per dwelling unit as determined for small to medium size developments. This is to ensure that existing residents are not disadvantaged by an increase of usage from additional residents the proposed development would generate. The level of contribution is determined by the size of the existing facilities and current population served by the facilities together with the anticipated increase in the population served by the new development. In some instances there will be no contribution required if the facilities are of adequate size to cope with both existing and anticipated usage. Contributions are calculated on the basis of the requirement for 0.69 m² of community facilities per household, using a build cost of £1,175 per m².

Sports and Recreation

Playing Fields and Pavilions

As a guide the cost of providing a Playing field is in excess of £60,000 this includes the average land purchase costs and site works. The requirement per 1000 residents is 6 acres (2.4 hectares) of recreational space which includes at least one football pitch/ playing field of 0.7 hectares. At the average of 2.3 persons per household this equates to 435 households requiring the full mitigation of at least one full size pitch and associated recreational open space. The requested gross contribution of £135 per household is therefore in line with the £138 that would be required to fulfil these requirements and is considered fair as the reduction includes a full

discount of the land element within the overall cost to the Council. Where existing recreational facilities are undersized for the current population a contribution towards improvements or additional facilities is considered fair and reasonable in order to extend the operating capacity of these facilities. This may for example be used in the form of additional drainage to enhance the playing capacity of the pitch.

The figure of £135 per household was further refined to indicate what portion of the funds would be required for the works and that which would be required to maintain those works in order that the facilities can become operational in the short term. It is apparent that one could not proceed without provision for the other. The overall level of discount for the actual works is therefore substantial and developers are cautioned that these figures will require upward revision in the near future. For pavilion provision, a figure of £360 per new household will be used.

Library

The Council will seek contributions towards the creation of libraries. The guideline requirement for floor area in relation to population indicates that a building of at least 42 m² per 1,000 residents is required. As the population criteria are exceeded only by continued expansion of the settlement by developers the need for a contribution towards such facilities is therefore reasonable and justifiable. The Authority will plan for any contributions towards improvements to the service provision in the most effective manner. Contributions are calculated assuming 2.3 persons per household, at a build cost of £1,175 per m², required by the population arising from the development.

Core Path Network

Core Path 31 Kingswells Avenue to Old Skene Road is a linear route through Kingswells village from Kingswells Avenue to Old Skene Road, which links many community facilities such as the school, the community centre, local shops, Post Office, pharmacy and school playing fields. The route provides a safe route to the Primary School by linking paths and pavements through Kingswells and will link development to the south of Kingswells to the heart of the community.

An Aspirational Route AP5 Kingswells- Bucksburn has been identified to the north of Kingswells which aims to provide an off-road path linking the communities of Kingswells and Bucksburn. The exact route that the path will take is yet to be decided in consultation with communities. There is significant demand for a path link between the communities, especially to enable children to cycle to the new Bucksburn Academy and adults to travel to work on foot. Contributions will be sought to promote and extend the Core Path network in and around Kingswells based on a contribution of £371 per 'standard' unit.

For clarification, the contribution is required for implementing or linking to the Core Paths Network. Core paths and links to the Core Paths Network are an infrastructure facility necessary for the purposes of recreation and sustainable active travel. New developments are required to install or upgrade core paths that are designated within the site and contribute to any cumulative impacts on surrounding core paths. This figure is based on current costs for path construction per square metre accounting for fit for purpose surfacing, signage and interpretation panels.

Strategic Transport Fund

All housing, commercial, industrial, retail and commercial leisure developments in both the Aberdeen Housing Market Area (AHMA) and the Strategic Growth Areas (SGA's) will be expected to make a fair and proportional contribution. These funds will ensure the delivery of a package of road and public transport interventions where the cumulative impact of new housing and employment uses is likely to cause increased congestion. This site will be liable to Strategic Transport Fund contributions, with the exact level of contribution determined by the Transportation Service. Transport will advise on this issue.

Public Transport

Frequent public transport services to serve the whole masterplan area which may include extensions to existing services. Contributions will be made as necessary.

Integration with the Park and Ride services will be provided via the network of existing paths in the vicinity which the site will link to.

Healthcare

Infrastructure requirements have been calculated with NHS Grampian on the basis of national health standards and by estimating the likely number of new patients generated by each proposed development. Contributions will be calculated using nationally recognised space standards and build costs, based upon the population requirements for GP surgeries, dental chairs and community pharmacies. Any financial contributions will need to be agreed with the Council, in consultation with NHS Grampian, before an application can be determined.

For information, potential provision may include:

Extension at Kingswells Health Centre to support the General Medical Services for the additional patients from the various developments in the Kingswells and Maidencraig areas. Space for 1 additional GP will be required.

A 2 Dental Chair facility either in the recommended extension to the Kingswells Health Centre. This would also serve the Maidencraig development.

The amount and type of contributions will be commensurate with the scale and impact of development as required by Scottish Government Circular 1/2010; Planning Agreements. Developers will not be expected to make good existing shortfalls. They will be the subject of negotiation and agreement at the planning application stage. The joint developers will enter into individual legal agreements with Aberdeen City Council to ensure the delivery of all financial contributions. It is envisaged that each developer will contribute a pre determined amount per completed house.

Contributions may also be required in order to address the impact of development on the transport network. Any such impact will be determined through the Transport Assessment. Contributions to the Strategic Transport Fund will be assessed at the planning application stage against approved guidance and contributions made if necessary. Similarly, upgrades to the water and sewer network will be influenced by the Drainage Impact Assessment carried out for the site. The precise level of infrastructure provision and developer contributions required for any development will need to be agreed with the Council via Planning Gain in consultation with other statutory agencies. This will be carried out via a legal agreement at the planning application stage.

Yours Sincerely,

Adam Sime
Planning Gain Officer

12 October 2012

Development Plan Team
Planning & Sustainable Development
Enterprise Planning & Infrastructure
Aberdeen City Council
Business Hub 4
Ground Floor North
Marischal College
ABERDEEN, AB10 1AB

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✓		
GB		LP

Dear Sir/Madam

WEST HUXTERSTONE MASTERPLAN – OP42

As I am a resident of Kingswells, I am interested in any plans relating to new developments at Kingswells. This interest has recently been heightened by what I, and others, consider to be a disgraceful development at West 1 as carried out by SMG.

I should be devastated if the theme of the West 1 Development were to be repeated at Huxterstone, something which I fear is a distinct possibility since SMG are one of the developers involved.

My first point with regard to Huxterstone is that there should be a minimum standard of, in particular, the low-cost housing and that this minimum standard should be considerably higher than at West 1.

Secondly, I observe that there is a proposal to access the Lang Stracht from the development. Under no circumstances should this be permitted. The Lang Stracht is narrow and buses have difficulty when encountering on-coming vehicles. The road is not designed to cope with any increase in traffic. It is now widely used by pedestrians, cyclists and horse riders and has become a useful amenity in the area. This should not be interfered with. Any increase of usage by vehicular traffic will merely encourage the road to be used as a rat-run. There is ample room at the proposed Huxterstone development to allow accesses to and from Fairley Road.

My final point with regard to the Lang Stracht is that I suspect any additional use by vehicular traffic will act as an argument to promote future development at Gillahill, another site which SMG is keen to develop. The history of this site is something you will be well aware of.

Thirdly, I understand that Kingswells School is already over-capacity, a situation which is being exasperated by the number of children residing at the West 1 Development. Clearly the school is currently unable to absorb the increase in child numbers arising from the proposed development at Huxterstone.



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2

Development Plan Team

12 October 2012

Accordingly, I submit that this development should be delayed until such time as the school roll diminishes to the extent that the school is capable of absorbing an influx of children from Huxterstone. Incidentally, I understand that the present over-capacity at the school has arisen due to forecast errors made by ACC. Do not, therefore, compound errors the Council may have already made in the past.

Yours faithfully

A large black rectangular redaction box covering the signature of W Guy Bentinck.

W Guy Bentinck

cc Kingswells Community Council
Councillors David Cameron, Steve Delaney and Len Ironside CBE

1/27/2001

One of the most important aspects of the study of the history of the United States is the study of the role of the individual in the development of the nation. The study of the individual is a study of the human condition, and it is a study of the human condition that is essential to the understanding of the history of the United States.



The study of the individual is a study of the human condition, and it is a study of the human condition that is essential to the understanding of the history of the United States.



HISTORIC SCOTLAND
ALBA AOSMHOR

Louise MacSween
Planner
Planning and Sustainable Development
Aberdeen City Council
Business Hub 4
Marischal College
Aberdeen
AB10 1AB

Longmore House
Salisbury Place
Edinburgh
EH9 1SH

Direct Line: [REDACTED]
Switchboard: 0131 668 8600
[REDACTED]

Our ref: LDP/A/2
Our Case ID: 201203927

12 October 2012

Dear Ms MacSween

**Consultation
OP42 West Huxerstone, Kingswells Masterplan**

Thank you for your letter of 17 September 2012 seeking our comments on the Masterplan for the OP42 site at Kingswells from the Aberdeen Local Development Plan. The following comments are based on our statutory historic environment interests. That is scheduled monuments and their setting, category A listed buildings and their setting and gardens and designed landscapes and battlefields in their respective Inventories. We would advise you also seek comments from Aberdeen City Council's Conservation and Archaeology Services who will also be able to advise on the potential for significant impacts on the historic environment and of potential impacts and mitigation for any sites of regional and local importance.

As you will be aware, Historic Scotland were contacted previously during the preparation of this masterplan. In our response (dated 13 April 2012) we noted that development in this area was unlikely to impact upon our statutory interests for the historic environment. Having studied the supplied masterplan I note that these comments have been brought forward into the plan. Therefore, other than welcoming the preparation of this masterplan I can confirm we have no further comments to offer.

Should you wish to discuss any issue raised in this response please do not hesitate to contact me on [REDACTED] or [REDACTED]

Yours sincerely

[REDACTED]
Andrew Stevenson
Senior Heritage Management Officer (SEA)



www.historic-scotland.gov.uk

From: "Muir, Jim (JimMuir)" [REDACTED]
To: "ldp@aberdeencity.gov.uk" <ldp@aberdeencity.gov.uk>
CC: Pamela Nathaniel [REDACTED]
Date: 15 October 2012 00:20
Subject: Public Consultation for OP42 - West Huxterstone Masterplan.
Attachments: Final education - west Huxterstone .doc

Dear Members of Aberdeen City Council, Development Plan Team, Planning and Sustainable Development, Enterprise, Planning and Infrastructure,

This email refers to the Public Consultation for OP42 - West Huxterstone Masterplan and is sent from myself Jim Muir on behalf of the Kingswells Primary School Parent Council; Pamela Nathaniel Chair of Kingswells Primary School Parent Council is copied.

The Parent Council expresses severe concern at the proposed timing for the development given school roll implications. The council does not object to the development if it is planned in a manner which prevents the school going over capacity.

Properly using ACC data and methodologies it is clear Kingswells Primary School will go over capacity in 2013 (not 2015 as erroneously claimed) and will remain so until at least 2016.

The obvious conclusion is further development at Kingswells should be not be considered until at least 2016.

However there is further concern over the factor (0.3) for children used by Aberdeen City Council, the factor is far too low given that Kingswells attracts young families.

The obvious implication is the capacity projections are overly pessimistic therefore even suggesting 2016 for the development may be optimistic.

I have attached a communication from Kingswells Community Council which clearly demonstrates that postponing and rephrasing the development will work in terms of school capacity.

Kingswells Primary School Parent Councils requests Aberdeen City Council reconsider the timescale for the development, to do otherwise clearly beings with it education concerns which could easily be averted using fully considered planning.

I would be very grateful if receipt of this email could be confirmed.

Best Regards, Jim.

Jim Muir
Member of Kingswells Primary School Parent Council

[REDACTED]

Primary school provision

Kingswells Primary School is facing a crisis. The ACC school roll forecast suggests that the school will go over capacity in 2015. These figures are based on the 72 homes at the West One development being completed in 2013. The development was actually completed in 2012. Consequently, the school will go over capacity next year rather than ACC's prediction of 2015.

The proposed phasing of the development results in the school going over capacity as shown in the following graph.

	Phase 1	Phase 2	Phase 2	Phase 2	Phase 3
	2013 (Q4)	2014	2015	2016	2017
SMG Units	10	15	15	20	10
GH Units	10	15	15	10	

Masterplan Phasing

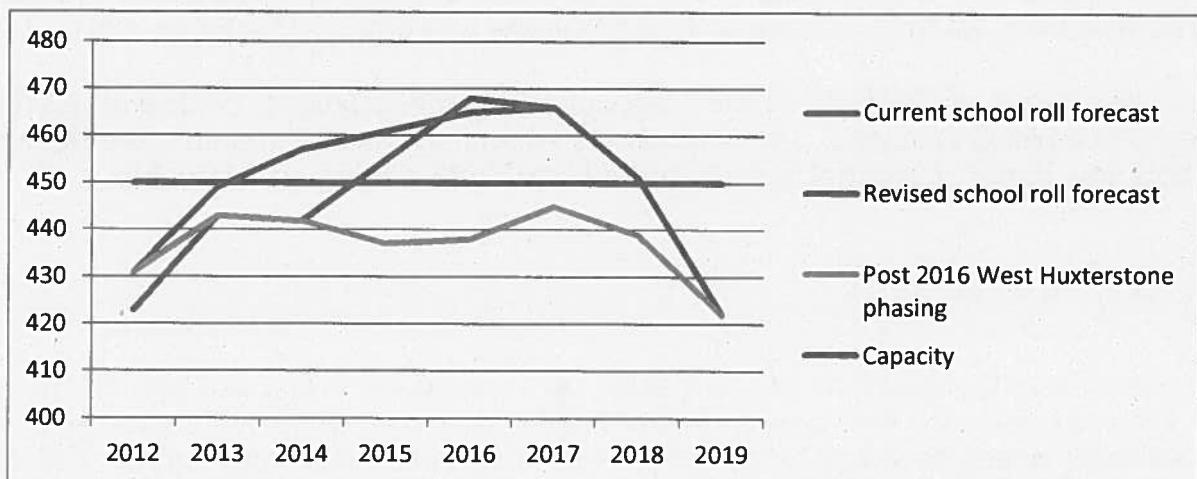
Year	2016	2017	2018	2019
Combined Units	10	30	30	40

KCC Recommended Phasing

KCC propose the following phasing which will allow the development to proceed without a major impact on the education of the Kingswells Primary Children.

The following table and graph show the roll broken down.

KINGSWELLS PRIMARY SCHOOL ROLL FORECASTS	2012	2013	2014	2015	2016	2017	2018	2019
Current Housing Forecast	27	27	0	60	60	0	0	0
Cumulative	27	54	54	114	174	174	174	174
Revised Housing Forecast - SMG phasing	54	20	30	30	30	10	0	0
Cumulative	54	74	104	134	164	174	174	174
Difference in cum housebuild	27	20	50	20	-10	0	0	0
Roll adjustment	0.3	8	6	15	6	-3	0	0
	2012	2013	2014	2015	2016	2017	2018	2019
Current school roll forecast	423	443	442	455	468	466	451	422
Revised school roll forecast	431	449	457	461	465	466	451	422
Post 2016 West Huxterstone phasing	431	443	442	437	438	445	439	422
Capacity	450	450	450	450	450	450	450	450
Revised school roll forecast (No WH)	431	443	442	437	432	430	415	386
Current Under/Over capacity	-27	-7	-8	5	18	16	1	-28
Revised Under/Over capacity	-19	-1	7	11	15	16	1	-28
Revised Under/Over capacity (NO WH)	-19	-7	-8	-13	-18	-20	-35	-64
Post 2016 West Huxterstone phasing	-19	-7	-8	-13	-12	-5	-11	-28



KCC have shown that it is possible to phase the building works to avoid impacting the education of the primary school pupils by running the school at 'over capacity'. We also request that phasing should not be fine-tuned to run the school at capacity as this would increase the likelihood of having one or more composite classes which impacts on the 'actual capacity' figures that are possible.

Previous Errors Made With School Roll Figures

The West One Development, which is nearing completion, was approved with 72 homes rather than the 50 homes identified in the Local Development Plan because ACC Planning Department and Elected Members were provided erroneous capacity figures (540 instead of 450) by ACC's Education Department.

KCC received the following account of how this occurred...

Revised school capacities were reported to our Education, Culture and Sport Committee on 7 January 2010, following analysis of the school buildings in relation to their ability to meet the requirements of the Curriculum for Excellence .

<http://committees.aberdeency.gov.uk/mqConvert2PDF.aspx?ID=289&T=10>

Within the Appendix 5 to this report, (which was agreed), the revised capacity for Kingswells School is clearly shown as 450. Unfortunately, within some of our supporting internal paperwork, this number appears to have been wrongly transposed as 540, (rather than 450). It therefore seems that, when one of my colleagues was asked at short notice by Dr Maggie Bochel, Head of Planning & Sustainable Development, on the morning of a Development Management Sub-Committee, about the capacity of Kingswells School, he inadvertently quoted the 540 figure, rather than the correct figure of 450.

As soon as we realised this error, we alerted colleagues within Planning, but unfortunately this was after the Sub Committee in question. (ACC 26th January 2012)

Size of the Development

The Local Development Plan has identified the West Huxterstone development is suitable for 120 homes

However, the Indicative Block Layout Plan (Figure 24) on Page 20 of the West Huxterstone masterplan shows 114 houses plus 4 blocks of either 6 (=24) or 4 blocks of 8 (=32). **This suggests that the final total number of homes on the site could actually be 138 or 146.**

We have also noted on Page 10 of the Masterplan that the "adjoining landowner has intimated their desire to develop their land, part of which lies within the masterplan area". **We have no clear indication how many additional homes might be added on this part of the site.**

Other School Problems

The problems are not helped by ACC's insistence on using the formula of 0.3 children per new home. This approximation may broadly apply for a large number of homes across Aberdeen as a whole but could clearly be wrong for smaller developments like West Huxterstone. It is significant that 5 bedroomed family homes would equate to just 1.5 children and that one-bedroomed

affordable homes are completely discounted. Even a small discrepancy in the projected number of children could easily push Kingswells Primary over capacity.

Kingswells has had an abysmal history of poor decision-making by ACC over primary school provision. More and more new homes were built with no provision for the education of the growing population of children moving into the area. The primary school was forced into using general purpose rooms as classrooms, then the old Fairley Road school was reopened as a nursery and portacabins were installed for P1 and P2. As a consequence of the split site and overcrowding, the education of Kingswells children was severely compromised for years. The school has already been extended and the playground had to be sited outwith the school grounds. The school site footprint is now fully utilised with buildings and with a staff car park which is inadequate. Often staff park in residential streets around the school. There is no further suitable land available for portacabins or an extension

Conclusion

The problems are serious and have been caused by miscalculations by personnel within ACC and the insistence of maximising development beyond the numbers identified in the Local Development Plan. The Planners and Developers have a duty of care to the community to 'get it right'.

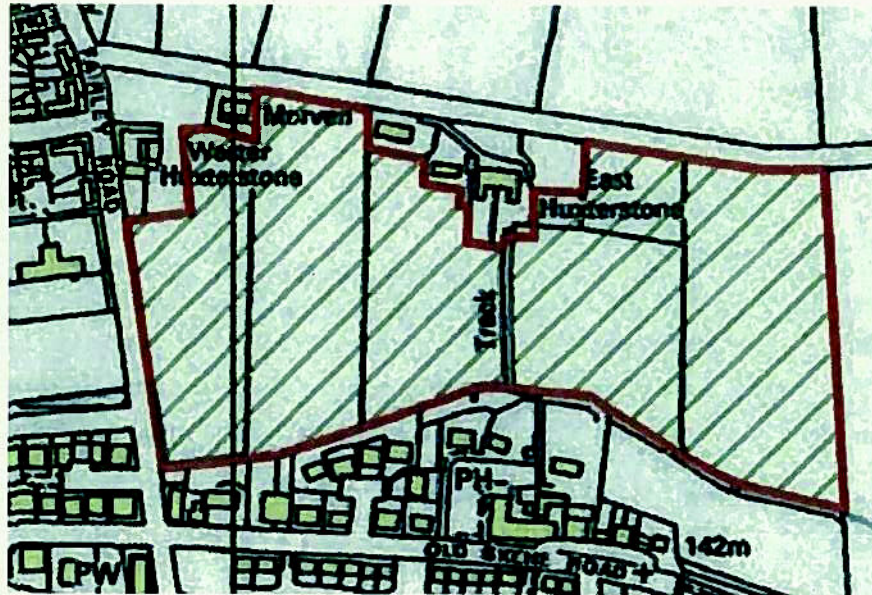
Kingswells community will not tolerate overcrowding at the primary school again.

We demand that the building of houses at West Huxterstone is phased so that the school can accommodate the children without detriment to their education.

Also, with the phasing suggested by KCC, the developers can then complete the whole development over 4 years instead of 5. Any additional housing above 120 would extend the period of development.

1. **Masterplan Area**

The area identified for the Masterplan in planning reference 120271 is shown in the following extract.



The area being considered for development is as shown below



We also note that the proposal to make the Lang Stracht a street would also require a foot path to extend to Fairley Road.

The area required for the footpath and the area shown in purple are out with the red line in the original application.

It is noted that there may be some land ownership issues with installing a footpath along the Lang Stracht to Fairley Road. If this is the case then both accesses should be from Fairley Road.

2. Access

2.1 Lang Stracht Access

KCC agree with the requirement to have two access roads into the site, but want these to be from Fairley Road. Both of these accesses should operate in two directions in case of an emergency on site requiring emergency vehicle access, with one road blocked or impassable with snow.

KCC strongly object to any vehicular access to/from the Lang Stracht from the development at Huxterstone. Some of the reasons are listed below but these must be read in conjunction with Section 3.2:

1. The Lang Stracht will be designated a 'bus lane' (changed from a 'bus gate') this financial year. We wish it to remain a free flowing bus route, an access to the local residents only and kept relatively traffic free as it is much used by walkers, cyclists and horse riders. It is thus an asset to the community and other users.
2. Visibility approaching the proposed access is poor.

The gradient at the lower end in conjunction with the false crest conceals traffic approaching from the east.



3. The Lang Stracht is unsuitable for too much traffic due to its narrow width and poor surface condition. There is not enough space for a car and a bus to pass. The car would generally give way and pull over. Road improvements could possibly solve some of the safety issues, but these would change the nature and setting of the area.
4. The developer also wants to form a street with seven new homes presumably with driveways off the Lang Stracht. This will add to the traffic issues and further detracts from the rural setting in this area. The existing homes blend into the setting, they are low

lying 1.5 storey homes and consider the Lang Stracht to be the 'back entrance' (albeit only entrance) to their homes. The developer proposes homes with the level of the top of the windows above road level whereas the existing buildings are much lower with the top of windows at road level. All homes should be at similar levels to form a suitable street setting.

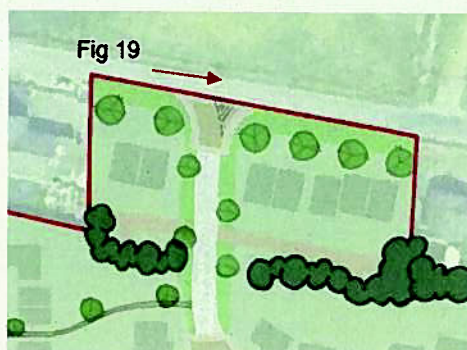


Figure 19 Artists Impression of development facing the Lang Stracht





5. KCC accept that there will be a development on this site, but insist that it respects the setting and must minimise the impact it has on the area. If the access is provided and a street is formed then this will have a huge impact local residents and the way they use their homes. Would the owner of the above home be asked to remove the fence? Obviously there are privacy issues with passing people and vehicles.
6. Kingswells insist that there is no access onto the Lang Stracht by vehicles and the development preserves rural feel for people using the Lang Stracht. Houses should be setback from the Lang Stracht to preserve privacy of new home owners and to allow walkers to use the Lang Stracht without the feeling of imposing on residents. This could be achieved with a green space adjacent to the road.
7. Discussions with Masterplanning team indicate that access to homes adjacent to the Lang Stracht would be from inside the development and there would be no driveways off the Lang Stracht. This should be formalised in this Masterplanning document by making this a requirement.
8. Views from new homes on the Lang Stracht built with top of window levels at road level to blend with existing homes would be limited considering the fields on the opposite side of the street rise high above road level. Figure 24 shows strategic tree planting to the rear of these homes which will also restricts the views to the south and means these homes are isolated from the remainder of the development.



2.2 Both accesses from Fairley Road

KCC believes the straight stretch of Fairley Road can accommodate two access routes to the site. There is good visibility the full length and fewer gradients to contend with than an access via the Lang Stracht. With the additional area of development, shown in purple on

Page 33 of the Masterplan document, the proposals for two accesses from the Fairley Road become even more viable.

The developer has stated that ACC would not accept a crossroads with the road which connects Fairley Road to the Kingswells perimeter road. However, ACC Roads Department has stated that a crossroads can be acceptable as there are engineering solutions for any issues that may exist. The actual position of the southern junction must not infringe on the GSN area at the southern edge of the site, and could be north of the existing T junction.

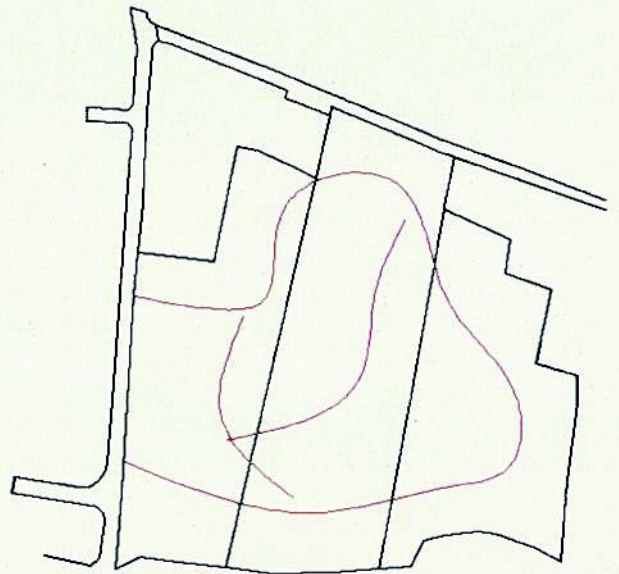
In summary, there is no engineering reason for excluding two access roads off Fairley Road. Increasing the frontage onto Fairley Road by including the purple area makes this option even more viable.

2.3 Internal road

KCC strongly objects to the internal road layout. There is a "dead-end" road which SMG stated will "lead to future development in the adjoining fields." This is simply not acceptable.

If the area within the site boundary is to be developed as one master planned site then the obvious layout is a main loop with some minor roads crossing the loop. The exact layout is difficult to determine if no information is provided on the house types.

The proposed layout appears to reflect the ownership of land by the two developers involved (first field Graham Homes and the other two fields Stewart Milne Group). The introduction of the additional purple area owned by Raich would require the Masterplan to reconsider the road layout. Piecemeal development would not be acceptable to KCC. We would prefer the area to be developed as one to produce the best development possible. Two access roads from Fairley Road obviously impacts on the potential of the Graham Homes area. An agreement between the developers could compensate those that may lose out due to the location of access roads. This would allow good planning to be the driving factor for this development.



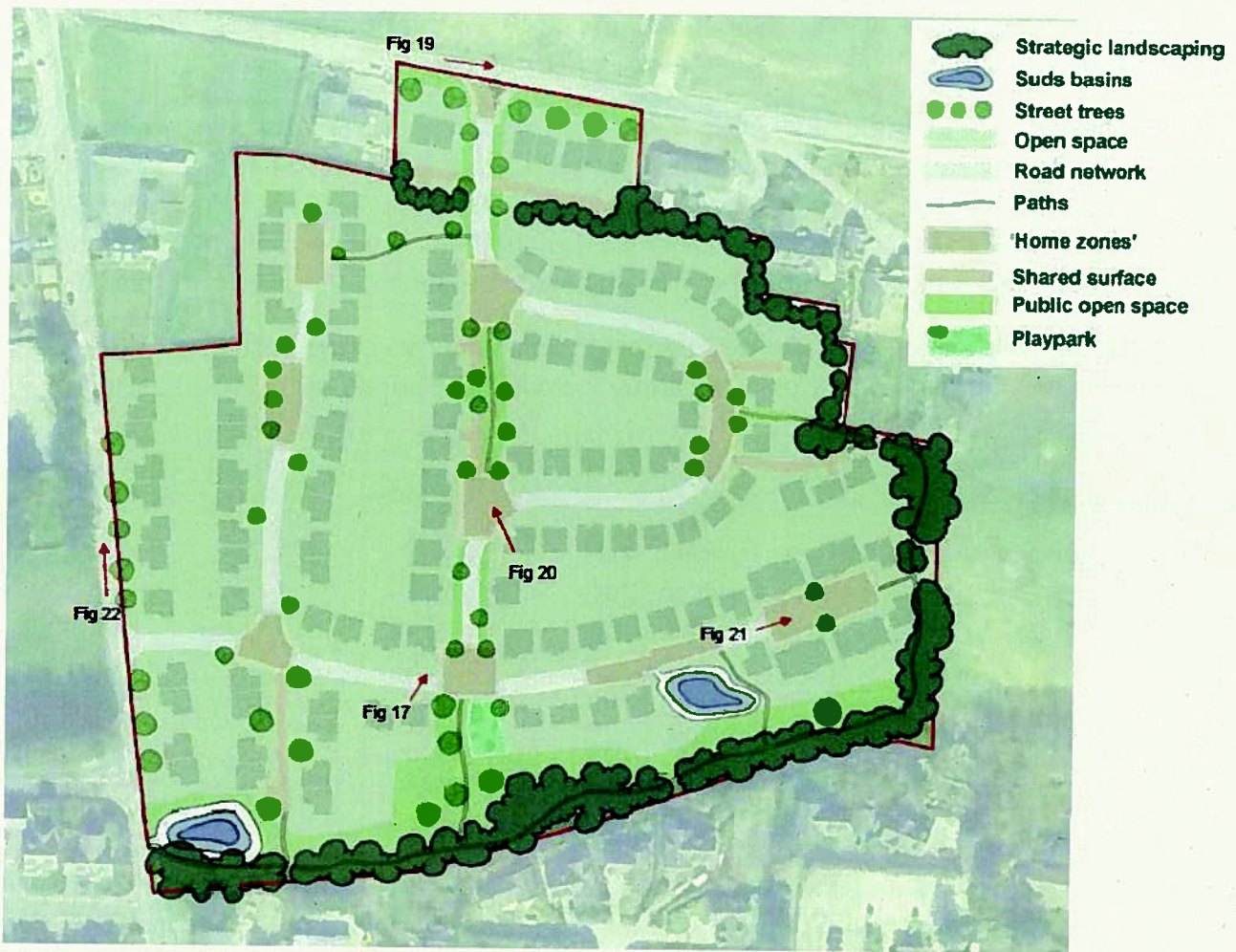


Figure 24: Indicative Block Layout Plan

3. Forming Streets

3.1 Houses facing Fairley Road

KCC finds this acceptable as it integrates the development with the existing community. The homes may potentially be separated from the Huxterstone development but they would be integral with the existing homes in Fairley Road.

3.2 Making the Lang Stracht a Street

Many of the points made in Section 2.1 apply here.

KCC strongly objects to the concept of making the Lang Stracht a street. The seven houses facing the Lang Stracht would be excluded from the rest of the development and would be out of keeping with the existing houses on the Lang Stracht. These homes ("The Manse" and "Morven") have set a precedent as they have their backs to the Lang Stracht and the fronts of the houses have views over the valley.

Proposed houses on the Lang Stracht would face onto open fields. The area opposite the development site was proposed at the Main Issues stage of the Local Development Plan and was deemed as unsuitable for housing. Development of this type would receive strong objection from the community.

The community do not want the Lang Stracht to be a 'street' as this would eventually result in the Lang Stracht being re-opened and Kingswells would become a 'rat-run'. The importance of the Lang Stracht in providing efficient bus access to the city centre will be required when Prime Four development starts in September / October 2013. This development was justified in part due to the Park & Ride service. Recent changes to the Park & Ride will have to be readdressed when Prime Four comes on-stream. Making the Lang Stracht a street will affect the efficiency of the bus service.

4. House types –

KCC has concerns about the proposal for more West 1 house types. The homes are extremely small and do not offer home owners a good quality of life. SMG argues that the existing West 1 units sell well and this shows that there is a demand for this type of home.

The reality and impact on people living in homes with so many limitations is yet to be seen, but the quality of life for these home owners must be in doubt. Some of the limitations are:

- storage space is at a premium
- the bedrooms do not provide sufficient space to walk around the bed
- second bedrooms have no space to hang clothes
- neither bedroom has space for a chest of drawers.
- eating space is restricted – so plates on laps is likely to be preferred.
- gardens are public space so there is no room for a shed to store toys, bikes or other sports equipment.

The resale of these properties must also be in question when new buyers will not enjoy the 'low deposit' that attracted the original owners to these properties.

KCC realises that the above is not a planning issue, but ACC has a duty of care to future residents attracted to the area and this should be taken into consideration. We need to develop communities and not just houses for the short-term gain of the developer. Housing development is a legacy for future generations and should be something in which we can be proud. The provision of West 1 housing should be very seriously questioned. The masterplan must take quality of life and health issues into account.

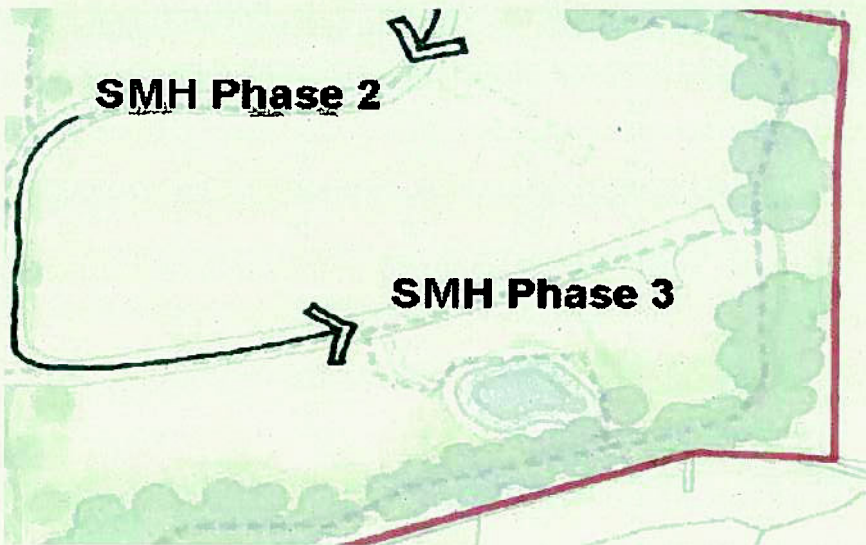
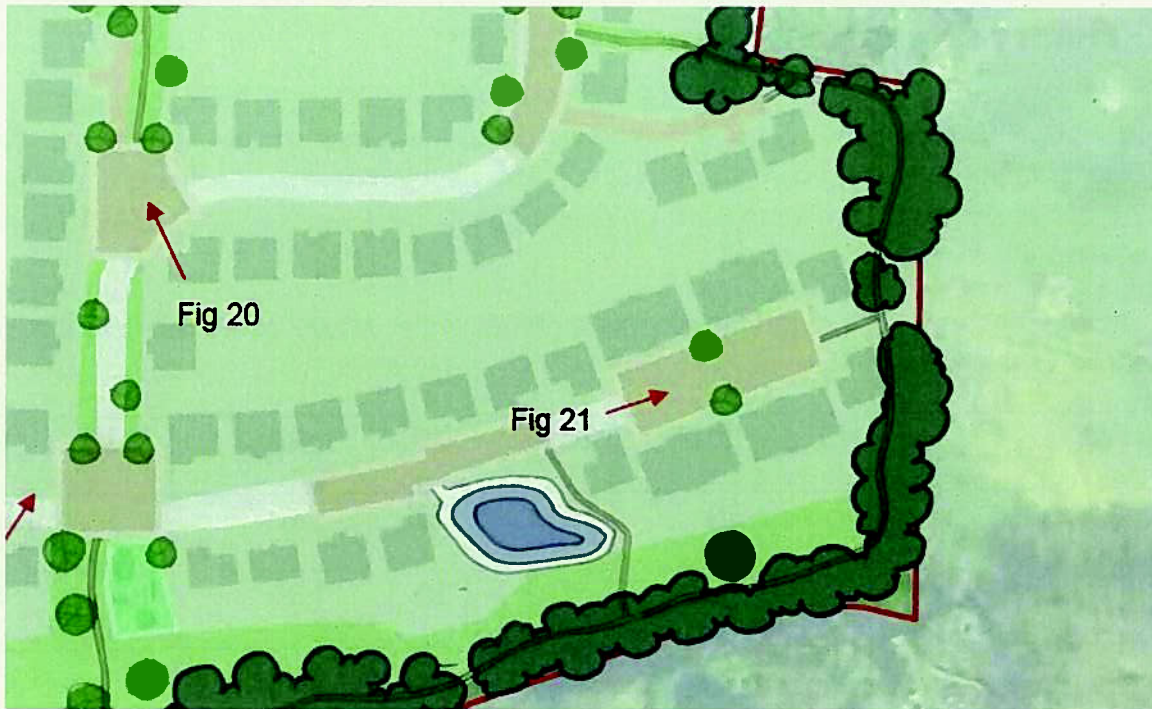
SMG claim that the West 1 type home is designed for people who are downsizing. KCC would question if anyone with many possessions could move into these homes.

We are also aware that many new owners use these homes to be close to Aberdeen Monday to Friday and they go to their real homes in outlying areas at the weekend. KCC is concerned about the effect this may have on community life.

KCC is disappointed that there is no plan for bungalows for elderly or disabled people. This is seen as ageism.

4.1 Affordable housing

It would appear that the affordable housing for the site will be in the form of the blocks of 8 homes used at SMG West 1 development. KCC object to the use of these homes as they do not meet the criteria that affordable homes should fit seamlessly into the development and should not be identifiable. These blocks are distinctive. We also object to the affordable housing being delivered as the last homes to be built in Phase 3..



5. Primary school provision

Kingswells Primary School is facing a crisis. The ACC school roll forecast suggests that the school will go over capacity in 2015. These figures are based on the 72 homes at the West One development being completed in 2013. The development was actually completed in 2012. Consequently, the school will go over capacity next year rather than ACC's prediction of 2015.

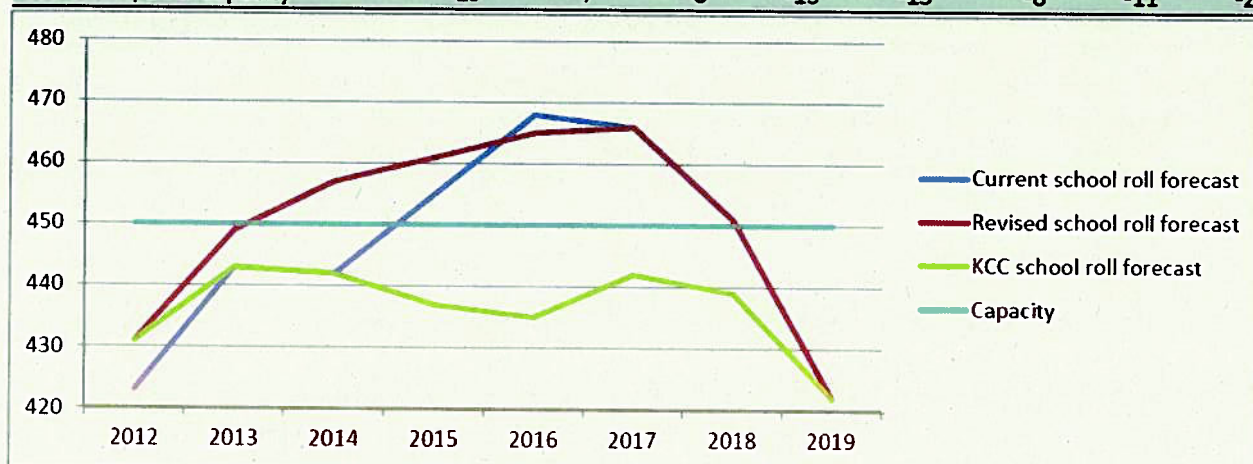
The proposed phasing of the development results in the school going over capacity as shown in the following graph.

Phasing	2013	2014	2015	2016	2017	2018	2019
In Masterplan	20	30	30	30	10		
KCC Proposal				10	30	40	40

KINGSWELLS PRIMARY SCHOOL ROLL FORECASTS

	2012	2013	2014	2015	2016	2017	2018	2019
Current Housing Forecast	27	27	0	60	60	0	0	0
Cumulative	27	54	54	114	174	174	174	174
Revised Housing Forecast	54	20	30	30	30	10	0	0
Cumulative	54	74	104	134	164	174	174	174
Difference in cum housebuild	27	20	50	20	-10	0	0	0
Roll adjustment	8	6	15	6	-3	0	0	0
KCC Phasing	54	0	0	0	10	30	40	40
Cumulative	54	54	54	54	64	94	134	174
Difference in cum housebuild	27	0	0	-60	-110	-80	-40	0
Roll adjustment	8	0	0	-18	-33	-24	-12	0

	2012	2013	2014	2015	2016	2017	2018	2019
Current school roll forecast	423	443	442	455	468	466	451	422
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KCC Under/Over capacity	-19	-7	-8	-13	-15	-8	-11	-28



KCC propose the above phasing which will allow the development to proceed without a major impact on the education of the Kingswells Primary Children.

The above table and graph show the roll broken down.

KCC have shown that it is possible to phase the building works to avoid impacting the education of the primary school pupils by running the school at 'over capacity'. We also request that phasing should not be fine-tuned to run the school at capacity as this would increase the likelihood of having one or more composite classes which impacts on the 'actual capacity' figures that are possible.

5.1 Previous Errors Made With School Roll Figures

The West One Development, which is nearing completion, was approved with 72 homes rather than the 50 homes identified in the Local Development Plan because ACC Planning Department and Elected Members were provided erroneous capacity figures (540 instead of 450) by ACC's Education Department.

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Within the Appendix 5 to this report, (which was agreed), the revised capacity for Kingswells School is clearly shown as 450. Unfortunately, within some of our supporting internal paperwork, this number appears to have been wrongly transposed as 540, (rather than 450). It therefore seems that, when one of my colleagues was asked at short notice by Dr Maggie Bochel, Head of Planning & Sustainable Development, on the morning of a Development Management Sub-Committee, about the capacity of Kingswells School, he inadvertently quoted the 540 figure, rather than the correct figure of 450.

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5.2 Size of the Development

The Local Development Plan has identified the West Huxterstone development is suitable for 120 homes

However, the Indicative Block Layout Plan (Figure 24) on Page 20 of the West Huxterstone masterplan shows 114 houses plus 4 blocks of either 6 (=24) or 4 blocks of 8 (=32). This suggests that the final total number of homes on the site could actually be 138 or 146.

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The problems are not helped by ACC's insistence on using the formula of 0.3 children per new home. This approximation may broadly apply for a large number of homes across Aberdeen as a whole but could clearly be wrong for smaller developments like West Huxterstone. It is significant that 5 bedroomed family homes would equate to just 1.5 children and that one-bedroomed affordable homes are completely discounted. Even a small discrepancy in the projected number of children could easily push Kingswells Primary over capacity.

Kingswells has had an abysmal history of poor decision-making by ACC over primary school provision. More and more new homes were built with no provision for the education of the growing population of children moving into the area. The primary school was forced into using general purpose rooms as classrooms, then the old Fairley Road school was reopened as a nursery and portacabins were installed for P1 and P2. As a consequence of the split site and overcrowding, the education of Kingswells children was severely compromised for years. The school has already been extended and the playground had to be sited outwith the school grounds. The school site footprint is now fully utilised with buildings and with a staff car park which is inadequate. Often staff park in residential streets around the school. There is no further suitable land available for portacabins or an extension

5.4 Conclusion on School Issues

The problems are serious and have been caused by miscalculations by personnel within ACC and the insistence of maximising development beyond the numbers identified in the Local Development Plan. The Planners and Developers have a duty of care to the community to 'get it right'.

Kingswells community will not tolerate overcrowding at the primary school again.

We demand that the building of houses at West Huxterstone is delayed / phased so that the school can accommodate the children without detriment to their education.

Also, with the phasing suggested by KCC, the developers can then complete the whole development over 4 years instead of 5. Any additional housing above 120 would extend the period of development.

Neither ACC nor the Developer should compound previous errors made in Kingswells. A slight delay on 120 homes will not have a big impact on the delivery of Aberdeen housing, but it could have a big impact on the education of Kingswells children. Developers have alternative sites to develop in the short term.,

6. Green Space Network and SUDS ponds

The GSN area along the Den Burn should be protected and enhanced to encourage biodiversity. Care should be taken to maintain the flow and cleanliness of the burn.

If SUDS ponds are located in the GSN area, as shown on the plans, then they must not be unattractive and lifeless "dry basins". The "SUDS pond" (also in Kingswells) shown in the first photo below shows exactly what we don't want.



The SUDS area at West 1, at this stage, is no better although we have been assured by SMG that it has still to be completed.

The footpath surrounding the SUDS area at West 1 is far too wide and takes up too much of the GSN space. We do not want to see something similar at Huxterstone.

The other photographs show examples of proper, bio diverse SUDS ponds at Drumoak which have water in them for most/all of the year. We see no good reason why something similar could not be achieved at Huxterstone.



7. Landscape and environmental issues

We welcome the commitment in the Masterplan to preventing skyline development and terracing of the site, to incorporate stone dykes (built in traditional manner – no mortar) as far as possible, and to safeguard and enhance the ecological habitat of the Den Burn.

However, we dispute the contention in Section 2.3 (page 5) that “There are no landscape features of any significant importance within the site.” The entire Den Burn Valley, which includes this site, is a glacial outwash channel dating from the last Ice Age which is of considerable geomorphological interest. This interest has been recognised, in part, by the creation of the Den of Maidencraig local nature reserve further down the valley. The rolling green valley immediately east of the site, running towards Maidencraig, is a highly attractive landscape feature clearly visible from the A944. Fully effective tree screening of the eastern boundary of West Huxterstone is therefore crucial in maintaining the visual amenity of the rest of the valley.

On the Indicative Block Layout Plan (page 20), it is very clear from the alignment of the spine road, the positioning of houses and the gap in the tree screening on the eastern boundary that the developer intends to use this road as a main access to future developments in the fields to the east of the current site. Previous public consultations have shown that the Kingswells community will simply not accept any further building in the Den Burn Valley and KCC will resist very strongly any future plans in this direction. It is highly disappointing that ACC have not made a long-term commitment to protecting a unique and beautiful landscape feature within the City of Aberdeen. **KCC strongly objects to any facility provided in the Masterplan for future eastwards development. Either the spine road should be redirected or its possible future extension should be halted by buildings and an unbroken tree belt.**

The Internal Connections map (page 11) implies that a footpath created along the Den Burn within the site might be extended eastwards in future. This extension would be both unnecessary and undesirable. There are already core paths running east-west to the north and south of the Den Burn Valley and no more are needed. A new core path along the Den Burn towards Maidencraig would compromise the biodiverse area of marshland further along the valley.

It is planned to include SUDS basins within the Green Space Network along the Den Burn. Other SUDS basins created recently at Old Skene Road and West 1 have so far failed to promote biodiversity at all as they are simply grassy hollows that never retain any standing water. At West 1, it is unacceptable that the SUDS basin actually replaced a small area of marshland that had some biodiversity value. This contrasts with the SUDS basins being developed at the Prime 4 business park which have been specially designed to hold some standing water throughout the year. **The SUDS areas at West Huxterstone must deliver on the promise to promote “ecological value and opportunities for wildlife creation”.**

8. Public consultation

On Tuesday 11 September the EPI Committee agreed the following:

- (i) to delay consideration of the report until the next meeting of the Committee, during which time officers to consult with Kingswells Community Council on the Masterplan; and
- (ii) that officers be instructed to report back to the next meeting of the Committee on the masterplanning process and to also receive a presentation in this regard.

On Monday 17 September, two members of KCC met with Sandy Beattie (Masterplanning Department) and Harry Campbell (Planning Department). The officers stated that West Huxterstone would go out to public consultation as from 19 September. KCC members informed the officers that this was not what had been decided at committee and advised that consultation should only be with KCC at this stage. Sandy Beattie said he would check with the EPI committee minutes and get back to us.

In the meantime, KCC informed our Local Elected Members. The minute was checked and the Council did not instruct a public consultation be undertaken. KCC held off informing members of the public of this public consultation through the Community Newsletter.

The officers have not responded as yet and we still do not know if this was an error - or if we have entered a true public consultation.

However, the consultation has not appeared on the "ACC current consultations" web page and is hidden on a difficult-to-find "masterplanning" link. This public consultation was for a period of 4 weeks. Masterplanning public consultation should be of 6 weeks duration.

Therefore the "public consultation" on West Huxterstone is deeply flawed.

- The EPI committee did not instruct this public consultation.
- A masterplan public consultation should be for a period of 6 weeks, not 4.
- The public consultation has not been properly publicised.
- KCC could not inform the public through our community newsletter as ACC officers did not respond to our request for confirmation.

From: "Brooks, Liz S" [REDACTED]
To: <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 11:40
Subject: OP42 - West Huxterstone masterplan.

I understand that Aberdeen City are currently consulting in the masterplan for development at the West Huxterstone area of Kingswells (OP42).

As a Kingswells resident and member of the Parent Council at Kingswells Primary School, I would like to raise a number of reservations about this plan:

1. Infrastructure - Education: This development envisages up to 120 homes, most of which would be family homes, meaning that a significant number of new pupils would be looking for places at Kingswells Primary, which as I understand, does not currently have such spare capacity. There is much speculation in the masterplan that capacity may exist. However, until such spare capacity in Kingswells Primary School is confirmed as being available (using the correct school capacity), I would therefore object to this development.
2. Infrastructure - Roads: The main road around Kingswells is already exceptionally busy (at times at a standstill all that way for several miles towards Newhills), with many people using local roads through Kingswells as a short cut, in particular, many use Fairley Road for this purpose. The addition of 120 homes in this area will lead to a huge additional amount of traffic which will have a detrimental effect on the whole of Kingswells. I am concerned for the safety of schoolchildren and elderly residents (with a nursing home on the only road going through Kingswells). Aberdeen City Council should actively consider appropriate ways of restricting the ability of Kingswells residents being put in danger by the continued through traffic caused by those using Kingswells to lessen the time spend in a queue at the traffic lights. This is now worsened by the building works on the new office development adjacent to the Park and Ride site, and will become considerably worse once these office are occupied. Despite some of the assurances that this development will cause an imperceptible impact on traffic, I do not believe this to be the case - 120 family homes will cause a significant impact to an already struggling traffic area. Until Aberdeen City Council actively puts in practice means to lessen the traffic 'cutting through' Kingswells in this way, I would therefore object to this development.
3. Drainage: The land at the southerly end of this area is prone to flooding, due to the location of the Den Burn. I presume that this area will be afforded protect to protect the plants and wildlife in that area and will not be built on, but a risk of flooding would remain. Is it good practice to build near an area with a known risk of flooding?

Regards,
[REDACTED]

Our ref: PCS/122492
Your ref:

Louise MacSween
Aberdeen City Council
Planning and Sustainable Development
Business Hub 4
Marischal College
Broad Street
Aberdeen
AB10 1AB

If telephoning ask for:
Alison Wilson

15 October 2012

By email only to: LDP@aberdeencity.gov.uk

Dear Ms MacSween

Consultation OP42 West Huxterstone, Kingswells Masterplan

Thank you for your consultation letter of 17 September 2012 which SEPA received on 19 September 2012. We welcome this opportunity to comment on the masterplan for OP42. Please be aware that our advice at this stage is based on emerging proposals and we cannot rule out potential further information requests as the project develops.

We note under the Aberdeen Local Development Plan 2012 that site OP42 provides an opportunity for development of 120 homes. As you will be aware we have provided advice on this site under the Local Development Plan consultation and the Kingswells Development Framework. We consider that that the majority of our previous comments and recommendations have been considered and incorporated into the masterplan therefore we have no significant concerns regarding the principle of this development proposal.

However for the avoidance of doubt we would recommend that the following issues are addressed prior to the submission of a planning application to avoid unnecessary delay and/or objection from SEPA:

- Foul and Surface Water Drainage
- Pollution Prevention and Environmental Management
- Protection of the water environment

Further more detailed advice is provided below for the applicant.

1. Waste water drainage



Chairman
David Sigsworth

Chief Executive
James Curran

Aberdeen Office

Inverdee House, Baxter Street
Torry, Aberdeen AB11 9QA
tel 01224 266600 fax 01224 896657
www.sepa.org.uk

- 1.1 Drainage is a material planning consideration and will be assessed as part of your planning application in line with PAN 79 Water and Drainage and Policy NE6 – Flooding and Drainage in the Aberdeen Local Development Plan 2012.
- 1.2 Where there is a public sewerage system, waste water drainage from development within and close to the settlement envelope should be directed to that system, as the same level of environmental protection is unlikely to be achieved if individual or groups of privately owned drainage schemes are set up within towns and villages. If the system has insufficient capacity, then early dialogue with Scottish Water will be required to determine if works are planned to overcome this problem, or what developer pro-rata contributions will be necessary to remove the constraint.
- 1.3 We note from the Masterplan document that the development will be connected to the existing public drainage system serving Kingswells. If there is limited public sewerage infrastructure, given the scale of this development we would still expect the development of strategic infrastructure to adoptable standards. Contact should be made with Scottish Water to determine the standards required to ensure adoption of new infrastructure. For further guidance on waste water drainage please refer to our Policy and Supporting Guidance on Provision of Waste Water Drainage in Settlements.

2. Surface water drainage

- 2.1 The treatment of surface water runoff by sustainable drainage systems (SUDS) is a legal requirement for most forms of development, however the location, design and type of SUDS are largely controlled through planning. As responsible authorities under 2(2) Water Environment and Water Services (Scotland) Act 2003, planning authorities are required to work to prevent deterioration in and promote improvements in Scotland's water environment. Ensuring development sites are serviced with appropriate SUDS is one of the key ways in which SEPA consider planning authorities can discharge these duties. We encourage surface water runoff from *all* developments to be treated by SUDS in line with Scottish Planning Policy (Paragraph 209), PAN 61 Planning and Sustainable Urban Drainage Systems, PAN 79 Water and Drainage and Policy NE6 – Flooding and Drainage in the Aberdeen Local Development Plan.
- 2.2 We are pleased to note from the masterplan document that several areas have been allocated for SUDS and that, as per section 7 of the document, consideration has been given to individual SUDS features and that the combination of filter trenches/grass swales and extended detention basins will provide the whole development area with the required two levels of treatment.
- 2.3 We highlight that best practice require two levels of treatment for all hardstanding areas including roads. An exception is run-off from roofs which requires only one level of treatment. Best practice requires the first level of SUDS treatment to take the form of source control. The SUDS treatment train should be followed which uses a logical sequence of SUDS facilities in series allowing run-off to pass through several different SUDS before reaching the receiving waterbody. Further guidance on the design of SUDS systems and appropriate levels of treatment can be found in the CIRIA C697 manual entitled The SUDS Manual. Advice can also be found in the SEPA Guidance Note Planning advice on sustainable drainage systems (SUDS). Please refer to the SUDS section of our website for details of regulatory requirements for surface water and SUDS. Comments should be sought from the local authority roads department and the local authority flood prevention unit on the acceptability of post-development runoff rates for flood control.

- 2.4 Comments from Scottish Water should be sought where the SUDS proposals would be adopted by them. We encourage the design of SUDS to Sewers for Scotland Second Edition standards and the adoption of SUDS features by Scottish Water as we are of the view that this leads to best standards and maintenance.

3. Pollution prevention and environmental management

- 3.1 One of our key interests in relation to major developments is environmental management and pollution prevention measures during the periods of construction, operation, and any demolition or restoration works. We advise that the applicant should, through the planning submission, systematically identify all aspects of construction site work that might impact upon the environment, potential pollution risks associated with the construction proposals and identify the principles of preventative measures and mitigation. This will establish a robust environmental management process for the development which will help us to assess the environmental impact of the proposals prior to determination. In addition, this information can provide the basis for a more detailed environmental management plan and construction method statements, which are likely to be requested as planning conditions. Details of the specific issues that we expect to be addressed are available on the Pollution Prevention and Environmental Management section of our [website](#).

4. Space for waste management provision within site layout

- 4.1 Scottish Planning Policy Paragraph 215 states that “residential, commercial and industrial properties should be designed to provide for waste separation and collection.” In accordance with this policy, Policy R6 in the Aberdeen Local Development Plan and PAN 63 *Waste Management Planning*, space should be designated within the planning application site layout to allow for the collection, segregation, storage and possibly treatment of waste (eg individual and/or communal bin stores, composting facilities, and waste treatment facilities) Please consult the council’s waste management team to determine what space requirements are required within the application site layout.

5. Site waste management

- 5.1 We are pleased to note that the developers will adopt sustainable building practices and use locally sourced materials and optimise the use of recycled materials whilst minimising construction waste. Guidance and information on waste prevention and waste minimisation can also be found at our [website](#).

6. Protection of the water environment

- 6.1 We note from the Masterplan document that the Kingswells Development Framework required any future masterplan for the site to include the safeguarding and enhancement of the ecological habitat of the Burn and that in response one of the aims of the masterplan is to achieve the safeguarding and enhancement of the ecological habitat of the Den Burn. In our response to the Local development Plan Consultation we highlighted the following issues as relevant to any masterplan being developed for the site:
- Kingswells developments may impact on Bucks Burn and Den Burn at poor ecological potential because of sewage pollution and alterations to beds and banks.
 - SG 9.12 highlights need for development proposals to not cause detriment to water quality or ecology. We support this statement but recommend it be expanded to take account of the existing water features within the site, the pressures which apply to these features and

should direct developers to look for opportunities to protect and improve the water environment.

- 6.2 Where developments cover a large area, there will usually be opportunities to incorporate improvements in the water environment required by the Water Framework Directive within and/or immediately adjacent to the site either as part of mitigation measures for proposed works or as compensation for environmental impact. We encourage applicants to seek such opportunities to avoid or offset environmental impacts. Improvements which might be considered could include the removal of redundant weirs, the creation of buffer strips and provision of fencing along watercourses. Fencing off watercourses and creating buffer strips both helps reduce the risk of diffuse water pollution and affords protection to the riparian habitat.
- 6.3 The Den Burn borders the southern boundary of the site and is enclosed within LDP Green Space Network (GSN). It enters the site just above the south-west corner. The sharp right and left turns it makes (looking downstream) could be softened, if this corner of the site is made part of the site's greenspace strategy. There is an opportunity here to enhance the watercourse and its riparian edges. Consideration could be given to options for doing this such as potential minor realignments to create a more natural form. This would also help restore more natural bankside habitat for wildlife.
- 6.4 We would welcome the opportunity to undertake early discussions with the applicant about any opportunities for improving the watercourse in this area.
- 6.5 In addition you should also ensure that any proposals are in accordance with Aberdeen City Councils Buffer Strips Supplementary Guidance.

7. Flood Risk

- 7.1 As noted the Den Burn lies to the south and south east of the site. Some reference is made to flood risk with the masterplan. The site should be assessed for flood risk from all sources (fluvial (water course), pluvial (surface water), groundwater, sewers and (blocked culverts)) in line with Scottish Planning Policy (Paragraphs 196-211). Further information and advice can be sought from your Local Authority technical or engineering services department, Scottish Water and from our website. Our Indicative River & Coastal Flood Map (Scotland) is also available to view online. If a flood risk is identified then a flood risk assessment (FRA) should be carried out following the guidance set out in the Annex to the SEPA Planning Authority flood risk protocol. Our Technical flood risk guidance for stakeholders outlines the information we require to be submitted as part of a FRA, and methodologies that may be appropriate for hydrological and hydraulic modelling. Further guidance on assessing flood risk and planning advice can be found at our website.

8. Advice for the applicant

- 8.1 Details of regulatory requirements and good practice advice for the applicant can be found on our website at www.sepa.org.uk/planning.aspx. If you are unable to find the advice you need for a specific regulatory matter, please contact a member of the operations team in your local SEPA office at:

Inverdee House, Baxter Street, Torry, Aberdeen, AB11 9QA, tel. no. 01224 266600.

If you have any queries relating to this letter, please contact me by telephone on 01224 266656 or by e-mail to planning.aberdeen@sepa.org.uk.

Yours sincerely

Alison Wilson
Planning Officer
Planning Service

Disclaimer

This advice is given without prejudice to any decision made on elements of the proposal regulated by us, as such a decision may take into account factors not considered at the planning stage. We prefer all the technical information required for any SEPA consents to be submitted at the same time as the planning application. However, we consider it to be at the applicant's commercial risk if any significant changes required during the regulatory stage necessitate a further planning application and/or neighbour notification or advertising. We have relied on the accuracy and completeness of the information supplied to us in providing the above advice and can take no responsibility for incorrect data or interpretation, or omissions, in such information. If we have not referred to a particular issue in our response, it should not be assumed that there is no impact associated with that issue. If you did not specifically request advice on flood risk, then advice will not have been provided on this issue. Further information on our consultation arrangements generally can be found in [How and when to consult SEPA](#), and on flood risk specifically in the [SEPA-Planning Authority Protocol](#).

From: Kingswells School PTA [REDACTED]
To: "ldp@aberdeencity.gov.uk" <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 20:06
Subject: Countesswells Development

Dear ACC

As a resident of Kingswells with 3 children at the school I am emailing to voice my concern over the proposed development at West Huxterstone.

Given the current role of the school being around 430 and the full capacity of the school being 450 I have huge concerns about another development of 120 family homes in the village. The current Primary 7 group is around 47 children, as they leave the school in the summer the P1 intake is expected to be similar to this year (c.65). Without any additional housing at all this will take the school to capacity.

Unless there is another primary school built in the vicinity in the immediate future it is inconceivable that these houses can be built with Kingswells Primary as its catchment school.

I trust you will take my thoughts and those of other people in Kingswells into consideration.

Kind Regards

[REDACTED]

From: Gary Catto [REDACTED]
To: <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 20:12
Subject: Kingswells developments

Dear sir/madam,

I am writing with concern that the two planned developements in and near Kingswells will have a major impact in the school as well as the surrounding facilitys and roads. The West Huxterstone Development in Kingswells will see an additional 120 family homes being built in Kingswells in the next 12 months.

Given that the school capacity is 450 (exc nursery) and our current role is 431 (exc nursery), the school will be pretty much at capacity at the beginning of the next academic year without any additional housing in the village. I feel this and the Countesswells development will have an impact on the school and the pupils already attending.

My oldest son is in primary 2 and youngest in Anti-pre school and I can already see the impact of the school at capacity. Any further developments to the expansion of the village will need to have plans to either expand the current primary or build an additional primary school for the village. We also need a secondary school in the area if Counteswells goes ahead.

I feel at the moment that the items such as the state of the roads, the very very slow broadband speeds and the lack of other shopping / food facility's in Kingswells is bringing down the village's appeal to young family's. The traffic is already within the village and adding 120 more homes is roughly putting another 240 cars into the village.

I agree there has to be a compromise but you cannot keep on adding homes and more homes and seeing if the village and it's school will cope because quite frankly at the moment, it wont.

I look forward to your reply.

Regards,

Gary Catto.

From: Yvonne Wallace [REDACTED]
To: <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 20:14
Subject: West Huxterstone Masterplan

Hi,

I am just emailing with regards to the proposed development of 120 new houses in Kingswells. I don't believe the current amenities in kingswells most notable the school and doctors could cope with more families in the area.

Regards
Yvonne

From: Vinny & Annr [REDACTED]
To: "ldp@aberdeencity.gov.uk" <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 20:17
Subject: Op 42 west huxterton kingswells

I wish to register my extreme concern regarding the plan to expand kingswells yet again. The school is pushed as it is, my son cannot get a place in nursery as it is. We were told that the last development near the church would not qualify to have children at kingswells primary yet children are in nursery and my son is on the waiting list! Unless you can guarantee an extended school with more places in the next year to cope then you should not consider more housing. Get the infrastructure right first for a change. I have to register my strong objection to the development.

Yours sincerely

Anne Goss
Derbeth Grange

Sent from my iPhone

From: Elaine Grosvenor [REDACTED]
To: "ldp@aberdeencity.gov.uk" <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 20:35
Subject: Op42

Please reconsider and do not build any more houses as detailed in OP42. The school and nursery are full to capacity, the roads are congested and with all the new offices it will be chaos and unsafe for children, If the WPR should be top of the priority list to ease transport within Aberdeen city.

Please do not build any more houses in Kingswells

Elaine Grosvenor
Sent from my iPad

From: "Muir, Jim (JimMuir)" [REDACTED]
To: "ldp@aberdeencity.gov.uk" <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 21:26
Subject: Public Consultation for OP42 - West Huxterstone Masterplan.

Dear Members of Aberdeen City Council, Development Plan Team, Planning and Sustainable Development, Enterprise, Planning and Infrastructure,
This email refers to the Public Consultation for OP42 - West Huxterstone Masterplan

I am contacting you on behalf of residents of the access and buses only section of the Langstracht

After many years of working with local councilors and officers at ACC, the residents, bus companies and increasing numbers of walkers, runners, cyclists and horse riders, all live in harmony.

Improvements which benefit all have been made incrementally; most recently there has been a "change of use" with the road now designated a bus lane, previously a bus gate.

However, having viewed the masterplan for OP42 we have concerns about the possibility of an entrance onto the Langstracht and the rationale behind certain houses in the development facing and directly accessing the road.

We all support the Park and Ride and bus lane initiatives, clearly ACC has put considerable resource into making the road work in practice, therefore it seems absolutely illogical to propose additional traffic should be directed onto this road.

In summary we urge ACC to fully consider the necessity of an entrance to the development on the Langstracht and for houses to face and directly access the Langstracht.

[REDACTED]

From: Tracy Johnstone [REDACTED]
To: "ldp@aberdeencity.gov.uk" <ldp@aberdeencity.gov.uk>
Date: 15 October 2012 23:16
Subject: West Huxterstone masterplan op42

I am a resident of Kingswells and my eldest son attends the local primary school which is just about at capacity already. The development at west huxterstone of 120 new homes will only put more pressure on the very limited resources within Kingswells for nursery school and primary school places. The village has not had sufficient infrastructure planning or development in the past to support such large increases in population going forward.

Rgds

Tracy Johnstone

Sent from my iPhone

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Loirston Development Framework
REPORT NUMBER:	EPI/12/200

1 PURPOSE OF REPORT

- 1.1 This report outlines the Loirston Development Framework, prepared as a strategy for the future development of land identified in the Aberdeen Local Development Plan (ALDP) as OP77, OP80 and OP78.
- 1.2 A summary document of the Development Framework is appended to this Committee Report. The full Development Framework for Loirston (November 2012) is a large document containing a lot of illustrative material and can be viewed by accessing the following link:

www.aberdeencity.gov.uk/masterplanning
- 1.3 A hard copy of the full Development Framework is available in the Members' Library (Town House), within the Planning and Sustainable Development Service at Ground Floor North, Marischal College, or by contacting the Masterplanning, Design and Conservation team.
- 1.4 The Development Framework has been made available to the Nigg, Kincorth & Leggart and Cove & Altens community councils for information prior to this report being considered for public consultation.

2 RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
 - (a) Approve the Loirston Development Framework as interim planning advice; and
 - (b) Agree for officers to implement the process to ratify the Development Framework as Supplementary Guidance. This will include a 4 week public consultation with results reported to Committee prior to submission to Scottish Government.

2.2 Definitions

‘Interim Planning Advice’ – this specifies that the Development Framework is in the public domain and available for the purposes of a formal City-wide consultation. As such it becomes a material consideration in the determination of any planning application. The duration of consultation will typically be 4 weeks, given that public engagement has taken place throughout the preparation of the Framework. This allows responses to be collated and reported back in the next Committee cycle.

‘Supplementary Guidance’ (SG) – this is adopted and issued by a planning authority in connection with a Local Development Plan, as a result any such guidance will form part of the Development Plan. Before adoption, the SG must be publicised and a period for representations to be made specified, this includes the targeting of key consultees and stakeholders who may want the opportunity to comment. Following the specified consultation period and as a result of comments received, relevant changes will be made to the final document before reporting back to Committee and subsequent submission to Scottish Ministers. After 28 days have elapsed, the authority may then adopt the guidance unless Scottish Ministers have directed otherwise.

3 FINANCIAL IMPLICATIONS

- 3.1 The site is in multiple ownership, however, the largest two landowners, Hermiston and Aberdeen City Council have worked together to involve all interested parties throughout the masterplanning process. A plan of the site ownership is shown in Section 2.2 (page 14) of the Development Framework. The Framework has been developed ‘landownership blind’ with regard to density, land use and character. However, where delivery of key infrastructure is critical, ownership has been considered to ensure proposals are pragmatic. As such the Council has a financial interest in the planning designation and future development of the site.
- 3.2 The developers have met the cost of preparing the Development Framework including all consultation and engagement work.

4 OTHER IMPLICATIONS

- 4.1 The Lochinch Farm Countryside Interpretation Centre is currently located within OP77. Any options for its future and potential relocation will be considered through close engagement with Aberdeen City Council’s Asset Management Service and during detailed masterplan and planning application stages.
- 4.2 The Development Framework reduces the risk of piecemeal and inappropriate development in the site area. Mixed use development

and the efficient use of land will contribute towards the Council's aim of promoting sustainable economic development.

- 4.3 Approving the Development Framework will contribute to efficiencies in determining future planning applications and a reduction in Council staff time to assess future detailed proposals. By encouraging acceptable forms and uses of new development to be agreed publicly, it allows more informed decision making to be made earlier, saving time and resources for staff, applicants and the public.
- 4.4 The Development Framework contributes towards a higher rate of new house building in the City, including 25% affordable housing provision in accordance with the Aberdeen Local Development Plan (2012).
- 4.5 In accordance with the Aberdeen Local Development Plan: Action Programme and the Infrastructure and Developer Contributions Manual, the Development Framework references how the development will seek to address requirements for cumulative and local transport infrastructure; public transport along with walking and cycling.
- 4.6 A Strategic Environmental Assessment has been undertaken on the Loirston site as part of the preparation for the Aberdeen Local Development Plan.

5 BACKGROUND / MAIN ISSUES

- 5.1 The Development Framework has been produced by Optimised Environments Ltd. (OPEN) on behalf of Hermiston Securities and Aberdeen City Council and aims to produce a new residential community to the south of the City on land surrounding Loirston Loch. A multi disciplinary project team has contributed to the preparation of the Framework, consisting of:

OPEN (Optimised Environments Ltd.) – Masterplanners
Knight Frank – Planning Consultants
Ironside Farrar – Environmental Consultants
Transport Planning Ltd. - Transport Consultants
Fairhurst Ltd. – Engineering Consultants
Media House – Public Relations
TPS Planning – Education Consultants.

Site Description

- 5.2 The Loirston site lies to the south of Aberdeen and on the border between the Aberdeen City and Aberdeenshire boundaries. The A90 Aberdeen-Dundee Trunk Road forms a boundary on the west side, while A956 (Wellington Road) both bisects the area to the south and forms an eastern boundary.

The site area falls into or abuts three Community Council catchments of Nigg, Kincorth and Leggart, and, Cove and Altens. The nearest neighbourhood centre with retail and community facilities is located in Cove. The site occupies an important strategic position within the context of Aberdeen City both as a gateway site and as a resource for open spaces such as Loirston Loch and Kincorth Hill. The site covers a total area of 186 hectares.

Evolving Context

5.3 Section 2.3 (Page 15-16) of the Framework document details a number of existing proposals and consents are already in place within the masterplanning area. For the purposes of this Development Framework these have been considered as fixed.

- Aberdeen Western Peripheral Route (AWPR)
- Aberdeen Football Club (AFC) stadium
- Cove Rangers stadium and training pitches
- Cove masterplan
- Aberdeen Gateway Site
- Balmoral Business Park

5.4 Framework options – due to current uncertainty regarding the final location for a new Secondary School and the delivery of various football clubs' proposals, a number of options to the 'Baseline' Framework have been developed which allow the Framework to accommodate various scenarios. As final decisions on these various components are not expected prior to the completion of the Development Framework, these options show flexibility and allow future decisions on these to be taken account of. Details of the options can be found in Section 5.20 (page 75-76) of the Framework document.

5.5 It should be noted that for the purposes of the 'Baseline Framework', a slightly altered scheme for the AFC is proposed which reflects the current land deal between AFC and the landowner. Although the Framework has suggested how elements could be altered for masterplanning the site as a whole, it is understood that such changes will have implications for planning permissions and legal agreements. An Option has been produced within Section 5.20 of the Framework illustrating how the baseline framework can be adjusted to accommodate the consented AFC layout with minor changes locally to specific development blocks.

Policy Summary

5.6 The Development Framework has been prepared following the Council's adopted Aberdeen Masterplanning Process and contributes towards the Scottish Government and Aberdeen Local Development Plan policy aims of raising design quality through the 6 key qualities of

creating successful places, namely – distinctive, welcoming, safe and pleasant, adaptable, easy to get to and move around, and resource efficient.

- 5.7 The Loirston site is identified in the Aberdeen Local Development Plan (2012) as Opportunity Site (OP77) for 1500 homes, 11 hectares of employment land, with the potential to accommodate a football or Community Stadium, and a Gypsy / Traveller site. The housing provision is split into land release phases, 1100 homes proposed for the period up to 2016 and 400 homes up to 2023.
- 5.8 The Local Development Plan also identifies land at Charleston (OP78) to the south Wellington Road for 20.5 hectares of employment land for the period 2024-2030; a 2 hectare extension to the existing employment site OP69 Aberdeen Gateway; and land at Calder Park OP80 for a new stadium and sports facilities for Cove Rangers Football Club.
- 5.9 This proposal helps bring forward land for development in the early phases of the Local Development Plan and help meet the targets and objectives in the Structure Plan, including that development should generally be no less than 30 dwellings per hectare as an average across the whole site.

Opportunity and Vision

- 5.10 The vision is to create a highly desirable and sustainable mixed-use community which is built around a strong green space network and allows easy access to its surrounding resources. The Development Framework details how the existing natural resources at Loirston offer the opportunity for an integrated new neighbourhood of Aberdeen. There is potential for high quality waterside development which enhances the edge of the loch whilst acting as a highly visible gateway to the City of Aberdeen. The vision for Loirston is based around 5 key aspirations:
1. A place tied to the existing landscape
 2. A place of green spaces
 3. A place of safe and green streets
 4. A place with a core density
 5. A place of distinctive character

Design Principles

- 5.11 The Framework sets out a number of key **urban design principles**, which have in turn informed the detailed proposals for the various character areas.
1. Active frontages
 2. Internal block accessibility

3. Legible street layout
4. Civic activities
5. Relationship to open space
6. Use of key gateway and corner features
7. Integration with natural environment
8. Variety of architecture
9. Coherent public realm

Character Areas

5.12 The Framework identifies 8 ‘character areas’, as named below. Within the document (pages 58-65) each area has a detailed set of ‘key character aspects’; however a short description of each area has been provided in this Report. Integration of these character areas is critical to the success of the overall development – particularly community integration (existing and new) and delivery of functional open space.

Character Area	Description
Lochside	Predominantly higher density residential with some potential for community and commercial uses at ground floor level. Area includes some employment uses allocated in the southern plots.
Primary Street / Core Settlement	Area is composed of development blocks which primarily front the Primary Street or Redmoss Road. Medium-high density residential with some commercial and local retail uses on ground floors fronting the Primary Street.
Burnside	Development blocks and areas which front onto the watercourse which runs into and out of Loirston Loch. Primarily employment, local retail and some medium density residential uses.
Sports and buffer landscape	Contains football proposal land and associated buffer landscapes. Potential for shared facilities between the sports facilities and any educational facilities which are determined to be delivered within the site.
Hillside	Area is centred on the lower south-facing slopes of Kincorth Hill consisting of low-density residential. A secondary core is located at the junction of Redmoss Road and the Primary Street with the opportunity for local retail and commercial uses in this location.
A90	Area comprises the south-facing development blocks which face towards the A90. Predominantly low-density residential, plot for the Primary School, extensive SUDs and green space corridor.
Existing residential and employment	Adjacent to the A90 with existing residential and employment uses. Little significant residential development is expected in this area, although plans for individual dwellings are currently under

	development in a number of locations within this strip.
Charleston	Identified within the ALDP as OP78 and allocated for future employment use. Detailed development has not yet been explored, although indicative access points and development parcels have been illustrated for the purposes of the Framework.

Environment and Open Space

- 5.13 Loirston Loch is a man-made water feature and designated a Local Nature Conservation Site (LNCS). The Loch and LNCS boundary are important for nature conservation, the buffer strips helps to protect the water environment and the LNCS designation, plus the site has high landscape value, and therefore, has been safe-guarded within the Framework, including opportunities for improved public access.
- 5.14 Whilst initial environmental studies suggest that potential environmental impacts can be avoided or minimised to an acceptable level through sensitive design, the proposed development requires that a statutory Environmental Impact Assessment is produced to support the Planning Permission in Principle application (PPiP), this will cover OP77 and part of OP80.
- 5.15 The agricultural history of the site means there are a number of landscape features which will be retained in the development layouts, including field patterns and consumption dykes. There is also the opportunity for landscape and channel improvements for the watercourses which enter and exit Loirston Loch.
- 5.16 The Framework has been designed with reference to ACC's Open Space Supplementary Guidance (March 2012). Section 5.4 of the Framework details how the key open spaces and their functions will be accommodated throughout the site. This includes:

Major Open Space	Loirston Loch and associated open space (c.17 ha)
Neighbourhood Open Space	Landscape strip along A90. Deculverted/improved stream corridor to the watercourse entering the Loch. Landscape buffer to the stadium and open space around power lines.
Local Open Spaces	Local areas have been identified where there are landscape features which are to be integrated and retained into the development (e.g. consumption dykes) and areas appropriate for civic or public functions.

Relationship with Loirston Loch

5.17 Loirston Loch is a significant part of the overall identity and landscape setting of the development. The LNCS boundary defines the buffer from the Loch into which development blocks should not encroach. Although views to Kincorth Hill from Wellington Road across the Loch are limited due to both the existing topography and the shelterbelt planting along Redmoss Road, the Framework document provides guidance and a series of '**design solutions**' for how the open space and built character should be considered. Section 5.18 and 5.19 of the Framework document provides further detail.

5.18 Design solutions

- High quality architecture to reflect the desire to create a dramatic new gateway to the City.
- Ensure sightlines to Loch are maintained.
- Gable ends to Loch punctuated by 'fingers' of open space.
- Limit development height beside the Loch and utilise a setback to upper floors.
- Public access and environmental improvements within the LNCS boundary, utilising and extending the existing path network
- Balance public access within sensitive areas.
- Consider use of undercroft parking solutions where topography and infrastructure requirements allow.

Housing

5.19 The proposals for Loirston include a full range of housing types and sizes including: apartments and flats, terraced units, townhouses, semi-detached and detached. All respective developers will work with the Council and the registered social landlords to provide the required level of **affordable housing** in accordance with the Aberdeen Local Development Plan – being no less than 25% of the total number of units.

5.20 There is a requirement for one Gypsy / Traveller site to be allocated in the Framework. Several potential sites have been identified within OP77 for a Gypsy / Traveller site of around 0.5 hectares, following guidance contained within ACC's Gypsy and Traveller Sites Supplementary Guidance (2010). Please refer to pages 49 and 52 of the Framework document for further detail. Such a site could be either transit or permanent, with detailed aspects of design, location, delivery and phasing to be agreed with ACC through the Phase 1 masterplanning process.

Access and Connectivity

5.21 Access to the Loirston site is primarily from Wellington Road with additional access from the proposed stadium junction, Redmoss Road and Wellington Circle. A full Transport Assessment (TA) Scoping

Report has been prepared to accompany the Development Framework and should be referred to for specific details relating to access and connectivity. A Transport Assessment Scoping Report has been submitted to Aberdeen City Council Roads and Transportation Officers for consideration and has informed the production of the Loirston Development Framework. A full Transport Assessment (TA) is currently being prepared by the developers transport consultants to support a future Planning Permission in Principle (PPiP) which examines the current and future transport matters associated with the proposed development.

5.22 The Framework document and the resulting development at Loirston is designed to provide the following:

- Well-connected and permeable network of streets.
- Integration with Core Path and existing path network, including aiding delivery of an aspirational Core Path.
- Integration with surrounding cycle network and promote opportunities for cycling.
- A clear hierarchy of streets which responds to different transport users. In order of decreasing vehicular traffic this includes (1) Entrance boulevard (2) Primary Street (3) Secondary Streets (4) Minor Streets.
- A set of parameters and character for each street type (as mentioned above).
- Accommodate frequent public transport through the site along the primary street corridor.
- Two access points onto Wellington Road and explore the opportunity at Redmoss Road for a public transport/pedestrian/cycle connection. Although detailed vehicular access points are to be fully confirmed as part of the TA.
- Contribution to Strategic Transport Fund to mitigate cumulative impact of development.

Infrastructure

5.23 Education provision – the ALDP Action Programme details the requirement to provide one new Primary School and potential to provide one new Secondary School. The project team have marked provision for a Primary School within the Framework.

5.24 The current position which the Scottish Government have agreed in principle would provide 2/3 funding for a single replacement school for Torry and Kincorth Academies. The Council would identify a preferred site, considering all possible sites, before undertaking a statutory consultation exercise on any proposal. A potential site for a Secondary School has been identified as an option in the Development Framework and will be reserved until a decision is made by ACC about the provision of secondary education in this area.

- 5.25 The detail and exact phasing of the school requirements for Loirston will also be informed by the Education Study which is currently being carried out by TPS Planning on behalf of the developers.
- 5.26 Health care provision – the ALDP Action Programme details the requirement for an extension at Cove Bay Health Centre to support an additional 3 new GP’s, 2 new dental chairs, and 1 new Community Pharmacy within the new settlement. These requirements will be funded from developer contributions and the phasing agreed with NHS Grampian.
- 5.27 Water and drainage – a full list of requirements will be identified following completion of a detailed Drainage Impact Assessment and Scottish Water Development Impact Assessment which Hermiston Securities will commission. A Drainage Strategy will also be prepared and submitted as part of the application for Planning Permission in Principle.
- 5.28 Local retail, commercial and employment – the Framework identifies locations for local retail and commercial development along key routes and those which address civic spaces. Such uses are separate to other ‘Employment’ allocations and are intended to provide support services appropriate for the new residential community, for example an ‘express’ style local supermarket. Several development blocks are identified for Employment uses, specifically at the southern entrance from Wellington Road. These sites will provide a range of types from commercial, leisure and offices uses.
- 5.29 A detailed break down of the Infrastructure Requirements for the site can be found on pages 81-83 of the Development Framework.

Phasing

- 5.30 An indicative phasing strategy has been developed which details the delivery of the allocation in accordance with the ALDP 2012.

Phase 1 2007-2016	400 units, access from Wellington Road, range of residential densities along Primary Street and Lochside improvements.
Phase 2 2007-2016	400 units, landscape park along watercourse and primary school at end of phase.
Phase 3 2007-2016	300 units, southern side to Redmoss Road complete and associated buffer landscaping.
Phase 4 2017-2023	400 units, remaining plots to the north opened up.
Beyond 2020	Charleston employment area (OP78).

Public Consultation and Involvement

5.31 Throughout the design process for Loirston the project team have involved local people and stakeholders in the proposals. The public consultation for both the Development Framework and an application for Planning Permission in Principle have been run in tandem to avoid duplication and confusion. One major consultation event and separate meetings with the three Community Councils that represent the area (Nigg, Cove & Altens and Kincorth & Leggart) have taken place.

5.32 Design for the site has evolved with continued input from the community. Their input has been fundamental in shaping the proposals ensuring that they meet the aspirations for the site. Events that took place during the masterplanning process for the sites include:

- Local Development Plan: Development Options Consultation Event – 8 June 2009
- Local Development Plan: Main Issues Report Consultation Event – 18 November 2009
- Joint Community Council meeting - 7 June 2012
- Community Exhibition - 19 June 2012
- Joint Community Council meeting follow up - 23 August 2012

5.33 The main issues arising from consultation so far include:

- **Education, Community and Supporting Facilities**

Concerns were raised regarding provision of primary and secondary education and potential of joint campus on site should be considered. Comments were in favour of some form of retail provision and community facilities including leisure and open space, affordable and sheltered housing was considered essential, but the provision of a Gypsy/Traveller site was not favoured. The project team are currently producing an Education Study, this along with close consultation with Aberdeen City Council will inform the exact education provision.

- **Transport and Infrastructure**

Concerns were raised about the impact of the development and traffic on the surrounding road network. Use of Redmoss Road and Nigg Road as bus only routes, and causeway across Loirston Loch were not favoured. The need for a new access from the A90(T), safe pedestrian crossings of Wellington Road, better bus services and retention of existing path networks were also key concerns. The project team are currently undertaking a detailed Transport Assessment, which will inform a detailed road layout and identification of transport infrastructure requirements. The causeway proposal has been removed.

- **Environment and Loirston Loch**

Concerns were raised about the impact of the development on wildlife, flora and fauna, and green corridors which link surrounding open space resources. Concern was expressed regarding the visual impact of the development, impact on gateway to the City, the height and proximity of buildings on the edge of the Loch. The importance of balancing accessibility, angling and wildlife preservation whilst retaining and enhancing as much of the natural character of the Loch was another key concern. Support in principle was also raised for a community trust to be formed to help manage and protect the future of the Loch. The project team have included a detailed section within the Framework on the relationship to the Loch (Section 5.18 and 5.19) and they have begun an Environmental Impact Assessment process to cover environmental issues in detail.

- **Football Stadium**

Concerns were raised about potential parking problems on matchdays and existing permission for Aberdeen Football Club Stadium prejudicing the masterplanning of the wider Loirston area. Discussions are ongoing between the site developers and AFC regarding site acquisition and other details relating to provision of a stadium. Details of controlled parking and access arrangements will form part of the Transport Assessment.

5.34 Full details of the comments received for the engagement process and the responses made can be found on Pages 8-12 of the Development Framework.

5.35 Pending approval of this report, the Loirston Development Framework will be the subject of statutory consultation. The following list highlights those that will be consulted. Please note that this is not an exhaustive list:

- Nigg Community Council
- Cove & Altens Community Council
- Kincorth/Leggart Community Council
- Kincorth Nature Reserve Management Committee
- Aberdeen City and Shire Strategic Planning Authority
- Aberdeenshire Council
- Forestry Commission Scotland
- Scottish Water
- SEPA
- Scottish Natural Heritage
- Historic Scotland
- Scottish Enterprise Grampian
- Transport Scotland
- NHS Grampian

- NESTRANS
- Planning Gain

- 5.36 The results of the statutory consultation will be reported to the Enterprise Planning and Infrastructure Committee in due course, including any recommended amendments to the Framework.
- 5.37 It is important to note that engagement will continue throughout the development process, during detailed masterplan stages and any subsequent planning applications.

6 IMPACT

- 6.1 The proposal contributes to the following Single Outcome Priorities: 1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2 – we realise our full economic potential with more and better employment opportunities for our people; 10. We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.
- 6.2 The proposal contributes to the 5 Year Business Plan in terms of objective – Communication and Community Engagement; sharing our plans and aspirations for the city, – delivering an up-to-date plan, – facilitating new development projects to improve Aberdeen’s living environment and, – support open space initiatives.
- 6.3 The proposal contributes towards the Aberdeen City Council Administrations vision for Aberdeen: 2012 – 2017, particularly creating a City which is a great place to live, bring up a family, do business and visit. It specifically contributes to the follows objectives: Governance – encouraging citizens to participate in design and development; Living – improving opportunities for physical activity; Environment – energy efficient design and construction, attractive streetscapes and access to green space; Economy – improve access to range of affordable housing; Mobility – encourage cycling, walking and promoting a sustainable transport systems which reduce carbon emissions.
- 6.4 The proposal is consistent with the Council’s Corporate Plan in particular with regard to delivering high levels of design from all development, maintaining an up-to-date planning framework, sustainable development and open space provision.
- 6.5 The proposal is consistent with the Planning and Sustainable Service Plan, in particular engaging the community in the planning process, and the delivery of masterplans/development frameworks in line with The Aberdeen Masterplanning Process.



- 6.6 An Equalities and Human Rights Impact Assessment has been prepared. In summary, many of the aims of the proposal will have positive benefits, including;
- Race – the proposals include the provision of a Gypsy Traveller site to improve access to facilities.
 - Younger – the proposals include the provision of Play Spaces for children.
 - Other (Housing) – greater provision of housing to meet the housing demand and affordable housing to meeting housing needs.
 - Other (Health) – improved access useable open spaces for leisure pursuits and opportunities for walking/cycling/exercise.

7 BACKGROUND PAPERS

- 7.1 The Loirston Development Framework (November 2012) is a large document containing a lot of illustrative material and can be viewed by accessing the following link:
www.aberdeencity.gov.uk/masterplanning
- 7.2 Aberdeen Local Development Plan 2012
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=42278&slD=9484>
- 7.3 Aberdeen Masterplanning Process 2008 (article 22, Planning Committee Minute, 6/11/08)
http://www.aberdeencity.gov.uk/Planning/pla/pla_planningbriefs.asp

8 REPORT AUTHOR DETAILS

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An aerial photograph of a rural landscape in Loirston. The image shows a mix of green agricultural fields, brownish-grey patches of bare earth, and a network of roads and paths. A prominent road runs diagonally from the top left towards the bottom right. In the upper left, there is a cluster of buildings, possibly a farm or small industrial site. A large, irregularly shaped lake or pond is visible in the middle-left area. The background shows rolling hills under a clear sky.

LOIRSTON

Development Framework
November 2012

SUMMARY

Loirston Development Framework: Summary

Introduction

The Loirston site occupies an important strategic position within the context of Aberdeen City both as a gateway site and as a resource for open spaces such as Loirston Loch and Kincorth Hill. The site offers an excellent location for development, with the potential to connect to and support existing surrounding communities whilst integrating with the distinctive landscape setting.

The Loirston masterplan area is located to the south of Aberdeen and is adjacent to the residential areas of Nigg, Cove and Charleston and in close proximity to the employment uses of Altens industrial estate. The relevant “Opportunity Site” boundaries cover a total area of c177 hectares.

The document is set out in the following sections which are summarised on the following pages:

Section 1: The Masterplan Process

Sets out the process which has been followed during the evolution of the Development Framework, the planning context, existing consents within the DF which are considered as “fixes” and details the consultation and engagement process which has been undertaken along with recorded responses to comments.

Section 2: The Site

Describes the site, historic development and site analysis. A brief outline of the EIA and TA scoping process is presented.

Section 3: Opportunity and Vision

Details the potential of the site as a major gateway to the City and outlines the key principles which have been adopted for the DF.

Section 4: Design Development

Illustrates the design progression from early sketches and options to material included in the public exhibition and more detailed testing of urban grain and character.

Section 5: The Development Framework

Defines the various components of the DF and demonstrates how they address points of policy, landuse, movement, open space, urban design, character and design. Three-dimensional material sets out the aspirations for various character areas.

Section 6: Phasing and Delivery

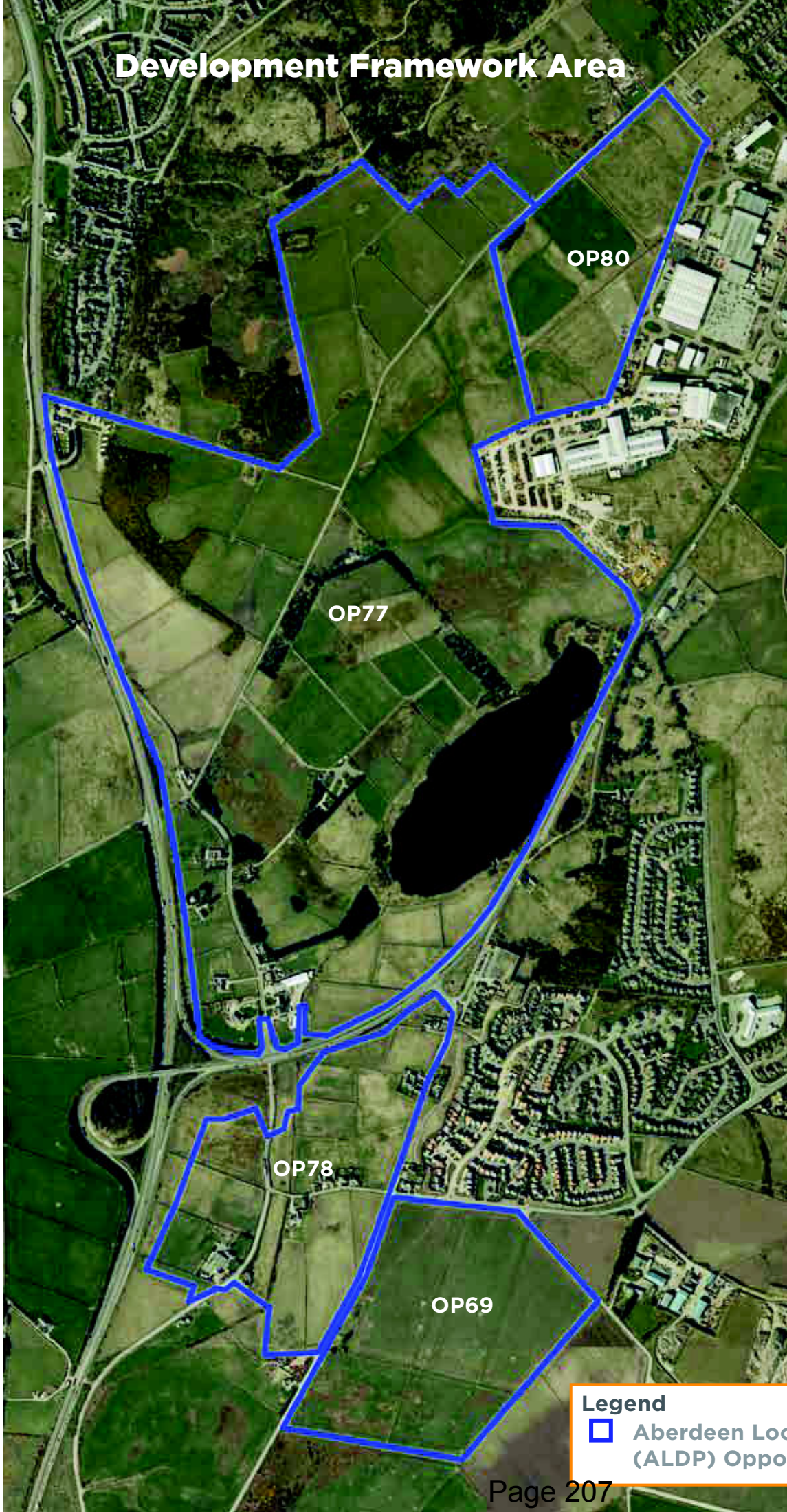
Key principles of phasing and delivery are set out through indicative phasing diagrams and a matrix of infrastructure requirements.

Section 7: Appendices

Additionally, the following supporting information has been submitted in support of the Development Framework and should be read in tandem with this document:

- Drainage Statement
- Geo-environmental Desk Study
- Traffic Assessment Scoping
- EIA Scoping

Development Framework Area



OP80

OP77

OP78

OP69

Legend

-  Aberdeen Local Development Plan (ALDP) Opportunity Site

Section 1: Masterplan process

This section sets out the purpose of the Development Framework and sets out the planning context in terms of development plans, policy and other relevant guidance documents. The technical workshops with ACC stakeholders is described and the process of consultation with the Community is set out. A public exhibition was held in June 2012 and the relevant material and collated comments is recorded. The design team response to those comments is set out along with identification of how the proposals have changed to reflect the aspiration or specific requests made by the community. Several meetings have been held with the three involved Community Councils and the outcome of those meetings is noted.



Public exhibition, June 2012

Section 2: The site

Section 2 describes the site in detail and sets out the existing consents and relevant planning applications which fall within the Framework area. A number of these, including the proposals for Aberdeen Football Club and Cove Rangers Football Club, are considered as 'fixes' for the purpose of the Development Framework, and their current planning status and layout is recorded.

The Loirston site is located to the west of Cove Bay on the southern outskirts of Aberdeen City. It is bordered on the west side by the A90 and on the south-east by the A956 (Wellington Road). To the west and south lies agricultural land. Immediately north of the site is Kincorth Hill, an area of scrub and a nature reserve, and beyond that the residential area of Kincorth. Bordering the site to the north-east is a small area of agricultural and recreational ground, and further to the north and north-east are the large Tullos, Nigg and Altens industrial estates.

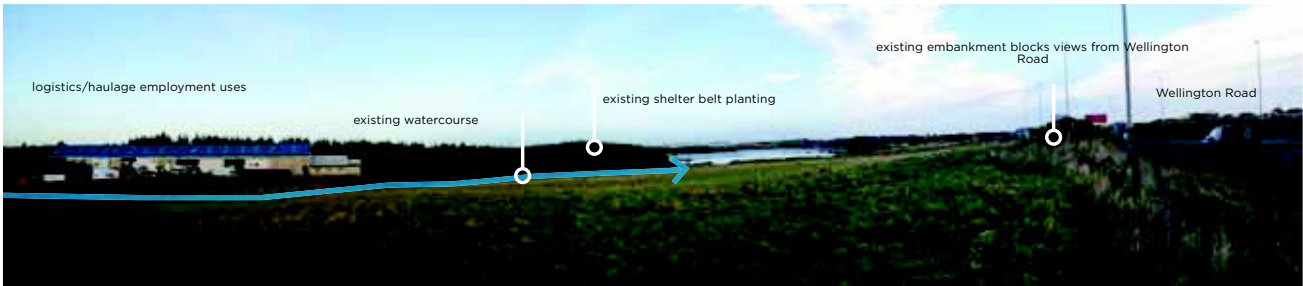
Most of the site lies between 80 and 90 metres above ordnance datum (m aod). The ground is generally fairly flat, sloping gently down towards Loirston Loch, but steepens upwards towards Kincorth Hill at the north end and Blue Hill to the south-west (outwith the site). There is an area of low-lying, marshy ground immediately to the west of the loch. Areas of boggy ground exist, particularly near to the loch. The main use of the site is as grazing land for cattle and sheep with some recreational uses.

The historic development of the site is set out through text narrative associated with historic maps; key landscape elements such as Loirston Loch and Kincorth Hill are described and their key sensitivities noted along with the environmental principles to be adopted as the framework is developed in more detail. Other environmental considerations are summarised along with an overview of the Environmental Impact Assessment scoping process. This includes an outline of the approach to assessing impact on amongst other topics: land use; hydrology, drainage, flood risk and water quality; ecology, nature conservation and biodiversity;

cultural heritage; noise and vibration; and traffic and transport. Initial studies suggest that potential environmental impacts can be avoided or minimised to an acceptable level through sensitive design.

Reference is made to the Transport Assessment scoping report which accompanies the DF and sets out the proposed approach to assessing impact on connectivity issues.

Finally, selected site panoramas are set out with key landscape elements identified along with relevant features in the immediate context.



View A: View from within embankment and site to loch from south-west extent of site



View B: To loch from Wellington Road



View C: To Blue Hill from location of proposed AFC junction at Wellington Road

Section 3: Opportunity and Vision

The Loirston site sits in a key strategic location which has the potential to be a significant gateway to Aberdeen City making the Loirston site a highly visible edge of city location, integrated positively with important landscape resources such as Loirston Loch and Kincorth Hill. With future changes planned to the transport network around the site the area covered by the Framework will be bounded by two key routes into the city; these key arteries, coupled with the potential for a public transport route through the new development and the open aspect of the site due to the existing topography, make the Loirston site a highly visible edge of city location.

The existing landscape resources of Loirston Loch and Kincorth Hill offer a fantastic opportunity for a new neighbourhood that is fully integrated with its landscape setting and that is distinctive and appropriate to the surrounding context. The proximity to the city centre and key employment areas creates the potential for a unique and attractive waterside mixed-use development. By sensitively establishing a landscape framework from the outset around the existing resources, a strong green space network can be created, into which sustainable development neatly fits, offering easy access to the surrounding open spaces and existing communities.



Indicative Framework block model

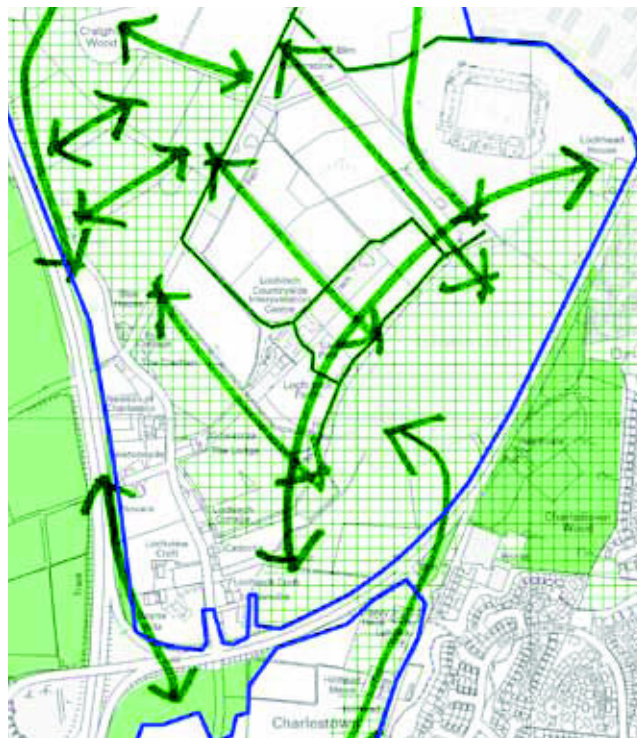


Illustrating key development principles

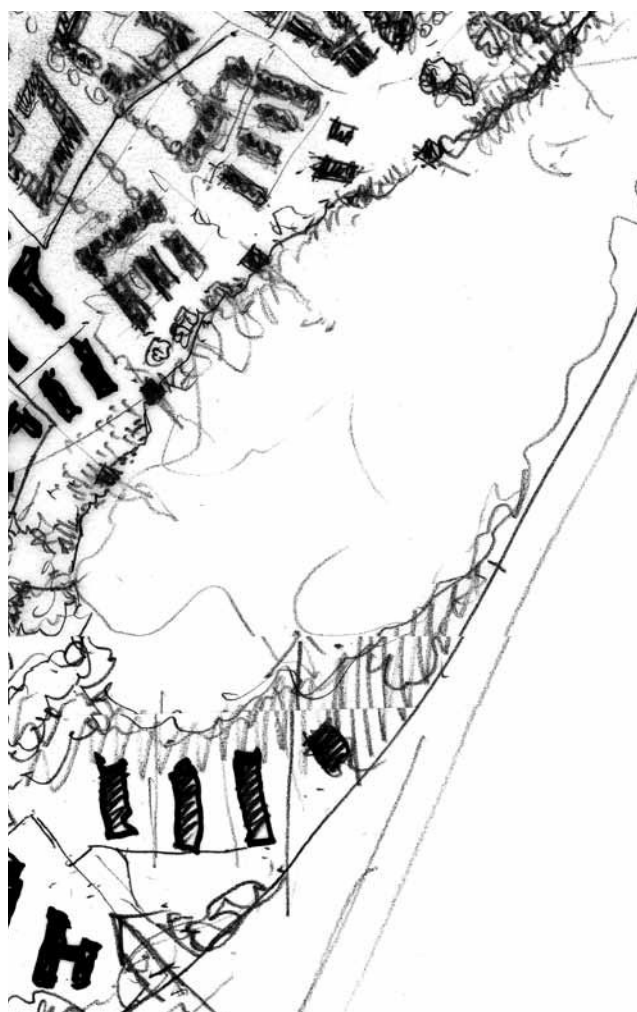
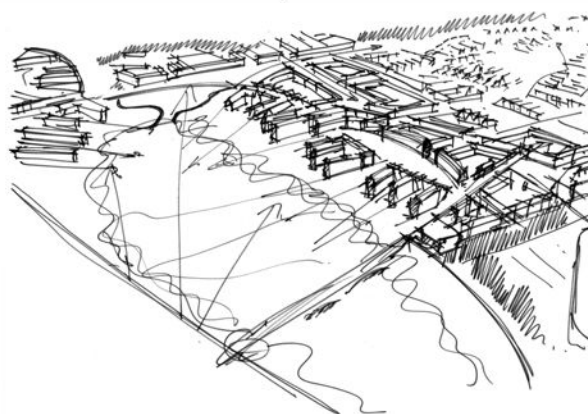
Section 4: Design development

The design team has used an iterative process to develop the Loirston Framework, based on an evolving understanding of the site and its technical parameters, the surrounding context and the aspirations of the key stakeholders. Consultation has been key, taking into account the views and expertise of the community, statutory consultees and ACC. Feedback received from these interactions has been assessed and integrated where appropriate in order to ensure the best outcome for the site and a Framework which can flexibly adapt and accommodate change in the future.

This section illustrates and records some of the outputs of this process, exploring through diagrams, plans and sketches the various paths that were followed on the way to establishing a concept and baseline that could be tested technically and with stakeholders.



Testing the open space network connections



Sketches in support of early visualisations for the loch.

Section 5: The Development Framework

The Framework has evolved from a careful study and analysis of the Loirston site and its context within Nigg, Cove and Charleston. It has taken into account the prime strategic nature of the site and the potential for it to develop as a gateway to the City over time, as well as being a place in its own right. The Framework establishes a flexible structure which is in line with the timing and allocations set out within the ALDP and allows for future detailed proposals to come forward in due course.

In line with the Aberdeen City Council 'Masterplanning Process' the Development Framework sets out a vision and a spatial Framework for the way in which the Loirston area will be developed. This section illustrates through concept diagrams how the framework has been structured in relation to existing landscape features, landform and orientation, views, spatial experience and connections.

Detailed diagrams set out the proposals for access and connectivity relating to existing and proposed core paths and cycle networks along with vehicular movements which follow the principles of 'Designing Streets' Policy Statement for Scotland. Two main junctions are proposed for access from Wellington Road supported by two other connections at Redmoss Road and Wellington Circle.

The requirements for usable, well-designed open space as set out in ACC's

Open Space Supplementary Guidance is clearly illustrated through a series of diagrams which identify relevant types and categories by use and area.

Landuse and density principles illustrate how and where 1500 residential units and 11 hectares of Employment land would be accommodated on the site and ranges of applicable storey heights are indicated. A strategy of taller buildings around the Loch has been identified to maximise the opportunity to create a gateway development which is visible across the water on approach to the city.

An initial assessment of the required surface water strategy is described along with a description of current topography and landform. Indicative character areas have been identified across the framework and two- and three-dimensional material has been developed to illustrate the proposed approaches and relevant urban design principles. A section relating to the specific sensitivities of the lochside area shows how the next steps in detail design should consider building heights, setbacks, environmental improvements and access arrangements.

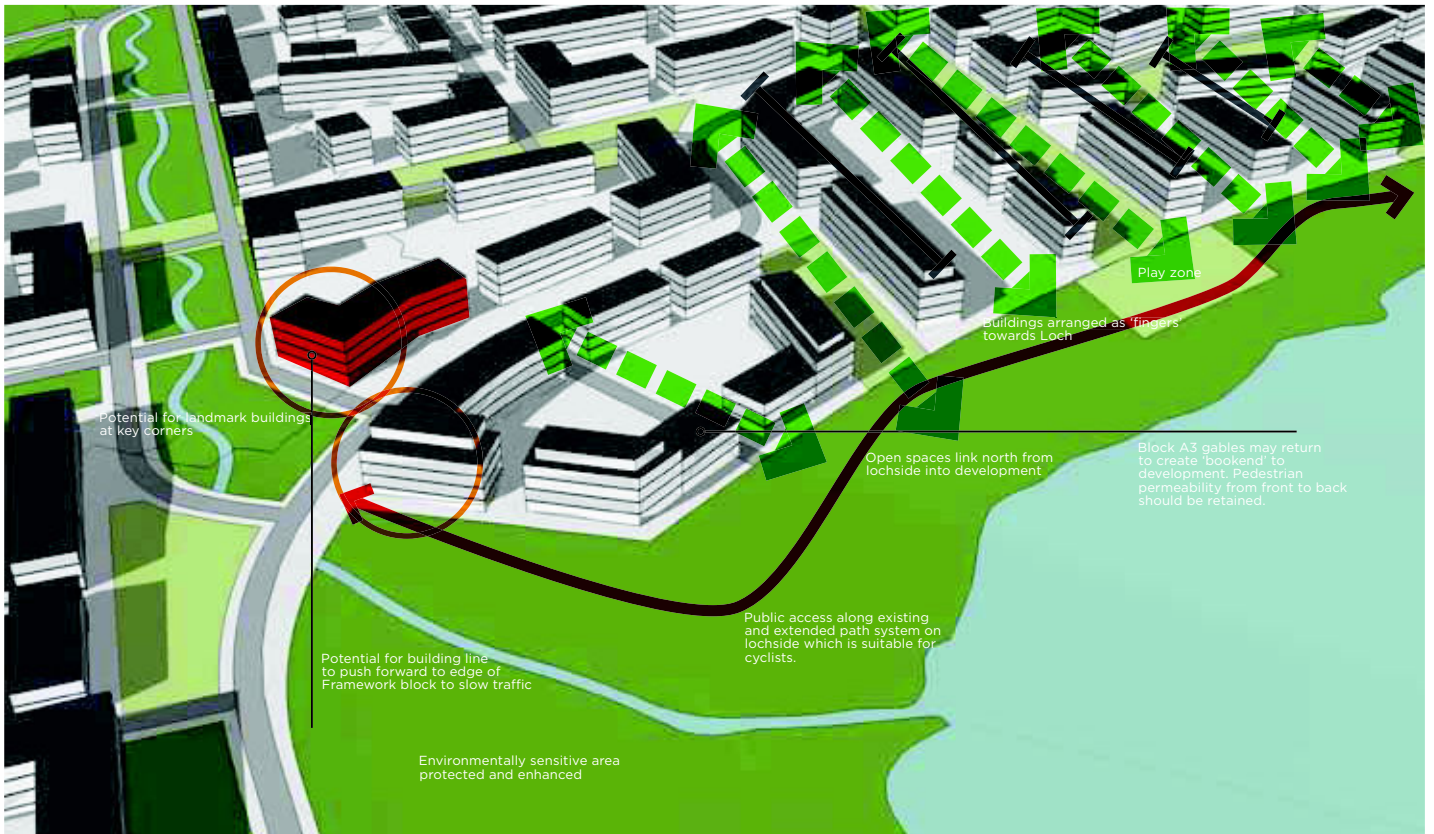
Finally, due to the current uncertainty regarding both the final location for a new Secondary School and the delivery of the various football club's proposals, a number of options to the 'Baseline' Framework have been developed which allow the Framework to accommodate various scenarios.

Loirston Development Framework

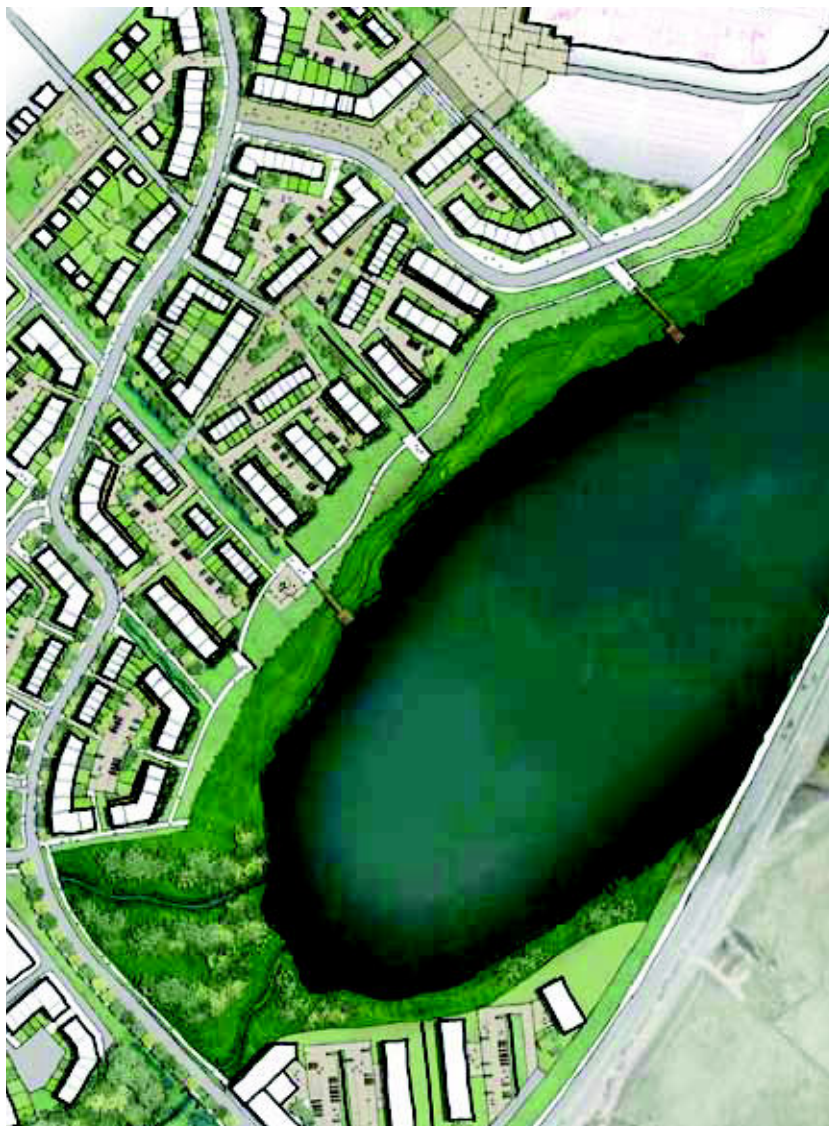


Legend

- Development blocks
- Existing development
- Streets
- Indicative location for new junction
- Major connection to surrounding road network
- Other connection to surrounding road network
- Core path (existing and improved)
- Core path AP3
- General path network
- Major Open Space
- Neighbourhood Open Space
- Local Open Space
- Indicative SUDS location
- Primarily continuous frontage
- Mixed frontage
- Key buildings/corners for orientation, landmarks and urban character.
- Suggested development block internal access.



Urban design principles of lochside area



Concept plan for lochside area illustrating key principles

Section 6: Phasing and Delivery

The last section of the Framework presents indicative phasing diagrams and associated principles which are devised to create a logical sequence of development and an appropriate build-up of the place. A matrix describing an outline delivery strategy identifies the key infrastructure requirements and the parties to be involved in their delivery at later stages.



Indicative phasing



Illustration of opportunity and potential for lochside area

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Maidencraig Masterplan
REPORT NUMBER:	EPI/12/198

1 PURPOSE OF REPORT

- 1.1 This report outlines the Maidencraig Masterplan, prepared as a guide for the future development of land identified as Opportunity Sites OP43 Maidencraig South East and OP44 Maidencraig North East in the adopted Aberdeen Local Development Plan (ALDP).
- 1.2 A summary document of the Maidencraig Masterplan is appended to this Committee Report. The full Maidencraig Masterplan for sites OP43 and OP44 (September 2012) is a large document containing a lot of illustrative material and can be viewed by accessing the following link:

www.aberdeencity.gov.uk/masterplanning
- 1.3 A hard copy of the Masterplan is available in the Member's Library or within the Planning and Sustainable Development service at Ground Floor North, Marischal College, or by contacting the Masterplanning, Design and Conservation team.
- 1.4 The Masterplan has been made available to the Mastrick, Sheddocksley and Summerhill and Kingswells community councils for information prior to this report being considered for public consultation.

2 RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
 - (a) Approve the Maidencraig Masterplan as interim planning advice; and
 - (b) Agree for officers to implement the process to ratify the Masterplan as Supplementary Guidance. This will include a 4 week public consultation with results reported to committee prior to submission to Scottish Government.

2.2 Definitions

‘Interim Planning Advice’ – this specifies that the Masterplan is in the public domain and available for the purposes of a formal City-wide consultation. As such it becomes a material consideration in the determination of any planning application. The duration of consultation will typically be 4 weeks, given that public engagement has taken place throughout the preparation of the Framework. This allows responses to be collated and reported back in the next committee cycle.

‘Supplementary Guidance’ (SG) – this is adopted and issued by a planning authority in connection with a Local Development Plan, as a result any such guidance will form part of the Development Plan. Before adoption, the SG must be publicised and a period for representations to be made specified, this includes the targeting of key consultees and stakeholders who may want the opportunity to comment. Following the specified consultation period and as a result of comments received, relevant changes will be made to the final document before reporting back to committee and subsequent submission to Scottish Ministers. After 28 days have elapsed, the authority may then adopt the guidance unless Scottish Ministers have directed otherwise.

3 FINANCIAL IMPLICATIONS

- 3.1 Aberdeen City Council owns an area of land to the north of the site identified in fig 74 on page 54 of the Masterplan. As such the Council has a financial interest in the planning designation and future development of the site.
- 3.2 The developers have met the cost of preparation of the Masterplan including all consultation and engagement. The proposals will result in efficiencies in the determination of future planning applications related to the Masterplan, leading to a reduction in Council staff time to assess future detailed proposals.

4 OTHER IMPLICATIONS

- 4.1 The Masterplan will reduce the risk of piecemeal and inappropriate development in the site area. Mixed use development and the efficient use of land will contribute towards the Council’s aim of promoting sustainable development.
- 4.2 Approving the Masterplan will contribute to efficiencies in determining future planning applications. By encouraging acceptable forms and uses of new development to be agreed publicly, it allows more informed decision making to be made earlier, saving time and resources for staff, applicants and the public.
- 4.3 In accordance with the Local Development Plan, Appendix 4, The Action Programme and the Infrastructure and Developer Contributions

Manual, the Masterplan explicitly references how the development will address requirements for cumulative and local transport infrastructure; public transport along with walking and cycling.

- 4.4 A Strategic Environmental Assessment has been undertaken on both Maidencraig sites as part of the preparation for the proposed Aberdeen Local Development Plan.

5 BACKGROUND/MAIN ISSUES

- 5.1 The Maidencraig Masterplan has been produced by Bancon Developments and aims to create a vibrant, welcoming, attractive and sustainable community which is safe and easy to move around in. A multi disciplinary team has been assembled to produce the Masterplan including Bancon (planning and development), Archial (design and architecture) and David Wilson Associates (landscape).

Site Description

- 5.2 Maidencraig North East (OP44) and Maidencraig South East (OP43) are located to the north and south of the Lang Stracht (A944) respectively. Situated adjacent to Sheddocksley on the western edge of the City, OP44 has a gradual slope from the north of the site down to the A944. The southern site (OP43) is next to Summerhill and is much more undulating in nature with the land dropping steeply at the south into Den of Maidencraig.

Policy

- 5.3 The Masterplan has been prepared following the Council's adopted Aberdeen Masterplanning Process and contributes to the Scottish Government and Aberdeen Local Development Plan policy aims of raising design quality in new development, through the 6 key qualities of creating successful places, namely – distinctive, welcoming, safe and pleasant, adaptable, easy to get to and move around, and resource efficient.
- 5.4 The sites are allocated for 450 homes on OP43 and 300 homes on OP44 in the Aberdeen Local Development Plan - a total of 750 homes. There is no specific employment allocation associated with either site. Both sites are proposed within the first period of the plan, 2007-2016. A contribution of 25% affordable housing is required within this site and discussions will take place between the Councils housing service and other appropriate bodies. Discussions are ongoing with Registered Social Landlords.

Design Principles

- 5.5 The Maidencraig Masterplan establishes a vision for the site, providing a comprehensive spatial framework, integrating landuse, housing,

landscape and transport proposals and setting out a clear phasing scheme.

- 5.6 The Masterplan aims to create a sense of place, confirming a number of existing environmental, historical and cultural features will be retained. Development will also respond to its surroundings and the local identity, creating a series of distinct character areas connected by an integrated network of streets, paths and spaces.
- 5.7 The topography of the site is key to shaping the design and layout of the development and defining the character areas. This is particularly apparent in the sections that provide detail on how the development will relate to the Den of Maidenraig, the woodland to the north, the existing communities to the east and Dobbies to the west. The location and shape of the site play an important role in the form, design and character of the development.

Character Areas

The masterplan promotes three distinct character areas –

- 5.8 **Area A** (page 39 of the Masterplan document)
Maidenraig North East, bounded by Sheddocksley, woodland to the north, Dobbies Garden Centre and the Lang Stracht. This area is made up of a variety of house types, including a large provision of affordable housing to the eastern edge of the site. A commercial zone with uses complementary to a residential area has been included with the potential for offices, small shop units etc below flats. There will be pedestrian and cycle connections and the bus route loops through this part of the site. Details on Area A can be found on pages 39-44 of the Masterplan.
- 5.9 **Area B** (page 45 of the Masterplan document)
The western section of OP43 bounded to the north by the Lang Stracht, the west by the switch back, south by the Den of Maidenraig and the east by a group of existing properties and a landscaped gully. This area is lower density in response to the landscape, containing mainly semi detached and detached properties, 1 – 2 storeys in height. Courtyards and areas of open space help to draw the landscaping into the site to soften the edge of the development and integrate it into the landscape. Details on Area B can be found on pages 45 and 46 of the Masterplan.
- 5.10 **Area C** (page 47 of the Masterplan document)
Area C is bounded by the gully to the west, to the south by the Den of Maidenraig, to the east open space between the site and Summerhill and the Lang Stracht to the north. This area has a number of existing properties within it that have been incorporated in the proposed layout of the site with a mix of house types. There is also a small area of commercial development located on the ground floor of two of the units in the flatted development. This area has a number of courtyards that

open up onto the Den of Maidencraig, bringing the landscape into the development and softening the southern boundary of the site. Details on Area C can be found on pages 47 to 51 of the Masterplan.

- 5.11 Within the masterplan density has also been carefully considered. Higher density development is located around the bus routes and core areas. Lower density development is located closer to the periphery providing a more sensitive approach adjacent to the rural edges of the site.

Environment and Open Space

- 5.12 The vision for the site is a successfully integrated green network through the site which brings the surrounding landscape into it. The key principles for the site can be viewed on page 18 concept plan in the Masterplan (fig 17). The north - south green connections linking the Den of Maidencraig with the woodland and core paths to the north are very important and figure prominently in the masterplan.
- 5.13 The Masterplan has carefully considered the landscape network around the two Maidencraig sites and how they can be linked to produce a strong green space network. A representation of the Masterplan's proposed green space network can be viewed on pages 27 and 28 of the document. This shows that there will be a greater amount and better connected spaces than the Local Development Plan proposed.
- 5.14 The majority of existing trees and hedges on the site have been incorporated into the proposed masterplan. Detailed surveys and landscape plans will be required as part of the planning application process.
- 5.15 Open space provision will also be included within the site as part of the requirement of the Local Development Plan. Details of this can be found on page 29 of the Masterplan document.

Housing

- 5.16 The developer aims to provide a high quality living environment that will meet housing need and demand, providing a mix of house sizes types and tenures.
- 5.17 The Masterplan promotes 25% affordable housing in accordance with the Aberdeen Local Development Plan. Work is ongoing with the council and affordable housing providers to deliver the right product. The affordable housing will be integrated with private development areas and will be of a design that does not differentiate them. Affordable housing provision is included within the first phase of development and in particular phase 1b is 100% affordable housing.

- 5.18 The developer is committed to sustainable house design having developed house types that utilise the best timber frame technology to minimise energy consumption. Consideration has also been given to aspect, orientation and maximising solar gain. The courtyards at the end of the streets allow light to permeate into the streets. The housing layout has also been considered to avoid overshadowing.

Access and Connectivity

- 5.19 The Masterplan considers in detail the needs of pedestrians, cyclists and public transport users as well as cars.
- 5.20 There are a number of core paths throughout both the sites. Core Path AP45 to the north connects the site to Sheddocksley. Details of the footpaths and core paths are indicated on pages 20 and 21 of the Masterplan. These support maximum permeability for the site in accordance with Designing Streets.
- 5.21 Concerns were raised by members of the community in relation to further connections to Sheddocksley. Pedestrian connections are essential for the connectivity and permeability of the site, including access to school, particularly through the proposed woodland belt on the east of the northern site. The exact details of these pedestrian connections will be a matter for detailed assessment as part of the planning application process, but the requirement has been included in the Masterplan.
- 5.22 Connections to the existing cycle network along the A944 are provided.
- 5.23 The two sites are fragmented by the Lang Stracht (A944). Careful consideration has been given to ensuring they are as successfully connected to each other as possible. The central core area provides a strong framework to encourage north-south movement of people. This junction will be fully signalised and will incorporate pedestrian crossings. Consideration has also been given to the entrance to both sites to provide a sense of arrival and welcome. Two further junctions are proposed to access the sites, one to the east and one to the west adjacent to the bus gate. These junctions will be left in - left out only.
- 5.24 The primary street through the site provides a bus route serving the site. The bus route can be seen in detail on page 21 of the Masterplan. This will be an extension to the number 23 service with a left in to the southern half of the site and a left out to the northern part creating a new loop link.
- 5.25 The development will be made up of a strong street hierarchy with a primary street including a bus route, green links, secondary streets and minor streets. Examples of how these may look can be found on pages 32- 35 of the Masterplan document.

Infrastructure

- 5.26 Health – Off site contribution proposed by the Infrastructure Requirements. The developers have noted the concerns of the community about provision and have begun discussions with NHS Grampian. The masterplan is flexible enough to provide onsite facilities should this be NHS Grampians preferred option.
- 5.27 Transport – The road access strategy for Maidencraig requires three accessed off the Lang Stracht (A944). The eastern and western junctions will both be left in- left out and the central junction at Dobbies will be fully signalised including a pedestrian crossing.
- 5.28 Education – Discussions need to take place with education and no school is required on site under the infrastructure requirements. If once the school review has taken place a school is required on the site there is enough flexibility within the masterplan to provide a school in later phases of the development. The developer will continue discussions with Education in relation to this issue.
- 5.29 A detailed break down of the Infrastructure Requirements for the site can be found on pages 56 and 57 of the Masterplan.

Phasing

- 5.30 The sites are both allocated within the first period for development (2007-2016) as set out within the Local Development Plan. Details of phasing can be seen on the diagram on page 54 of the Masterplan.

Public consultation

- 5.31 Design for the site has evolved with continued input from the community. Their input has been fundamental in shaping the proposals ensuring that they meet the aspirations for the site. Events that took place during the masterplanning process for the sites include:

- Presentation to Mastrick, Sheddocksley and Summerhill Community Council early 2009.
- LDP development bids exhibition 4 June 2009
- End June 2009 Kingswells Community discussions.
- Meeting with Mastrick and Sheddocksley and Summerhill Community Councils to discuss 8th February 2012.
- Community exhibition – 29 March 2012 – 30 March 2012.

- 5.32 The main issues arising from consultation so far include:

- Traffic concerns,

Concerns were raised about the impact on the road network from the traffic generated by 750 houses. The developer confirmed that

they will carry out a detailed Transport Assessment as part of the planning application process. The Masterplan also includes good pedestrian and cycle connections as well as a bus route to provide options other than the car.

- Education,

Concern was raised by the community on the distances pupils would have to travel to school. The Masterplan that is adaptable so that if a school is required onsite it could be accommodated in either OP43 or OP44.

- Impact on wildlife,

Concern was raised about the potential impact the development could have on the wildlife within the site. The masterplan ensures connection of habitats through two north - south corridors. There will also be significant landscaping throughout the site. A phase 1 habitats survey will be carried out in support of any planning application.

- Provision of community facilities/health care/ nurseries.

The requirement for the a contribution to health care facilities in Kingswells raised concerns with the local community, due to limited public transport access. The developer is currently in discussion with NHS Grampian and the Masterplan has capacity to accommodate healthcare facilities onsite should this be the best solution.

Full details of the comments received for the engagement process and the responses made can be found in Appendix 6.1 of the Masterplan.

5.33 Pending approval of this report, the Maidencraig Masterplan will be the subject of statutory consultation. The following list highlights those expected to be consulted. Please note that this is not an exhaustive list:

- Mastrick, Sheddocksley and Summerhill Community Council
- Kingswells Community Council
- Aberdeen City and Shire Strategic Development Planning Authority
- Aberdeenshire Council
- Forestry Commission Scotland
- Scottish Water
- SEPA
- Scottish Natural Heritage
- Historic Scotland
- Scottish Enterprise Grampian
- Transport Scotland

- NHS Grampian
 - NESTRANS
 - Planning Gain
- 5.34 The results of the statutory consultation will be reported to the Enterprise Planning and Infrastructure Committee in due course, including any recommended amendments to the masterplan.
- 5.35 It is important to note that engagement will continue throughout the development process and any subsequent planning applications.
- 5.36 Discussions will take place between the developer and other relevant bodies, including NHS, education and bus providers prior to the submission of a planning application.
- 5.37 A transport assessment is currently being produced and will be submitted to support an application for Planning Permission in Principle.

6 IMPACT

- 6.1 The Masterplan contributes to the following Single Outcome Priorities: 1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2. We realise our full economic potential with more and better employment opportunities for our people; 10. We live in well-designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.
- 6.2 The proposal contributes to the 5 year Business Plan in terms of working with our partners to attract visitors, workers and investment to protect the economic future of the city, encouraging the growth of local businesses through support of existing business sectors and development of new sectors and facilitating new development projects to improve Aberdeen's living and working environment.
- 6.3 The proposal contributes towards the Aberdeen City Council Administrations vision for Aberdeen: 2012 – 2017, particularly creating a City which is a great place to live, bring up a family, do business and visit. It specifically contributes to the follows objectives: Governance – encouraging citizens to participate in design and development; Living – improving opportunities for physical activity; Environment – energy efficient design and construction, attractive streetscapes and access to green space; Economy – improve access to range of affordable housing; Mobility – encourage cycling, walking and promoting a sustainable transport systems which reduce carbon emissions.
- 6.4 The proposal is consistent with the Council's Corporate Plan in particular delivering high levels of design from all development,

maintaining an up-to-date planning framework, sustainable development and open space provision.



- 6.5 The proposal is consistent with the Planning and Sustainable Development Service Plan, in particular engaging the community in the planning process, and the delivery of Masterplans in line with the Aberdeen Masterplanning Process.
- 6.6 An Equalities and Human Rights Impact Assessment has been prepared. In summary, many of the aims of the proposal will have positive benefits, including;
- Older – smaller house types including bungalows are included in proposals, and the design team have also expressed a willingness to consider other options to suit older people with the Council's housing team.
 - Younger – the proposals include the provision of Play Spaces for children.
 - Other (Housing) – greater provision of affordable and family housing in Aberdeen as a result of development.
 - Other (Health) – improved access useable open spaces for leisure pursuits and opportunities for walking/cycling/exercise.

7 BACKGROUND PAPERS

- 7.1 The Maidencraig Masterplan (September 2012) is a large document containing a lot of illustrative material and can be viewed by accessing the following link:
http://thezone/PI/SL_stratlead_reports.asp
- 7.2 Aberdeen Local Development Plan 2012
http://www.aberdeencity.gov.uk/Planning/ldp/pla_local_development_plan.asp
- 7.3 Aberdeen Masterplanning Process
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31782&slD=14394>

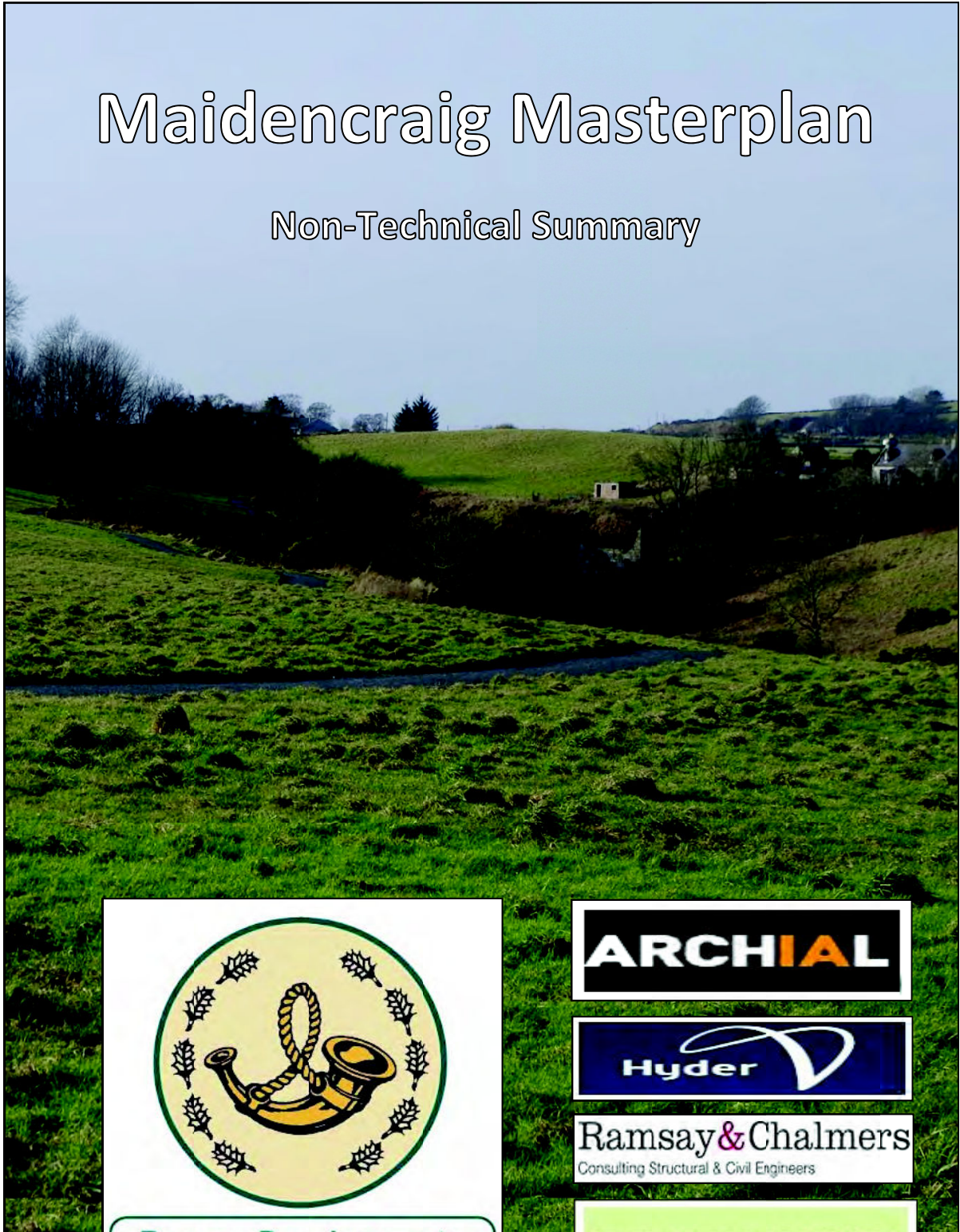
8 REPORT AUTHOR DETAILS

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Maidencraig Masterplan

Non-Technical Summary



Bancon Developments

ARCHIAL



Ramsay & Chalmers
Consulting Structural & Civil Engineers

DAVID WILSON ASSOCIATES
landscape architects

1 Context

1.1 The Site

The proposed development area is made up of two parcels of land, identified in the Local Development Plan as OP43 – Maidenraig south east, and OP44 – Maidenraig north east. The two areas are bisected by the A944 (Lang Stracht) Road.

Maidenraig South East totals 29.8 hectares, is bounded by the A944 to the North and west, Summerhill to the East, and the Den of Maidenraig Local Nature Reserve to the South.

Maidenraig North East totals 22.8 hectares. The A944 bounds the South of the site, Sheddocksley lies to the East, woodland lines the North boundary, and to the West is the Dobbies Garden Centre.

Both sites slope gently South, with the Maidenraig South East site becoming steeper towards the Denburn and the Local Nature Reserve. The aerial photograph below shows the site (outlined in red) in context, along with listed buildings and tree preservation orders marked in yellow. The woodland to the north, and the Den of Maidenraig Local Nature Reserve to the south are shaded white.

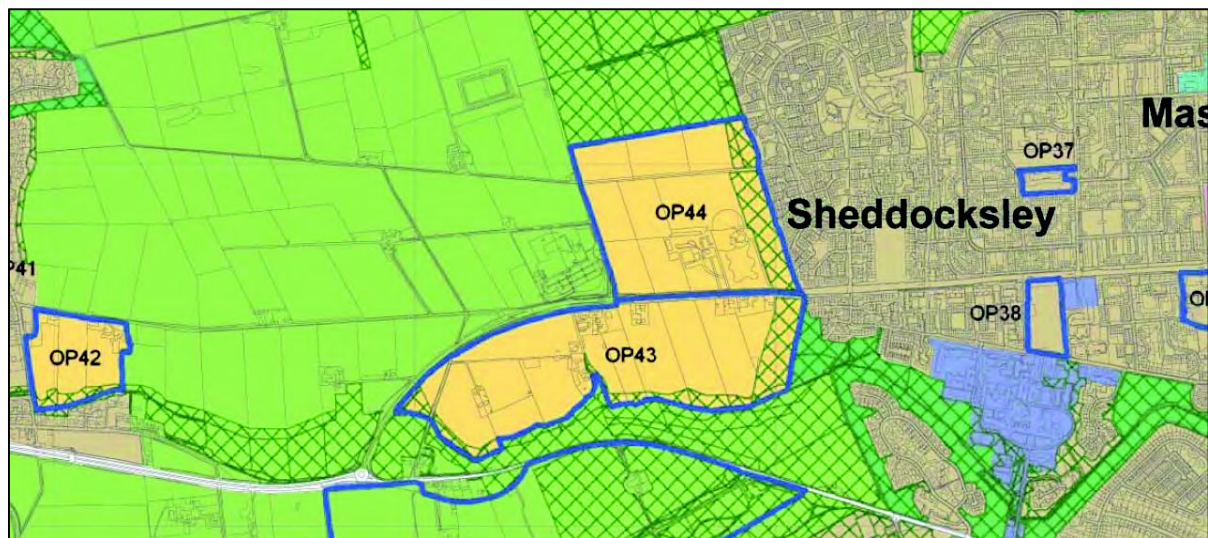


Maidenraig masterplan area.

1.2 Local Development Plan

The Aberdeen Local Development Plan, adopted on the 29th February 2012, aims to implement the broad growth strategy set out in the Structure Plan.

Site OP 43 Maidenraig SE is programmed to deliver 450 homes by 2016, while site OP 44 Maidenraig NE should provide 300 homes in the same time period. The Local Development Plan does not allocate employment land on the Maidenraig site. These allocations are confirmed on the Proposals Map.



Aberdeen Local Development Plan zoning map excerpt

1.3 Site Analysis Summary

A detailed analysis of the site has been carried out. The opportunities and constraints to the development have been identified, as have the existing services and facilities. The following key points have been identified to inform the design and layout.

- Open sloping and irregular site, facing south and east
- Views to Local Nature Reserve and beyond
- A944 runs between the two site areas.
- Site abuts the existing urban edge which is quite 'hard' to the north of the Lang Stracht but more organic to the south of the road presumably due to the difficult contours.
- Wayleave through southern section of site for drainage from garden centre.
- Denburn forms the meandering southern edge of site.
- Existing field boundaries suggest a natural 'grain' to the landscape
- Sections of the site adjacent to the Denburn slope so steeply that development is precluded.
- Desirability of links to Sheddocksley

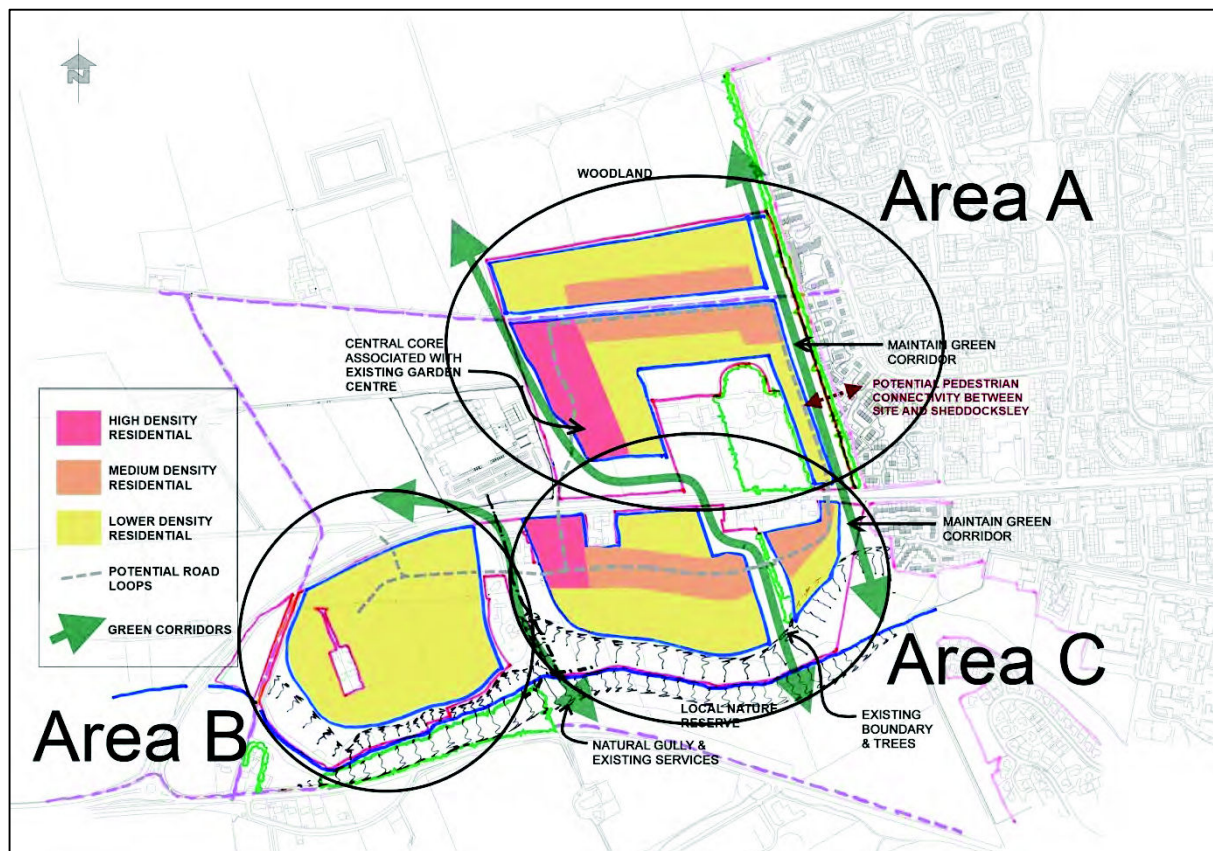
- Proposed Green Space Network around site
- Existing buildings on the site are generally small scale and traditional in character.
- Minimal vegetation due to agricultural/horticultural use with trees and shrubs restricted to property and field boundaries.
- Footpath (part of core path network) passes east/west through the north of the site.
- Proximity to wider footpath & cycle network
- Road access limited to the A944 corridor
- Public Transport available and actively promoted

2 The Vision for the Masterplan

Our Vision is to harness the advantages identified in the analysis to produce a vibrant, welcoming, attractive and sustainable community which is safe and easy to move around, while minimising the segregation which the Lang Stracht tends to impose.

The development will be highly landscaped – echoing and absorbing the surrounding landscape (of the Northern woodland, the Green Space Network, and the Den of Maiden Craig), and highly sustainable, with a mix of uses and good connections to walking, cycling and public transport routes. Within this over-arching framework we will develop three distinct character areas containing attractive residential neighbourhoods where families can live, work and play.

We are looking to deliver this Vision by adopting the concept shown below. This concept diagram suggests linking the new community to the major environmental assets and areas of recreational open space which lie to the north (the woodland) and south (the Den of Maiden Craig) of the site, and maintaining ready north – south connectivity by introducing three pedestrian and vehicular connections associated with broad wildlife corridors, so that the connectivity is visual as well as physical.

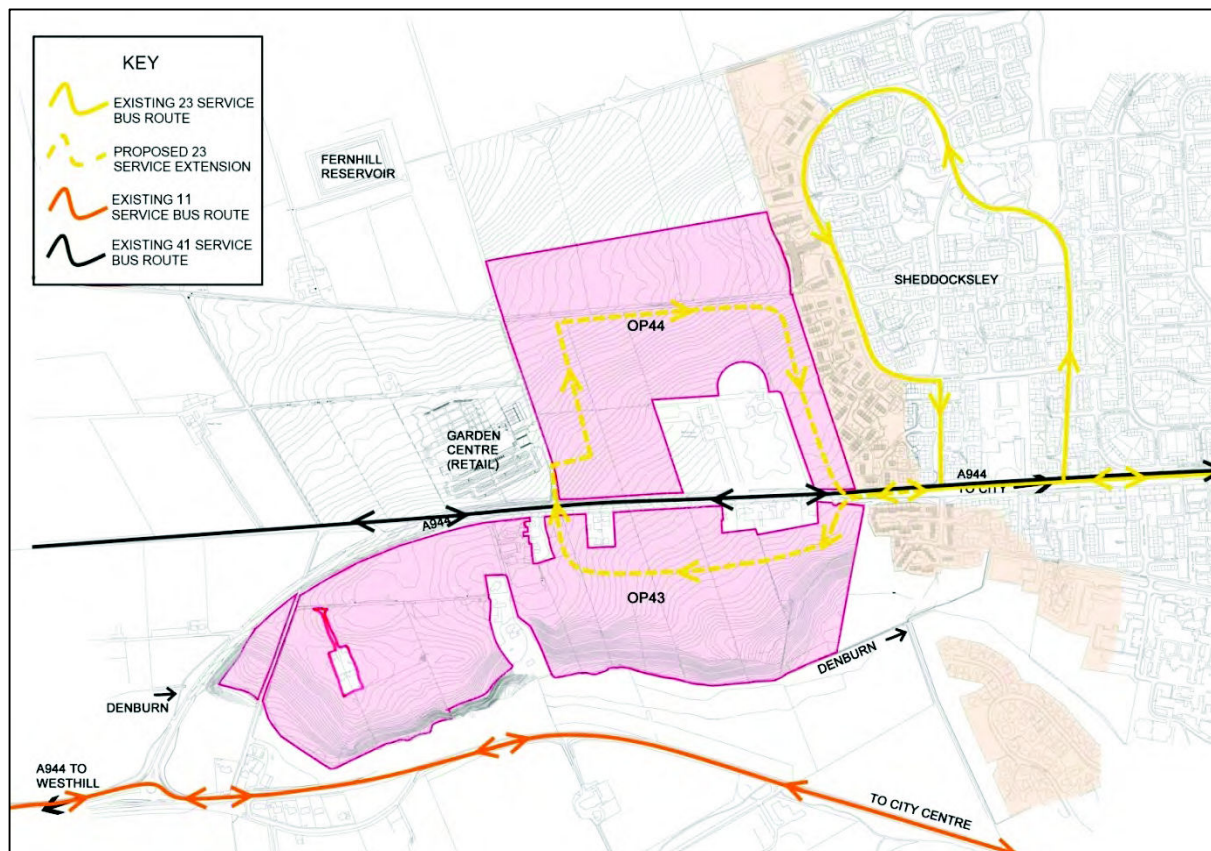


Concept plan

3 Developing the Masterplan

3.1 Over Arching Principles

The masterplan has been developed to encourage less reliance on private car use, and a modal shift towards walking, cycling and public transport use. The area is well served by existing footpath and cycle routes, as well as a range of bus services. The masterplan ensures that the development will utilise and enhance these wherever possible to do so, and offer the greatest possible opportunity for residents to use an alternative to the car. It is proposed to extend the First Bus 23 service to loop through the site, offering a frequent (10 minute) service to Aberdeen City Centre.



Proposed extension to the First Group 23 service, and the other existing First Bus services.

Convenient access is proposed to alternative existing bus routes, such as the First Bus 11 service, and the Stagecoach X17/N17 service that run along Queen's Road to the south. Walking and cycling routes have been carefully designed into the masterplan, and the principles of Designing Streets adopted to seek to reduce traffic speeds and prioritise pedestrians and cyclists.

The site will be accessed by three principle junctions. Upon completion of the plan, the east and west junctions will operate as left-in/left-out only arrangements, with the central junction fully signalised to allow access in all directions. Pedestrian crossing points have

been designed into the plan to mitigate the barrier that the A944 creates between the north and south parts of the development.

The Council's Action Programme does not require a school on the site, and allocates pupils from the development to existing schools in the area. However, to maintain flexibility, the masterplan could provide land for a school in the future, should one be required, in place of housing - in either the north or the south. Similarly land can be made available to provide health care facilities on site if required. Discussions are ongoing with NHS Grampian and the Council in this regard.

The site sits between areas identified in the Local Development Plan as Green Space Network. These are valuable areas for wildlife habitats, ecology and informal recreation. The masterplan seeks to ensure these areas are not fragmented, by providing additional landscaped corridors between the established areas of Green Space Network.



Green Space Network remaining after development of the site.

The masterplan has been carefully designed to utilise the existing topography, with the minimum amount of alteration to the levels required. The steepest parts of the site have been avoided, both in the interests of sustainability and visual impact. Considerable areas of landscape planting are proposed around the site to aid its visual integration with the surrounding area.

The principles set out in Designing Places and Designing Streets have been adopted as central to the design process. With regard to the urban design and to the street design, the masterplan is distinctive, safe and pleasant, easy to move around in, welcoming, adaptable and resource efficient.

The development is also designed to be highly sustainable, with modern house construction techniques incorporated into attractive designs with traditional proportions. Various measures to reduce the carbon emissions of the development have been explored, and the most suitable solution will be identified through the detailed planning process.

3.2 The Three Character Areas

Three distinct character areas have been identified on the Concept Plan. These are as follows: -

Area A sits to the North of the Lang Stracht, and is identified as site OP44 in the Local Development Plan, suitable for 300 houses. The image below shows woodland to the North, and areas of Green Space Network both to the east and west of the area. The layout exploits this relationship and by drawing the landscape into the scheme, opening up ready access to this resource and providing attractive views, and settings for the residents.



Area A

Area B forms the western section of Local Development Plan site OP43, to the south of the A944 Lang Stracht. The Denburn runs along the southern side of the site in a deep gully.

The image below shows the Den of Maidenraig running to the south and south west of the area. The layout exploits this relationship in 2 ways. Firstly by drawing the landscape into the residential area through a series of landscaped courtyards on the south side. There are no gardens backing on to this boundary (so no long line of fences facing the Den), and the courtyards will be designed to maximise these attractive views – including gable windows. Secondly by opening the whole of the south-western boundary (where there are no houses on the Den side of the road) up to the Den.



Area B

Area C forms the eastern part of Local Development Plan site OP43. Sloping from north west down to the south east and the Den of Maidenraig Local Nature Reserve. A group of existing houses adjoining a stream and a deep gully mark the western boundary of this area. It was logical to strengthen this wildlife corridor by adding a swathe of Green Space Network on its eastern edge to improve screening, landscape setting, and avoid undermining the character which the houses currently enjoy.

The steep slopes of the Den of Maidenraig define the southern boundary of the area, and the layout responds to this by drawing the greenery into the housing area through landscaped courtyards. None of the houses in this area back on to the Den (to avoid long lines of garden fencing) and the courtyards are designed to maximise views – including the inclusion of gable windows where appropriate.



Area C

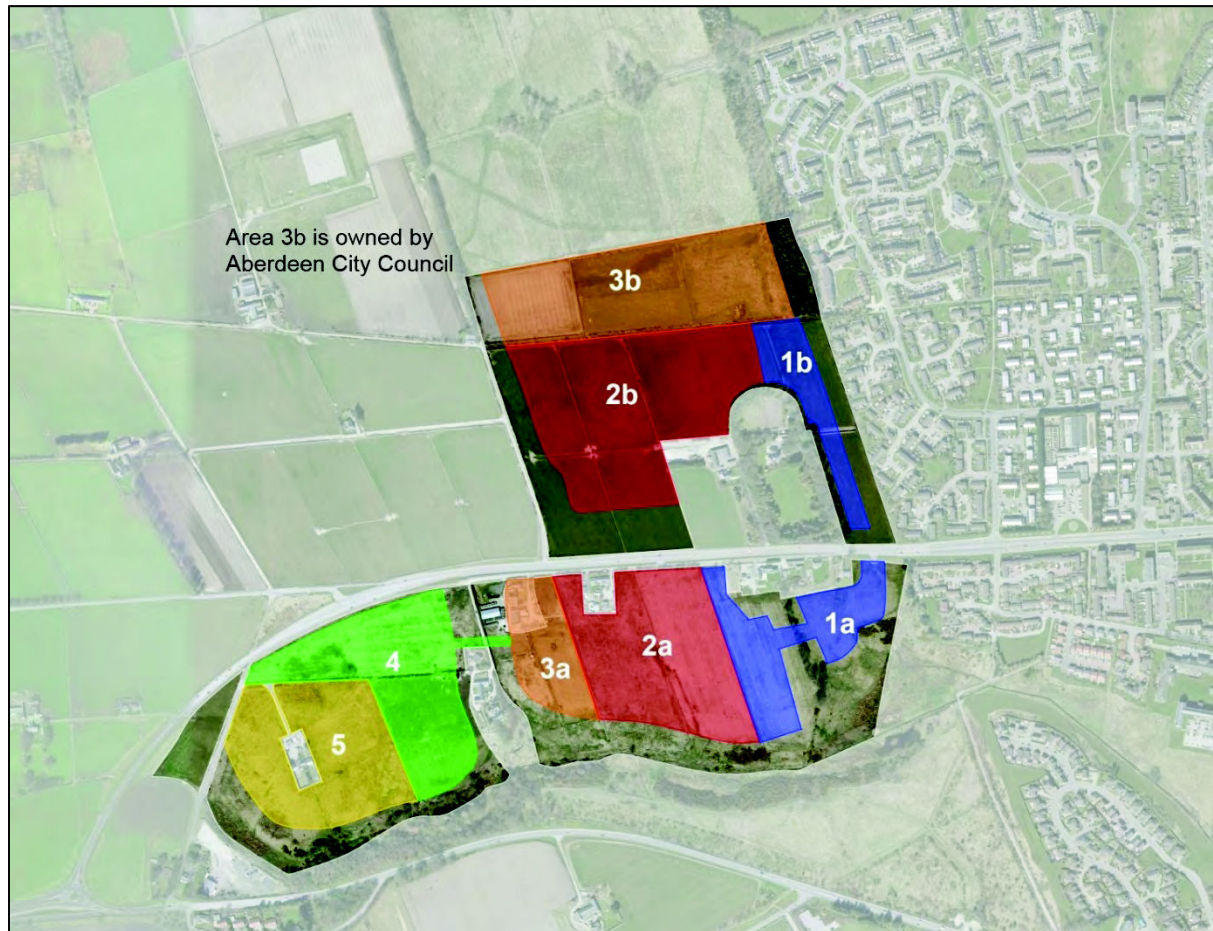
Overall, the masterplan has been developed to provide a sustainable community that responds appropriately to the landscape and the surrounding residential communities, whilst providing the opportunity for new services and facilities which will benefit the existing population as well as the new residents.



The masterplan

4 Delivering the Masterplan

The main report considers in detail the phasing and delivery of the Maiden Craig masterplan, including the required facilities and infrastructure set out in the Council's Action Programme. It also identifies specific strategies for the disposal of foul and surface water drainage, provision of servicing to the site and the incorporation of affordable housing into the development. The phasing plan is shown below, identifying the pattern of development, from east to west.



Phasing diagram (above) and associated numbers (below)

Phase	Number of Units	Number of Affordable Units
Phase 1a	88	22
Phase 1b	78	78
Phase 2a	c. 131	c. 35
Phase 2b	c. 138	
Phase 3a	c. 88	c. 23
Phase 3b	c. 114	c. 19
Phase 4	c. 104	c. 11
Phase 5	c. 78	c. 16
Totals	c. 816 (330 North and 486 South)	c. 204

5 Conclusion

In conclusion therefore, the masterplan report details the proposals for a highly sustainable, attractive, and distinctive development on the western edge of Aberdeen. The proposals are deliverable, and are flexible enough to ensure that requirements for education and/or healthcare facilities can be accommodated in due course if required. The masterplan is respectful of the sensitive areas that bound the site to the north and south, and to the established residential communities to the east. Below are some artists impressions of the development.



Left – an image from the eastern edge of phase 1b, with the landscape buffer on the right between the housing and the existing Sheddocksley area.

Right – residential courtyards are designed to open out into the principle open space areas such as the Den of Maidenraig to the south of the site.



Left – the principle bus route through the site will be appropriately traffic calmed, and will provide safe and convenient facilities for pedestrians and cyclists. Landscape planting will create an attractive and safe character.

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Adoption of Masterplans, Planning Briefs and Development Frameworks as Supplementary Guidance in support of the Aberdeen Local Development Plan
REPORT NUMBER:	EPI/12/203

1. PURPOSE OF REPORT

- 1.1 The purpose of this report is to outline the results of this six week consultation period, and where appropriate the results of the consultation that was carried out alongside the Proposed Local Development Plan, by presenting a summary of the representations received on each document, officers' responses to these representations and to detail any resulting action from this. A summary of this information is attached at Appendix 1 of this report. Appendix 1 contains summarised versions of representations received. Full, un-summarised copies of representations are available on request from the Local Development Plan Team.

2. RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
- a) Note the representations received on the draft Supplementary Guidance documents;
 - b) Approve officers' responses to representations received on the draft Supplementary Guidance documents; and
 - c) Agree, where appropriate for officers to send the requested Supplementary Guidance documents to be ratified by the Scottish Government and
 - d) Agree, where appropriate that the requested Supplementary Guidance documents remain as Local Planning Advice and do not get sent to the Scottish Government for ratification

3. FINANCIAL IMPLICATIONS

- 3.1 There are no direct financial implications arising from this report. Any future publication costs can be met through existing budgets.

4. OTHER IMPLICATIONS

- 4.1 This is the continuation of a significant piece of work that has involved and will impact upon many other council services, public bodies, the business and development industries and the citizens of Aberdeen. An Action Programme has been prepared to consider the implementation of the Aberdeen Local Development Plan and Supplementary Guidance.

- 4.2 The progression of these Supplementary Guidance documents will provide a clear framework for decision making, allowing comprehensive guidance for both applicants and officers, thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development. Detailed area-based Supplementary Guidance also has value in reducing officer time spent on pre-application discussions.

5. BACKGROUND/MAIN ISSUES

- 5.1 The Planning etc. (Scotland) Act 2006 paragraph 22 (1) states that a planning authority may adopt and issue guidance in connection with a local development plan. During the preparation of the Aberdeen Local Development Plan (ALDP 2012) Aberdeen City Council prepared a number of draft Supplementary Guidance (SG) documents in support of the ALDP. Following examination, the Aberdeen Local Development Plan was adopted on 29 February 2012 with a number of Supplementary Guidance documents adopted in the months following. It is intended that further tranches of approved Supplementary Guidance will be forwarded to Scottish Ministers in due course for adoption, whilst other Supplementary Guidance documents will be reported to Committee as they come forward.

- 5.2 This committee report relates to 12 draft SG documents (Masterplans/Planning Briefs/Development Frameworks) which have recently been subject to a public consultation. The draft documents were subject to a six week public consultation period which ran from 16th July to 27th August 2012, some of these documents have also been consulted on as part of the Proposed Local Development Plan.

- 5.3 These 12 draft SG consist of either new Masterplans/Development Frameworks/Planning Briefs which have been prepared as a requirement of the ALDP or are existing Masterplans/Development Frameworks/Masterplans which were carried over from the previous Aberdeen Local Plan (2008) to the ALDP. These 12 draft SG documents are:

- Bon Accord Baths Planning Brief;
 - Cove Charette Report and Masterplan;
 - Dyce Drive Planning Brief;
 - Foresterhill Development Framework;
 - Fire Station Site, North Anderson Drive Planning Brief;
 - Greenferns Development Framework and Masterplan;
 - Kingswells Development Framework;
 - Murcar Development Framework;
 - Pinewood Hazledene Planning Brief;
 - Aberdeen Harbour Development Framework;
 - Kingswells Development Framework and Phase One Masterplan (OP40) and
 - Kingswells Prime Four Business Park (Phase Two and Three)
- 5.4 Due to the size of all of the draft documents, hard copies have not been attached to this report but are available at request from the Local Development Plan team. Full copies of the draft documents can also be accessed from http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_committee_reports.asp
- 5.5 For the SG an assessment of the environmental impact was made alongside the ALDP. Since the adoption of the Plan there has been additional consultation undertaken, additional detail added and amendments made. Therefore, these documents will have to go through the Strategic Environmental Assessment Process. Given the assessment made at the Plan level and the mitigation incorporated into the Plan there would be no likely significant effect as a result of the SG. To ensure that this is properly documented a Screening report has been prepared and submitted to the SEA Gateway and the Consultation Authorities. In the case that any unforeseen issues are raised any affected SG will be reported back to committee.
- 5.6 The existing Masterplans/Planning Briefs/Development Frameworks were previously agreed by Council under the 2008 Aberdeen Local Plan or in some cases even earlier than this. The policies in these Plans and Briefs required to be updated in order to bring them up to date with current national, regional and local policy documents.
- 5.7 At the Enterprise, Planning and Infrastructure Committee on 31st January 2012 it was agreed that officers should update existing Masterplans and Planning Briefs and complete adoption procedures without reporting to Committee for approval. However, should the content of the Plans or Briefs require an alteration to their content, officers were instructed to report these back to Committee for approval. Please see [Appendix 2](#) for a copy of this Committee Minute (para 26).
- 5.8 During the consultation period, representations were received on some of these existing documents which related to their content and so these

existing Masterplans/Planning Briefs/Development Framework have been brought back to this Committee for approval.

- 5.9 Following analysis of the representations received during the consultation, 9 of these draft documents are now proposed for adoption as supplementary guidance to the ALDP. Each document is named and detailed below with a summary of any amendments that have been made to the document either as a result of representations received during the public consultation period or by officers to provide greater clarity.

Adopt as Supplementary Guidance to the ALDP

5.10 Cove Charette Report and Masterplan

Stewart Milne Homes and Scotia Homes jointly commissioned the Princes Foundation for the Built Environment and Urban Design Associates to conduct a participatory planning process involving Cove Community Council and Aberdeen City Council which aimed to develop the most appropriate neighbourhood form for land that was zoned in the ALDP for residential development. This document reports this process and presents a Masterplan which covers OP72: Cove and OP75: Stationfields, Cove. A total of three representations were received, one from Scottish Environmental Protection Agency (SEPA) and two from members of the general public. The issues raised through these representations were not felt to require any amendments to be made to the document.

5.11 Foresterhill Development Framework

This Development Framework covers an area currently zoned as 'Existing Community Facilities' in the Local Development Plan. It proposes a long-term strategy for the future development of the Foresterhill Campus. One representation was received from SEPA. The issues raised through this representation were not felt to require any amendments to be made to the document.

5.12 Fire Station Site, North Anderson Drive Planning Brief

This Planning Brief provides guidance for the future development of the Fire Station Site on North Anderson Drive. This Brief covers an area currently zoned as 'Residential' in the Local Development Plan. One representation was received from Mastrick, Sheddocksley and Summerhill Community Council. The issues raised through this representation were not felt to require any amendments to be made to the document.

5.13 Greenferns Development Framework and Masterplan

The Greenferns Development Framework covers both OP39: Greenferns and OP45: Greenferns and sets out a 2-dimensional spatial framework for the way in which these sites should be developed. The Greenferns Masterplan covers only site OP39: Greenferns. One representation was received from SEPA. The issues raised through

this representation were not felt to require any amendments to be made to the document.

5.14 Kingswells Development Framework

This Development Framework covers OP41: Kingswells C and OP42 Kingswells D and West Huxterstone. It sets out a guide for the preparation of a masterplan for a new, attractive, high quality, and sustainable residential development. A total of three representations were received, two from SEPA and one from Kingswells Community Council. The first representation from SEPA resulted in the addition of a paragraph to the document regarding existing water features within and around the site and the pressures which apply to these features. The second representation from SEPA did not require any changes to be made. Some of the issues raised through the Kingswells Community Council representation resulted in some minor text changes being required to be made to the document.

5.15 Pinewood Hazledene Planning Brief

This Planning Brief covers OP52: Hazledene and OP57: Pinewood. The major component of the Brief is the creation of a residential development but the proposals also incorporate community facilities, neighbourhood shopping, community open space and a network of pedestrian and cycle paths. One representation was received from GVA Grimley on behalf of Dobbies Garden Centre. The issues raised through this representation were not felt to require any amendments to be made to the document.

5.16 Aberdeen Harbour Development Framework

This Development Framework was prepared by Aberdeen Harbour Board following engagement with Aberdeen City Council. The Framework explores how the Harbour and the City can develop in partnership over the next 20 years. One representation was received from SEPA. The issues raised through this representation were not felt to require any amendments to be made to the document.

5.17 The Aberdeen Harbour Development Framework has been subject to a Habitats Regulation Assessment (HRA). According to Article 6(3) and 6(4) of the Habitats Directive (92/43/EEC) any formally adopted Supplementary Guidance which would be likely to have a significant effect on an identified European Site shall be subject to an 'appropriate assessment' of its implications, also known as a Habitats Regulations Appraisal. The aim of the assessment was to assess the likely impacts of the Aberdeen Harbour Development Framework against the conservation objectives of the River Dee Special Area of Conservation (SAC) and the Moray Firth SAC. The screening process identified that some of the Framework proposals or objectives would be likely to have significant effects on the qualifying interests of the River Dee SAC and Moray Firth SAC, either in isolation or cumulatively. However, the Appraisal also suggested a number of mitigation measures. The inclusion of these mitigation measures (page 65 of the document) and

the potential need for further specific project-based assessment would ensure that the Framework would have no adverse affect on the integrity of the River Dee SAC and the Moray Firth SAC.

5.18 Kingswells Development Framework and Phase One Masterplan (OP40)

This Development Framework and Phase One Masterplan was prepared by Drum Property Group to guide development of a new employment destination to the West of Kingswells. This document covers OP40: West Hatton and Home Farm, Kingswells. One representation was received from SEPA. The issues raised through this representation were not felt to require any amendments to be made to the document.

5.19 Kingswells Prime Four Business Park (Phase Two and Three)

This Masterplan was also prepared by Drum Property Group and represents further detailed information on Phases Two and Three of OP40: West Hatton and Home Farm, Kingswells. One representation was received from Kingswells Community Council. Some of the issues raised through this representation resulted in some minor text changes being required to be made to the document.

5.20 Following analysis of the representations received during the consultation period and the status of each document in terms of its connection to the ALDP, 3 of these documents are proposed to be kept as Local Planning Advice. Each of these documents is named and detailed below along with the reason for not adopting it as SG to the ALDP.

Keep as Local Planning Advice and do not take forward as Supplementary Guidance to the ALDP

5.21 Bon Accord Baths Planning Brief

This Planning Brief was approved by Council in October 2009 and covers the Bon Accord Bath site, which was declared surplus to Council requirements in 2008. This Brief sets out the planning framework that will guide the development process. No representations were received on this document during the consultation process. This document cannot be adopted as SG to the ALDP as there is no definitive link between this document and the ALDP. Under the Town and Country Planning (Development Planning) (Scotland) Regulations 2008 SG may only deal with the provision of further information or detail in respect of the policies or proposals set out in that Plan and only deal with those matters which are expressly identified in a statement contained in the plan. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document. Local Planning Advice can still be used as a material planning consideration during the determination of

planning applications; however, it does not carry as much weight as the ALDP or its associated SG.

5.22 Dyce Drive Planning Brief

This Planning Brief was approved by Council in March 2004 and covers an area of land to the south west of Aberdeen Airport. A total of four representations were received during the consultation period, three from a member of the public and one from Miller Developments. This Brief was written prior to the adoption of both the Aberdeen Local Plan 2008 and the Aberdeen Local Development Plan 2012. It covers part of the site now zoned as OP32: Dyce Drive in the ALDP, but does not show the full extent of surrounding development and allocations and does not represent the up to date boundary of OP33:A96 Park and Rides site. This Brief requires to be updated to bring it in line with the current land zonings and allocations. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated. The need to update this document would not affect any current Planning Applications. Local Planning Advice can still be used as a material planning consideration during the determination of planning applications; however, it does not carry as much weight as the ALDP or its associated SG.

5.23 Murcar Development Framework

This Planning Brief was approved by Council in June 2008 and covers an area of land to the north east of Bridge of Don. Two representations were received during the consultation period, one from Montagu Evans on behalf of Buccleuch Property and one from Paull and Williamsons on behalf of Stewart Milne Homes. This Brief was written prior to the adoption of the Aberdeen Local Development Plan 2012. It covers OP4: Findlay Farm, Murcar, OP3: Berryhill, Murcar and part of OP2: Murcar, but does not show the full extent of surrounding development and allocations. This Brief requires to be updated to bring it in line with the current land zonings and allocations. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated. Local Planning Advice can still be used as a material planning consideration during the determination of planning applications; however, it does not carry as much weight as the ALDP or its associated SG.

6. IMPACT

6.1 The Local Development Plan and associated Supplementary Guidance contribute to the following Single Outcome Priorities:

1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2. We realise our full economic potential with more and better employment opportunities for our people; 10. We live in well designed, sustainable places where we are able to access the

amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.

- 6.2 The Local Development Plan and associated Supplementary Guidance supports the Council's Corporate Plan, in particular the aim of delivering high levels of design from all development, maintaining an up to date planning framework, promoting sustainable development and ensuring an appropriate open space provision.
- 6.3 The Local Development Plan and associated Supplementary Guidance contribute to "Aberdeen – A Smarter City" in terms of providing a clean, safe and attractive streetscape and by promoting biodiversity and nature conservation, encouraging wider access to green space in our streets, parks and countryside, improving access to affordable housing in the private sectors by working with developers to maximize the effective use of developer contributions.
- 6.4 The proposal is consistent with the Planning and Sustainable Development Service Plan, in particular engaging the community in the planning process, and the delivery of Masterplans/Development Frameworks in line with the Aberdeen Masterplanning Process.
- 6.5 Given the wide range of policy areas the Aberdeen Local Development Plan and associated Supplementary Guidance covers, an Equality and Human Rights Impact Assessment was carried out on the Proposed Plan, it showed that there will be some positive impacts of the Plan on a range of equalities groups. The Equality and Human Rights Impact Assessment is available to view on the City Council's website at http://www.aberdeencity.gov.uk/Planning/ldp/pla_aldp_tech_appendix.a_sp or a hard copy can be obtained from the Local Development Plan Team.

7. BACKGROUND PAPERS

- Appendix 1 - Summary of representations received, officers' responses and any resulting amendments
- Appendix 2 – Minute from the Enterprise, Planning and Infrastructure Committee (31st January 2012)

Aberdeen Local Development Plan

<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=42278&SID=9484>

Aberdeen Local Development Plan –Action Programme

http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_action_programme.asp

Aberdeen City and Shire Strategic Development Planning Authority: Aberdeen City and Shire Structure Plan

<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=423&SID=149>

Planning etc. (Scotland) Act 2006

http://www.opsi.gov.uk/legislation/scotland/acts2006/asp_20060017_en_1

Scottish Planning Series: Planning Circular 1/2009: Development Planning

<http://www.scotland.gov.uk/Resource/Doc/261030/0077887.pdf>

The Town and Country Planning (Development Planning) (Scotland) Regulations 2008

http://www.opsi.gov.uk/legislation/scotland/ssi2008/ssi_20080426_en_1

8. REPORT AUTHOR DETAILS

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Appendix 1 – Summary of Representations

Supplementary Guidance	Respondent	Summary of Representation	Officers Response	Amendments made as a result of Representation	Action
Existing Masterplans/Planning Briefs/Development Frameworks					
Bon Accord Baths Planning Brief	No representations received	N/A	This Planning Brief was approved by Council in October 2009 and covers the Bon Accord Bath site, which was declared surplus to Council requirements in 2008. This document cannot be adopted as SG to the ALDP as there is no definitive link between this document and the ALDP. Under the Town and Country Planning (Development Planning) (Scotland) Regulations 2008 SG may only deal with the provision of further information or detail in respect of the policies or proposals set out in	N/A	Keep as Local Planning Advice

Appendix 1 – Summary of Representations

<p>Cove Charrette Report and Masterplan</p>	<p>Wim Gonweleeeuw</p>	<p>Object to any development of housing in the Loirston Green area next to Earnshugh road. If anything site should be used for recreation.</p>	<p>that Plan and only deal with those matters which are expressly identified in a statement contained in the plan. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
			<p>The Charrette covers two distinctive areas in Cove, one close to Loirston Loch and the other close to the railway line. There is a conceptual drawing on page 7 of the document highlighting how a proposed link may be achieved between these areas and the existing developed area which uses Loirston Green. Loirston Green is not</p>		

Appendix 1 – Summary of Representations

<p>Cove Charrette Report and Masterplan</p>	<p>SEPA</p>	<p>This area is in proximity to Loriston Loch and the East Tullos Burn (although both are outwith the boundary of the Charrette). The East Tullos Burn is in a very poor condition due to pressures from heavy modifications, diffuse and point source pollution. The Charrette provides little context in terms of the water environment within the boundary of the sites and in the surrounding area. It is requested that the Charrette document be amended to take account of the existing water features within and around the site and the pressures which apply to these features, and to direct developers to look for</p>	<p>an area for development within the Charrette document and is zoned as urban green space and green space network within the Aberdeen Local Development Plan.</p> <p>The comments raised would be more suitably addressed through the planning application process. The Cove Charrette discussed two areas within Cove. Part A sits close to Loiston Loch and Part B close to the railway line. At present part A is subject to three planning applications which cover the site. SEPA have been consulted on these planning applications and have outlined in their response a number of conditions that would be required to satisfy issues concerning water</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
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Appendix 1 – Summary of Representations

Cove Charette Report and Masterplan	Graham John Mackie	<p>opportunities to protect and improve the waterbodies.</p> <p>Part of the area covered by the Cove Charrette lies in close proximity to a licensed landfill site which is known to be actively producing gas, although the document highlights that a waste management licence is still in place, we request that the implications of this be clarified. We recommend that the Charrette document be amended to clarify that a Waste Management Licence is still in place over part of the site and any development must be preceded by suitable remediation and gas risk assessments.</p>	bodies and the proximity to the landfill site.	No amendments required.	Send to Scottish Govt for ratification
		I am sad to see the plans for the vast number of houses which are to be built directly across from my house. This will obliterate the beautiful views from my windows, block daylight, increase noise traffic and	The Charette Process and the ethos of the modern planning system propose to increase the design quality of new developments. Page 32 of the Charette		

Appendix 1 – Summary of Representations

		<p>pollution. I would be willing to support the masterplan if the following concerns were upheld: dykes, hedgerows, trees retained. Reasonable open space between roads and first row of hedges. For privacy new houses built end on to road and when developers are given the go ahead there will be no last minute changes to the masterplan.</p>	<p>document states 'natural features are protected and celebrated, where possible, by crafting unique spaces around them. The consumption dyke is one example of this'. Other features of the area which add to the character are likely to be retained and enhanced. Open space requirements are also stated in policy within the local development plan. Issues regarding loss of day light, traffic noise and pollution would be examined in a planning application however as the objective is to create sustainable communities it is expected that the increase in traffic movement and therefore pollution would not increase</p>		
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<p>Dyce Drive Planning Brief</p>	<p>Graeme Stewart</p>	<p>Regarding Section 18.1: 'reinstatement of the footbridge across Dyce Drive.' This would present further security issues, Escape routes for burglars, etc. Greenburn Cottage residents et al, would require to be involved in design process from earliest possible stage.</p>	<p>significantly. A planning application still has to be submitted for development even if there is a masterplan for the site. It is expected that the planning application would have regard to the masterplan but it is still possible that minor difference could occur between the masterplan and planning application.</p>	<p>No amendments currently required.</p>	<p>Keep as Local Planning Advice</p>
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			<p>not represent the up to date boundary of OP33:A96 Park and Rides site. This Brief requires to be updated to bring it in line with the current land zonings and allocations. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated. The need to update this document would not affect any current Planning Applications.</p> <p>The Planning Brief suggests that, given the existence of a Right of Way through part of the site, it will</p>		
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Appendix 1 – Summary of Representations

<p>Dyce Drive Planning Brief</p>	<p>Graeme Stewart</p>	<p>Regarding Section 16.1: 'The internal distributor roads (excluding the new spine road, which will provide access to the Airport Terminal) should discourage through traffic, excepting public transport.' Would this be redirected through Rowett North,</p>	<p>be appropriate to investigate ways to improve and enhance this link. This may include the reinstatement of the footbridge across Dyce Drive, but further study will be necessary to determine the best means of integrating the route with the proposed development. The exact nature of any enhancements to the route should take into account the desirability of creating a safe and secure route.</p>	<p>No amendments currently required.</p>	<p>Keep as Local Planning Advice</p>
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Appendix 1 – Summary of Representations

		<p>Greenburn Cottage and other residential dwellings? Greenburn Cottage residents et al, would require to be involved in design process from earliest possible stage. Agree with density figures.</p>	<p>show the full extent of surrounding development and allocations and does not represent the up to date boundary of OP33:A96 Park and Rides site. This Brief requires to be updated to bring it in line with the current land zonings and allocations. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated.</p> <p>Discouraging through traffic on internal distributor roads is good practice to avoid creating 'rat-runs' and</p>	

Appendix 1 – Summary of Representations

<p>Dyce Drive Planning Brief</p>	<p>Graeme Stewart</p>	<p>Road traffic congestion would be better eased through a choice of alternative routes/exit points, rather than an upgraded single route.</p>	<p>minimise traffic levels on local roads. It is not intended that any major traffic would be redirected through the Rowett North (OP28).</p>	<p>No amendments currently required.</p>	<p>Keep as Local Planning Advice</p>
<p>This Brief was written prior to the adoption of both the Aberdeen Local Plan 2008 and the Aberdeen Local Development Plan 2012. This Brief covers part of the site now zoned as OP32: Dyce Drive in the ALDP, but does not show the full extent of surrounding development and allocations and does not represent the up to date boundary of OP33:A96 Park and Rides site. This Brief requires to be updated to bring it in line with</p>					

			<p>the current land zonings and allocations. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated.</p> <p>It is acknowledged that no single transport measure will be sufficient to address congestion in the area. The proposed spine road between the A96 and the airport terminal is one of a number of measures, including delivery of the A96 Park and Ride facility, the Aberdeen western Peripheral Route and improved public</p>		
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Appendix 1 – Summary of Representations

<p>Dyce Drive Planning Brief</p>	<p>Miller Developments</p>	<p>Welcome the decision of Aberdeen City Council to carry over the provisions of the approved Dyce Drive Planning Brief (2004) into the new LDP. However, there is concern of the wording of Policy BI2 in the new plan, which states that within areas designated as 'Specialist Employment Areas', only Class 4 Business uses shall be permitted. This is a significant departure from the approved Planning Brief, which allocates our Dyce Drive site for a (Class 4) Business Park with associated Class 5 & 6 uses. Companies want to</p>	<p>transport facilities, which represent an integrated package intended to collaboratively relieve traffic congestion.</p>	<p>No amendments currently required.</p>	<p>Keep as Local Planning Advice</p>
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Appendix 1 – Summary of Representations

		<p>integrate their office functions with covered storage and yard areas. This is normally the case even with headquarter functions and is evidenced in the requirements of companies who have looked at the Dyce Drive area in recent years. To remove the 'and ancillary Class 5 & 6 uses' presents a serious threat to the viability of the proposed Business Park and we would strongly request that the LDP reinstates the original Planning Brief wording. We are fully supportive of the Council's aspirations for this area which, with the new Dyce Drive Link Road, will form an important and visible gateway between the airport, the AWPR and the City but this aspiration should still be achievable through policies which seek, for example, to locate the more attractive office</p>	<p>the current land zonings and allocations. It is therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated.</p> <p>The Brief indicates that development will generally be restricted to those falling within class 4 of the use classes order, but that other uses (such as classes 5 and 6, for example) would be permitted where 'they are necessary and maintain the required high quality environment'. The 'specialist employment'</p>	
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Appendix 1 – Summary of Representations

Foresterhill Development Framework	SEPA	<p>functions along the Link Road corridor, with any associated facilities facing internally into the park.</p> <p>No detailed comments to make on the document and note that much of the work is underway, but would highlight the issues set out in Appendix 1 (of SEPA's representation) as being useful for any proposals coming forward as part of stage 2 of the project.</p>	<p>designation expresses a similar sentiment, albeit in different terms, with an emphasis on class 4 uses, but an acknowledgement that ancillary facilities may be permitted where it can be demonstrated that they would enhance the attraction and sustainability of the area for business investment.</p> <p>Comments noted.</p>	No amendments required.	Send to Scottish Govt for ratification
Fire Station Site, North Anderson Drive Planning Brief	Mastrick, Sheddocksley and Summerhill Community Council	<p>We recognise the recent construction of a new Fire Station on this site but have concerns regarding the future of the rest of this site, should Grampian Fire &</p>	<p>Should the Fire Service decide to do something different with the site then we would agree that the Brief may need to be</p>	No amendments required.	Send to Scottish Govt for ratification

Appendix 1 – Summary of Representations

<p>Greenferns Development Framework and Masterplan</p>	<p>SEPA</p>	<p>Rescue Service decide to relocate its headquarters at any future date.</p> <p>We would welcome a review of this Planning Brief. The existence of a new Fire Station on site was not envisaged when the original brief was drawn up and the suitability of housing co-located on this site would need to be carefully considered in light of this. In addition, the proposed site layout would require to be updated with regard to proximity of buildings and changed access arrangements.</p> <p>We would welcome having appropriate input into the process on behalf of the wider community and in addition to those who live in close proximity to the site.</p> <p>The Bucks Burn runs along northern boundary of OP45 and is at moderate status because of alterations to</p>	<p>revisited. However, we are not aware of any plans the Fire Service have for this site since their decision to remain there. To amend the Brief we would need to know for instance, if the Fire Service wished to remain on part of the site or not. In the absence of any particular development pressure we would not wish to revisit the Brief at this time. If however, the Brief is revisited in future then consultation with the wider community would be required.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
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Appendix 1 – Summary of Representations

		<p>beds and banks and diffuse pollution. It is noted that SG 9.5 Masterplan for Greenferns makes no clear reference to water features. It is requested that the Masterplan be amended to take account of the existing water features within the site and the pressures which apply to these features, and to direct developers to look for opportunities to protect and improve the waterbodies.</p>	<p>Council in January 2010 covers only site OP39 Greenferns (residential opportunity to provide 120 homes). Site OP45 Greenferns which SEPA refer to in their representation is covered by the Greenferns Development Framework, which was also approved by Aberdeen City Council in January 2010. The Development Framework sets out a baseline or 2-dimensional spatial framework, for the way in which OP39 and OP45 should be developed. The Development Framework makes specific reference to the Bucks Burn as an existing feature that should be retained (page 53) and states that "Throughout the</p>		
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				<p>process of developing a framework for Greenferns it has been the aim to provide the highest quality place to live, supported by the highest environmental aspirations. In the context of this, one of the main elements has been to enhance and protect the Bucks Burn corridor, utilising its potential as an environmental route, supporting enhanced ecological and habitat activity, while bringing it literally to the doorstep of the inhabitants of Greenferns" (page 80). Any future Masterplan that is developed for OP45 Greenferns will comply with this Development Framework and will take account of existing water features.</p>		
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Appendix 1 – Summary of Representations

<p>Kingswells Development Framework</p>	<p>SEPA</p>	<p>The Den Burn, which is in close proximity to the site, is at poor ecological potential site due to sewage pollution and watercourse modifications. Kingswells developments may also impact on Bucks Burn which are at moderate ecological status because of culverting and diffuse pollution. While the Supplementary Guidance does highlight the need for development proposals to not cause detriment to water quality or ecology in general terms, we request that it be expanded to take account of the existing water features within and around the site and the pressures which apply to these features, and to direct developers to look for opportunities to protect and improve the water environment.</p>	<p>This paragraph was added into the document prior to the latest round of consultation.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
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Appendix 1 – Summary of Representations

<p>Kingswells Development Framework</p>	<p>SEPA</p>	<p>Welcome the recognition of the North Burn of Rubislaw as an important feature for the site and welcome the proposals to retain and enhance the Burn in the vicinity of the site. We note from Section 5.21 of the document that foul drainage will be directed to the public sewer and surface waters will be disposed of via SUDS.</p>	<p>Comments noted.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
<p>Kingswells Development Framework</p>	<p>Kingswells Community Council</p>	<p>Page 3 1.1 Site "A". This site has now been fully developed by Barratts with 33 houses. It is now completed (Newton). Site "D" and "E". This site is no longer an opportunity site (OP41) as it is in the midst of being developed with 72 houses by Stewart Milne Group (West One). Site "B" is in the midst of a master planning process for 120 houses (Huxterstone). Site "C" (The Fairley Road</p>	<p>The Development Framework was written in 2008 to help inform the preparation of future Masterplans for the various sites in Kingswells by setting out the broad principles that need to be considered within the Masterplans. These subsequent Masterplans provide more detailed information to guide and shape development. It is not felt to be necessary to</p>	<p>Remove reference to a mobile library service on page 3. The reference to a youth facility on page 6 will be removed and the sentence will be amended to read, "<i>The old primary school is now being used by</i></p>	<p>Send to Scottish Govt for ratification</p>

Appendix 1 – Summary of Representations

		<p>Old School playing field) should not be designated as Residential H1 in the Aberdeen Local Development Plan. KCC have instigated a land search which is inconclusive and complicated and the field may well have three landowners and partly be 'in trust'. Developer Contribution has been agreed for the field from planning gain from site "D" and "E". The field is to be flattened and drained for community recreational and sporting use. This site should now be re-designated to protect this valuable community asset.</p> <p>Page 3 1.2 The mobile library service has now been discontinued throughout the city, and therefore, Kingswells.</p> <p>Page 6 5.4 <i>"The old primary school is currently used as a youth</i></p>	<p>update the Development Framework to reflect evolving site status as the Masterplans incorporate the principles set out within the Framework. It is important to keep all of the opportunity sites (whether they are now developed or not) to show the original scope of the Development Framework.</p> <p>The H1 Residential zoning of Site C is appropriate and reflects the sites position within an existing residential area. Policy H1 allows for the development of appropriate complimentary uses such as community recreational and sporting as long as the proposal complies with</p>	<p><i>Adventure Aberdeen."</i></p> <p>References to bus routes will be updated in accordance with the First Bus review.</p>
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Appendix 1 – Summary of Representations

		<p><i>facility.</i>” This building was taken over and renovated by ACC’s “Adventure Aberdeen” in 2009. The youth group was then made “homeless” and disbanded.</p> <p>Page 7 5.14 <i>“New buildings should be designed to have a designed relationship with existing buildings. They should not turn their backs or leave blank facades to those on Old Skene Road and Fairley Road and should be designed to relate to a street, with clear definition between public and private space”</i> KCC would like to point out that on the north of Site B, (Huxterstone) the established houses along the Lang Stracht have their backs to the road. Any houses planned for Site B should follow this precedent. New houses would be inclusive to the site only by facing an inner</p>	<p>other policies in the Local Development Plan.</p> <p>The reference to mobile library service will be removed.</p> <p>The reference to a youth facility will be amended to read, <i>“The old primary school is now being used by Adventure Aberdeen.”</i></p> <p>It is important that dwellings front onto streets to provide active frontages. Active frontages promote a safer and livelier place that benefits the whole community.</p> <p>References to bus routes will be updated in accordance with the First Bus review.</p> <p>Core Path 31 is</p>		
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Appendix 1 – Summary of Representations

		<p>road within the development rather than segregated by facing outwards to the Land Stracht.</p> <p>Page 11 6.5 Kingswells is currently served by the number 14 bus service only and not as stated 14/14A. (The number 14 is to be replaced by the number 11 in September 2012.) The number 14 bus service is a half hourly service during the day and hourly in the evenings - not as stated “operates at a frequency of once every 15 minutes Monday to Saturday during the daytime, with the 14A supplementing the Park and Ride service during peak times giving 8 buses per hour.” There is no longer a 40 A bus service from the Park and Ride. There is no longer a 902 weekend night bus service.</p> <p>Page 10 6.2</p>	<p>designated as a Core Path in Aberdeen City Council’s Core Path Plan (2009) and it is therefore appropriate for it to be referenced in the Development Framework.</p>		
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Appendix 1 – Summary of Representations

<p>Murcar Development Framework</p>	<p>Montagu Evans LLP on behalf of Buccleuch Property</p>	<p>Core Path 31 has no pavement at east side of the north end of Fairley Road to access village centre and bus stop.</p>	<p>Previous references to the Science and Energy Park should be updated to Aberdeen Energy Park.</p> <p>We are keen to ensure that the landscaping to be provided is appropriate for the development and that a landscape masterplan is agreed and fair to all parties. The balance of landscape masterplanning should not be left to the end to be provided by the final development.</p> <p>In 5.3 there is reference to the Reporters recommendations, following the Local Plan Inquiry, for “extensive landscaping</p>	<p>This Brief was written prior to the adoption of the Aberdeen Local Development Plan 2012. This Brief covers OP4: Findlay Farm, Murcar, OP3: Berryhill, Murcar and part of OP2: Murcar, but does not show the full extent of surrounding development and allocations. This Brief requires to be updated to bring it in line with the current land zonings and allocations. It is therefore recommended that this document is not</p>	<p>No amendments currently required.</p>	<p>Keep as Local Planning Advice</p>
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Appendix 1 – Summary of Representations

		<p>within the northern boundary of OP94 Berryhill” although apart from areas associated with the access road, this has not been reflected in the indicative Framework Plan. It has not been accommodated within the landscape masterplan relative to Application Ref. 121031 either. The referenced Parkland Buffer to the west appears to be accommodated in the framework plan.</p> <p>Other issues to be accommodated include the buffer to the golf course and retention of existing planting and natural features. The indicative plan should aim to provide a more coherent landscape structure in order to assist in ensuring a coherent landscape masterplan across the whole site.</p> <p>Support the requirement for habitat surveys to be</p>	<p>sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated.</p> <p>The issues raised through this representation will be taken into consideration during any redrafting/updating work that takes place on the Development Framework in the future.</p>		
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Appendix 1 – Summary of Representations

				<p>consistent across the whole framework site, given the intention is to create a green network.</p> <p>We agree that the framework site should be developed in accordance with the principles of sustainability. It will be important to appreciate how this may affect the deliverability, cost plan and construction phasing across the site.</p> <p>Buccleuch Property are currently working up a masterplan for their future expansion land, which will include a phasing plan, in accordance with previous conditions relative to their land. The phasing of the various sites will also be an important planning consideration. The interconnectivity of road, pedestrian and cycle access, landscaping and services will all require to be</p>		
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Appendix 1 – Summary of Representations

				<p>carefully phased and agreement reached between the respective land owners and Aberdeen City Council.</p> <p>A green travel strategy should be developed for the masterplan site.</p> <p>Clarification is sought on the requirements to extend bus services and the provision of a bus gate link. How will this impact on the Berryhill Farm and Finlay Farm lands?</p> <p>We note that the main vehicular access to the site would be from the Murcar Roundabout. It is envisaged that there will be through access between the Murcar and Parkway Roundabouts, limited to public transport only. The previous approval to extend the Science and Energy Park was approved on that basis.</p>		
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Appendix 1 – Summary of Representations

				<p>Buccleuch Property are currently working up a masterplan for their future expansion land, to the north of the existing Energy Park. We wish to reserve our position on connectivity between the Berryhill Farm land and Finlay Farm land until such times as further discussions have taken place and agreement reached on an appropriate link between the two sites. The location of the link road will need to be agreed in order to interconnect the two masterplans seamlessly. The connecting road between Finlay Farm and Berryhill Farm lands, as identified within Figure 3, is therefore for indicative purposes only.</p> <p>Potential linkages across the framework area should be developed and travel corridors agreed.</p> <p>The framework document</p>
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Appendix 1 – Summary of Representations

				<p>should be updated in order to consider how the de-trunking of the A90 will affect the development. Will an additional access point be promoted to serve the central area of Cloverhill Farm? Clarification is sought on how this may impact / inform the strategies developed for travel links, environmental and landscaping aspects for the framework site.</p> <p>Park and Ride proposals should be updated and clarified for the masterplan site. It will be important to understand the impacts arising from a Park and Ride facility being developed on the site, including walking distances</p> <p>It will be important to ensure consistency between the Council's Core Paths Plan and the proposed footpaths and cycle routes within the Murcar area. It should be possible to provide the</p>	
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Appendix 1 – Summary of Representations

		<p>aspirational path connections contained within the Core Paths Plan across the Framework site.</p> <p>The provision of strategic utility upgrades should be taken into account with the routes and service provision calculated for the whole of the masterplan site and enabled in such as way as to avoid unnecessary cost.</p> <p>Drainage from the wider development may require to pass through the Finlay Farm site. We would not wish to sterilise parts of the site due to drainage wayleaves or cordon sanitaire requirements for third parties.</p> <p>Buccleuch Property and Scottish Enterprise will seek to ensure a fair and consistent approach to any planning restrictions to be applied across the framework site. Whilst</p>			
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Appendix 1 – Summary of Representations

<p>Murcar Development Framework</p>	<p>Paul and Williamsons on behalf of Stewart Milne Homes</p>	<p>research and development uses will continue to make an important contribution to the future development of the Aberdeen Energy Park and associated expansion land to the north, it will be essential to ensure that restrictions on use are reasonable and do not obstruct or hinder future investment opportunities</p> <p>The SG does not take into account any other allocated sites in the area. The SG should contain a requirement for the preparation of a joint Development Framework in conjunction with OP25 and Site Ref 2/02 Mundurno should be included. SG should be the most up to date guidance and reflect the allocations in the LDP in due course. A development framework would help deliver better phased development.</p> <p>The SG should be updated</p>	<p>This Brief was written prior to the adoption of the Aberdeen Local Development Plan 2012. This Brief covers OP4: Findlay Farm, Murcar, OP3: Berryhill, Murcar and part of OP2: Murcar, but does not show the full extent of surrounding development and allocations. This Brief requires to be updated to bring it in line with the current land zonings and allocations. It is</p>	<p>No amendments currently required.</p>	<p>Keep as Local Planning Advice</p>
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Appendix 1 – Summary of Representations

		<p>to incorporate and reflect the other allocations in the Plan.</p>	<p>therefore recommended that this document is not sent to the Scottish Government for ratification as SG to the ALDP, but instead remains as a Local Planning Advice document until such time as it is revisited and updated.</p> <p>The issues raised through this representation will be taken into consideration during any redrafting/updating work that takes place on the Development Framework in the future.</p>		
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Appendix 1 – Summary of Representations

<p>Pinewood Hazledene Planning Brief</p>	<p>GVA Grimley Ltd on behalf of Dobbies Garden Centres LLP</p>	<p>This SG should continue to recognise all available opportunities to link the two areas (Hazledene/ Pinewood and Former Dobbies Garden Centre) for pedestrians and cyclists. This would be advantageous to the future development of both areas.</p>	<p>Given that the sites at Pinewood and Hazledene both now have planning permission, we do not intend to revisit the existing planning brief. Therefore the linkages mentioned in the brief remain unchanged.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
<p>New Masterplans/Planning Briefs/Development Frameworks</p>					

Appendix 1 – Summary of Representations

<p>Aberdeen Harbour Development Framework</p>	<p>SEPA</p>	<p>We have already provided comments during the preparation of the Framework and we are pleased to note that many of the suggestions made previously by SEPA have been incorporated into the document. There is still scope for the natural environment (particularly the water environment) to be further emphasised, for example by highlighting that the River Dee may be protected not just as a Special Area of Conservation but also in order to meet the requirements of the EC Water Framework Directive to ensure that all waterbodies reach good ecological status by a set deadline, and by highlighting the pressures and impacts that are influencing the status of the mouth of the River Dee (including the harbour area).</p>	<p>Comment noted.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
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Appendix 1 – Summary of Representations

<p>Kingswells Development Framework and Phase One Masterplan (OP40)</p>	<p>SEPA</p>	<p>Pleased to note that Cults Burn has been identified as an important feature for the site and welcome the proposals to retain and enhance the Burn in the vicinity of the site through the creation of an open space buffer strip extending along the burn corridor. We welcome the recognition of the need for a Flood Risk Assessment (FRA) to be undertaken, this should inform the design and layout of the site. Note from Section 5 of the document that foul drainage will be directed to the public sewer and surface waters will be subject to 2 levels of treatment for roads and a single level for roof run off, and that an indication that ponds/basins will be incorporated into the drainage strategy within areas of open space. This is in principle acceptable to SEPA as it would provide the required 2 levels of</p>	<p>Comment noted.</p>	<p>No amendments required.</p>	<p>Send to Scottish Govt for ratification</p>
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Appendix 1 – Summary of Representations

<p>Kingswells Prime Four Business Park (Phase Two and Three)</p>	<p>Kingswells Community Council</p>	<p>SUDS treatment for this type of development. We have already provided advice on a pre-application enquiry (Council Ref P120340, SEPA Ref PCS/119104) for part of this site.</p>	<p>Summary Document: It is noted that there is a 90m band of GSN including an open setting to the Consumption Dyke, and a 120m no build zone. Kingswells Community is concerned about the appearance of the development when viewed from the north. The community was given assurances that the development would be screened and would have minimal visual impact. It would appear that the 90m open setting limits the screening that was promised. If this is the case then the 120m no build zone should be extended to ensure that promises made</p>	<p>Summary Document: During the Examination into the Aberdeen Local Development Plan the Reporter stated, "To the north, this development would be contained within the line of the large consumption dyke. It is a scheduled monument and both it and its immediate setting should be safeguarded. This can be secured through the green space network designation which runs along either side of it. The extent of the green space network shown on the proposals map</p>	<p>Remove 'where possible' from sentence at Part 4B. Design Principles and sentence at Part 4.B.1.3 Context. References to bus routes will be updated in accordance with the First Bus review.</p>	<p>Send to Scottish Govt for ratification</p>
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Appendix 1 – Summary of Representations

		<p>is indicative, and its precise extent will be confirmed through the subsequent master planning process.” Historic Scotland have intimated that they do not want landscaping right up to the consumption dyke and this is reflected in the phase 2 and 3 masterplan.</p> <p>Design Principles: We agree that the phrase ‘where possible’ should be removed from the text at Part 4B. Design Principles.</p>	<p>to the community can be fulfilled. The significant deviation from the Framework is not acceptable to the community. The masterplan should not be approved until community issues are addressed.</p> <p>Design Principles: KCC request the following text change in section Part 4B. Design Principles. Buildings must, where possible, be set within the landscape and be sympathetic to the rural setting and the West Hatton Woodland. KCC is concerned that the phrase ‘where possible’ allows the developer too much leeway. There is no argument into what is possible. This is purely subjective.</p>	
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		<p>Part 4B. Indicative Masterplan KCC would be opposed to the use of H shaped buildings on the northern boundary of the development as it contravenes the principle “Buildings must be broken down where possible into smaller blocks, and gables orientated North / South.” The grouping of buildings in ‘Clusters’ in the Framework document is much more acceptable to the community than the use in the Masterplan where the use makes it impossible to soften the visual impact from the north. The density of building on the northern boundary is too high and must be reduced to provide an acceptable solution for this sensitive area. The masterplan should be changed accordingly, and should make a detailed 3D model showing</p>	<p>Part 4B. Indicative Masterplan The building layout shown in the masterplan is more detailed than the ‘Clusters’ shown in the Development Framework and shows the relationships between buildings. Detailed 3D models and landscape and visual impact assessments, including long distance views from the north will be provided as part of the Planning Application process.</p>	

Appendix 1 – Summary of Representations

		<p>the visual impact a requirement of any planning application for development in this area.</p> <p>Existing Features: KCC is concerned that despite the agreement that Phase 1 would maintain tree belts the Planning Authority allowed the mass destruction of existing tree belt along the A944 and has approved a boulevard devoid of trees. KCC is extremely concerned that the planning system can permit the wholesale destruction of 80 trees in a sensitive area when the Framework clearly opposes such an action. Trees removed by the construction process need to be replaced nearby. This also should retrospectively apply to Phase 1.</p> <p>Phasing: KCC is concerned with the proposed phasing strategy.</p>	<p>Existing Features: The formation of the vehicular access to the business park off the A944 dual carriageway resulted in the loss of a number of trees along the road frontage. The Development Framework acknowledges that some tree removal would be required to form the main access into the site. This loss of trees was clearly set out in the planning application report (111653) that was approved at Committee on 22nd March 2012. The loss of trees will be mitigated through the planting of new trees across the site.</p> <p>Phasing: Aberdeen City Council has no control over the</p>	
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		<p>“There will be no definitive sequence to these phases. Phase Three could well be developed earlier than Phase Two. Furthermore the sequence of development within Phase Two will be determined by operator demand”</p> <p>The provision of Phase Two – the hub is important as it provides facilities all Phases rely upon. Delaying the development of this would not be seen in a good light. KCC request that this section is re-worded to ensure that Phase Two is developed in conjunction with other phases. Furthermore</p> <p>“The Phasing Strategy allows for this, and does not require individual Phases to be completed prior to developing Phase. Throughout the development of the site, opportunities must be</p>	<p>market conditions within the development industry. We would expect Phase 2 to be developed alongside or before Phase 3 but we do not have powers to insist upon it. The whole site is allocated for development within the Aberdeen Local Development Plan up to the period ending 2023. The phasing shown in the masterplan is indicative, however, we have no reason at this moment in time to expect other than what is suggested.</p>	
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			<p>appropriately managed to ensure they are not prejudicial to overall growth.” KCC accept that Phase 3 can start before Phase 2 is complete, but they would be concerned if Phase 4 started before Phase 3 was substantially complete. Cherry picking prime locations should be avoided, and good design should ensure that all ‘sites’ are developed to their best advantage. Development hard up to the ancient woodland is not acceptable, and GSN buffers should be included in this sensitive area.</p>	
	<p>Detailed Masterplan: Sustainable communities rely on ensuring that there are linkages between existing settlements and new developments. We</p>	<p>Detailed Masterplan: Sustainable communities rely on ensuring that there are linkages between existing settlements and new developments. We</p>		

		<p>should not be interpreted as 'integration'. KCC agree that:</p> <p>“Instead, the proposals should offer a sustainable development solution, incorporating into the Park a number of key local facilities which are currently lacking.”</p> <p>The development must be sustainable on its own and should not rely on Kingswells to provide facilities for the development where the use would be detrimental to existing users, and provision of on-site facilities would be sustainable.</p>	<p>would expect the two to be mutually supportive.</p>	
		<p>4.B.1.3 Context: “Both Phases Two and Three, where possible, respect this principle. Within ‘the Hub’ area of Phase Two the aim should be to encourage lively spaces at pedestrian level and a mixture of activities</p>	<p>4.B.1.3 Context: We agree that the phrase ‘where possible’ should be removed from the text at Part 4.B.1.3 Context.</p>	

		<p>and uses which will encourage the Hub to be used and useable outwith the operating hours of the Business Park” The use of the phrase ‘where possible’ leaves the developer too much leeway and should be removed – this is a general comment throughout the document.</p>	<p>4.B.1.5 Spaces: The concept of grouping buildings in closely spaced clumps goes against the principle of lessening the visual impact of the buildings by building gable on North / South orientations. See also Part 4B. Indicative Masterplan.</p>	<p>4.B.1.5 Spaces: This issue has already been addressed through the response to part 4.B Indicative Masterplan comments.</p>
		<p>Green Space Network: KCC is extremely concerned about the U-turn that has taken place since they discussed the main issues with the development and agreement on</p>	<p>Green Space Network: This issue has already been addressed through the response to the Summary Document and Part</p>	

Appendix 1 – Summary of Representations

			<p>4.B Indicative Masterplan comments.</p>	
		<p>acceptable mitigating strategies was reached. The community requirement that the development should have minimal visual impact when viewed from the north has been removed from the masterplan. The rural setting will be totally lost if buildings are not screened from view. The setting of the consumption dyke will be compromised if buildings overlook the dyke – even from a distance of 120-200m.</p>	<p>Community issues need to be addressed and the masterplan needs to identify a strategy for screening buildings from view from the north.</p> <p>The Masterplan should include a requirement that a detailed 3D model showing the visual impact of development in this area is provided with planning application.</p>	

		<p>Building Heights: “Buildings within the northern extent of the Northern Zone will be restricted to two storeys, whereas the southern extent of this Zone may accommodate slightly taller buildings. With the exception of the Landmark Buildings within the Hub, buildings in the Central Zone will generally be three storey. Where topography allows, additional top floors of buildings could be treated as ‘penthouses’ which could be achieved by recessed walls and lightweight materials. As a general principle all buildings must demonstrate that they are sympathetic to setting and relate to the scale of surrounding landscape features.” KCC request that all building including the “Landmark Buildings”</p>	<p>Building Heights: The masterplan already states that as a general rule all buildings must demonstrate that they are sympathetic to setting and relate to the scale of surrounding landscape features. This is clearly stated in the ‘Building Heights’ section. KCC’s statement suggesting that the current wording of the masterplan gives the developer a “freehand” to do anything is contradictory. The very fact that the masterplan states where landmark buildings should be located provides clarity on the siting of such buildings. The request for a detailed 3D model has</p>	
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Appendix 1 – Summary of Representations

		<p>should be sympathetic to setting and relate to the scale of surrounding landscape features. “The hub has already been identified as a suitable location for ‘landmark buildings’. In this regard a zone has been identified within the Hub where buildings of some prominence could be located. These feature buildings will act as focal points for the development.” The developer has obviously some ideas about this and these have not been shared with the community. KCC object to the above wording as it gives the developer a freehand to do anything.</p> <p>The masterplan needs to offer the community some safeguards over what is developed. The Masterplan should include a requirement that a detailed 3D model showing the</p>	<p>already been addressed through the response to Part 4.B Indicative Masterplan comments.</p>		
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Appendix 1 – Summary of Representations

		<p>visual impact of development in this area is provided with planning application.</p> <p>The ‘feature zone’: KCC has concerns about the impact prominent buildings in the feature zone will have on the adjacent homes. The area is higher than the homes and distance is required to avoid imposing on the nearby homes. The buildings can be prominent from within the development, but should not be prominent on the landscape. If this is not possible then the Hub could be moved to a more suitable location within the development where more prominent buildings can be accommodated.</p> <p>The significant change between the Framework and the Masterplan is not acceptable to the community and further</p>	<p>The ‘feature zone’: We agree that buildings should be integrated within the landscape, particularly when viewed from outwith the site. We would not recommend the location of the Hub being moved as it’s current position is at the end of the boulevard entrance, this is the most logical position to provide a focal point between the business park and Kingswells. As stated in the response to the Building Heights comments, the masterplan already states that as a general rule all buildings must demonstrate that they</p>	
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		<p>consultation is required before the Masterplan can be agreed.</p> <p>Access: “It recognised that a second access into the Prime Four site will require to be considered as the development progresses. This access could be taken from the east (as indicated adjacent), or from the south or west which would be investigated via a separate masterplanning exercise for Phase Four. A Transport Assessment will be undertaken, and will be assessed by Aberdeen City Council as Roads Authority via subsequent planning applications.” KCC can see the advantage of having an access to the Hub from the Home Farm track as shown, but would have serious concerns if</p>	<p>are sympathetic to setting and relate to the scale of surrounding landscape features. The masterplan has rightfully developed and evolved as planning decisions have been made and as development has taken place. The community has been appropriately consulted at all of these stages.</p> <p>Access: As already stated in the masterplan text under the ‘Access’ section, the second access would be considered as the development progresses and would be fully investigated through a separate Masterplanning exercise and Transport Assessment.</p>		

		<p>this access could be used by the wider development. Access to Phase 3 and 4 should be from the A944 or to the west. The masterplan should identify that additional access could be provided and will be subject to a separate masterplanning exercise and Transport Assessment when necessary. The current wording suggests that the Home Farm access has been subject to a masterplan and has some level of acceptance – this has not been discussed with the community.</p> <p>4B.3.4 Public Transport Connections: The masterplan take no account of the recent changes to the Park and Ride bus service which no longer connects Kingswells to Aberdeen as the service is terminated at ARI. The need to change bus is a major disincentive to using</p>	<p>4B.3.4 Public Transport Connections: We agree that it would be appropriate to update the masterplan to reflect the recent changes in bus services which have been implemented by First Bus. We will be</p>		
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		<p>the P&R service.</p> <p>4.B.1.6 Landscape Treatments: The list of tree types should include some evergreen types. It is also noted that birch trees at Dobbies have not worked well with many leafless trees.</p> <p>Conclusion: The proposed Masterplan represents a significant deviation from the Framework document. The community has not been involved in agreeing the deviations and their views have been substantially removed from the proposed Masterplan. This is unacceptable, and the Masterplan should not be approved until it addresses community concerns expressed throughout this document whether they are highlighted or not.</p>	<p>looking for opportunities to ensure that there is a direct service to/from Kingswells Park and Ride to the City Centre. This may only be achievable once all development is in place in order to achieve critical mass.</p> <p>4.B.1.6 Landscape Treatments: The example species list contained within the masterplan makes reference to ‘Scots Pine’ which is an evergreen tree.</p> <p>Conclusion: As previously stated in the response to the Feature Zone comments, the masterplan has rightfully developed and evolved as planning decisions have been made and</p>	
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			<p>as development has taken place. The community has been appropriately consulted at all of these stages. This round of consultation which we are currently responding to is another example of consultation. The views of all respondents are taken into account, responded to and influence the final judgments that the Council must make.</p>		
<p>General Comments</p>	<p>Transport Scotland</p>	<p>Phasing should take into account delivery timescales of infrastructure including the Aberdeen Western Peripheral Route (AWPR) and other critical infrastructure such as the Third Don Crossing.</p> <p>A significant amount of development has been consented recently and a number of pre-application</p>	<p>It is agreed that phasing should take delivery timescales of infrastructure into account and this should be reflected in all documentation as it is developed or updated. The appropriate level of analysis demonstrating the likely impacts on the</p>	<p>No amendments required.</p>	<p>N/A</p>

Appendix 1 – Summary of Representations

			<p>discussions are ongoing. This development will further impact on an already significantly constrained trunk road network. This issue has been discussed at length at a number of meetings between Transport Scotland and the Council in the last few months. Going forward an analytical approach is needed to determine what level of development can be accommodated in advance of the AWPR, and other critical transport infrastructure, before the current trunk road network performance becomes unacceptable. Given the pressures are not limited to the trunk road, consideration of the performance of the local road network will also be important. The outcome of this exercise should be reflected in the planning briefs, development frameworks and</p>	<p>agreed extent of the local road which is impacted must be provided in each document, as agreed through discussion with Transport Scotland and Aberdeen City Council, with reference and commitment to the appropriate Strategic Transport Fund (STF) contribution where there is a need to mitigate cumulative impact on the regionally strategic road network.</p>	

Appendix 1 – Summary of Representations

		<p>masterplans. We would welcome an early meeting on this matter.</p> <p>The detail contained within some of the documents is out of date and some have been superseded by planning consents or subsequent transport studies. It is appreciated that these documents will become dated relatively quickly, however for sites with consents, they should be updated to reflect the transportation requirements within the conditions. For example Dyce Drive, Stoneywood, Davidson’s Mill, Murcar (part thereof) and Robert Gordon University have all been consented.</p> <p>Reference to the Structure Plan Supplementary Guidance on the Strategic Transport Fund (STF) is welcomed. However, a review should</p>	<p>The action plan should be updated regularly and where documentation is revisited, or the next level of detail is required eg via planning applications arising from Masterplans, then the most up to date position should be reflected.</p> <p>All documentation is required to reflect the STF policy. Where an exemption or reduction is requested for consideration a</p>	
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Appendix 1 – Summary of Representations

		<p>be undertaken to ensure that the STF is listed as a requirement in all Supplementary Guidance documents where the STF is to be applied. This is of particular importance given these documents are to be statutory. It is critical that the STF is applied transparently and consistently. The concern is that should the Guidance not be applied and contributions are waived on a case by case basis, this risks the emergence of a funding gap. This in turn has consequences for delivery of the identified infrastructure contained within the STF, including those relating to the trunk road. Should this situation arise Transport Scotland will need to revisit the approach to addressing the transportation impacts of development. Where full STF contributions are not</p>	<p>process is now in place, which includes Transport Scotland, to collectively agree or otherwise where exemption or reduction is appropriate. Also, the policy identifies processes for reporting development progress and the application of this policy and the outcomes.</p>		
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Appendix 1 – Summary of Representations

<p>General Comments</p>	<p>Sport Scotland</p>	<p>sought, Transport Scotland may take the view that it would be more appropriate to seek no net detriment mitigation to the trunk road for new allocations that are to be promoted in advance of the AWPR. It is likely that the transport requirements for the allocations would change and would need to be reflected in the Planning Briefs, Development Frameworks and Masterplans.</p> <p>Include the following 'catch all' comments:</p> <p>The Pitches Strategy and Leisure Asset review that is currently being undertaken should be referenced.</p> <p>The requirements of the SPP (specifically paragraph 156) in relation to the loss of outdoor sports areas should be referenced.</p>	<p>It would not be appropriate to make reference in the Masterplan documents to the Pitches Strategy and Leisure Asset review as this has not yet been completed.</p> <p>The requirements of Scottish Planning Policy (SPP) are clearly stated and it is not necessary for this National Policy to be repeated in Local</p>	<p>No amendments required.</p>	<p>N/A</p>
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Appendix 1 – Summary of Representations

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ENTERPRISE, PLANNING AND INFRASTRUCTURE COMMITTEE
31 January 2012

EXISTING MASTERPLANS AND PLANNING BRIEFS

26. The Committee heard from the Convener who drew Members' attention to a number of existing Masterplans and Planning Briefs. She explained that the existing Plans and Briefs had previously been agreed by the Council under the 2008 Aberdeen Local Plan, albeit some even pre-date that Plan. The policies in these Plans and Briefs require to be updated to reflect the latest SPP, Structure Plan and new Local Development Plan which was anticipated to be adopted on 29 February, 2012. She explained that other than the policy updates, officers did not intend on making any changes to the contents, development requirements or any planned layouts or other matters contained in the Plans and/or Briefs. However, in order to meet Scottish Government requirements officers were required to consult on the updated Plans and/or Briefs for a minimum six week period and send them to the Scottish Government before the Council could adopt them. As officers on this occasion simply intend on updating the policies rather than changing their content, now sought authority to make these changes and to go through the adoption procedures without bringing them before the Committee. In the event that issues were raised which merited further change to the content of the Briefs, these would indeed be referred back to the Committee for consideration.

Finally, the Convener detailed the Plans/Briefs affected, namely:-

- Balgownie Centre
- Bon Accord Centre
- Broadford Works
- Cattofield Reservoir
- Cornhill Hospital
- Cove Charette
- Davidsons Mill
- Denburn and Woolmanhill
- Dyce Drive
- Fire Station North Anderson Drive
- Forresterhill
- Greenferns Masterplan
- Greenferns Development Framework
- Hillhead Campus
- Kingswells Development Framework
- Murcar
- Oakbank
- Pinewood and Hazledene

- Robert Gordons University
- Urquhart Road

The Committee resolved:-

to authorise officers to update the existing Masterplans and Planning Briefs and to complete the adoption procedures without reporting to Committee for approval. However, should the content of the Plans or Briefs require an alteration to their content, these should be reported to the Committee for approval.

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Adoption of draft Supplementary Guidance to the Local Development Plan
REPORT NUMBER:	EPI/12/204

1. PURPOSE OF REPORT

1.1 The Planning etc. (Scotland) Act 2006 paragraph 22 (1) states that a planning authority may adopt and issue guidance in connection with a local development plan. During the preparation of the Aberdeen Local Development Plan (ALDP 2012) Aberdeen City Council prepared a number of draft Supplementary Guidance (SG) documents in support of the ALDP. Following examination, the Aberdeen Local Development Plan was adopted on 29 February 2012 with a number of Supplementary Guidance documents adopted in the months following. It is intended that further tranches of approved Supplementary Guidance will be forwarded to Scottish Ministers in due course for adoption, whilst other Supplementary Guidance documents will be reported to Committee as they come forward.

1.2 This committee report relates to 4 draft Supplementary Guidance documents which have recently been subject to public consultation which was agreed at the Enterprise, Planning and Infrastructure Committee on 31st May 2012 (Agenda item 23). The draft Supplementary Guidance documents were subject to a six week public consultation period which ran from 19th June to 31st July 2012. The 4 draft Supplementary Guidance documents consist of:

- Infrastructure and Developer Contributions Manual (Appendix 1),
- Bats and Development (Appendix 2),
- Children's Nurseries and Sports Facilities (Appendix 3), and
- Gypsy and Travellers Sites (Appendix 4).

All documents have been prepared in relation to the ALDP. These documents (appendices 1 to 4) are available to view on the Council's website at:

http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_committee_reports.asp

1.3 At the Enterprise, Planning and Infrastructure Committee on 31st May 2012 the representations received from the previous consultation in 2010 were noted, the officers responses to those representations were approved and permission was given for the documents to be consulted upon. The consultation period has now concluded and all representations received taken into account. The representations, officer's responses and subsequent changes made to the documents are available to see in Appendix 5. The changes made to the documents are minor and relate only to matters of clarification, formatting and to update the guidance where necessary. The alterations do not change the policy direction of the documents and the context remains the same. This report presents to Committee the representations received, officer responses and subsequent alterations. In addition to this the documents are also presented as appendices.

2. RECOMMENDATION(S)

2.1 It is recommended that the Committee:

- (a) Note the representations received on the draft Supplementary Guidance documents concerned with the consultation undertaken;
- (b) Approve officers' responses to representations received on the draft Supplementary Guidance documents;
- (c) Approve the revised draft 'Infrastructure & Developer Contributions Manual' Supplementary Guidance, 'Children's Nurseries and Sports Facilities' Supplementary Guidance, 'Gypsy and Traveller Sites' draft Supplementary Guidance and draft Supplementary Guidance document – 'Bats and Development' to allow officers to submit the documents to Scottish Ministers for ratification as Supplementary Guidance to the Aberdeen Local Development Plan.

3. FINANCIAL IMPLICATIONS

3.1 There are no direct financial implications arising from this report. Any future publication costs can be met through existing budgets.

4. OTHER IMPLICATIONS

4.1 This is the continuation of a significant piece of work that has involved and will impact upon many other council services, public bodies, the business and development industries and the citizens of Aberdeen. An Action Programme has been prepared to consider the implementation of the Aberdeen Local Development Plan and Supplementary Guidance.

- 4.2 The progression of these Supplementary Guidance documents will provide a clear framework for decision making, allowing comprehensive guidance for both applicants and officers, thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development. Detailed topic-based Supplementary Guidance also has value in reducing officer time spent on pre-application discussions.

5. BACKGROUND/MAIN ISSUES

- 5.1 The 4 documents that are proposed for ratification by Scottish Ministers as Supplementary Guidance are named and detailed below with a summary of any amendments that have been made to the document, either as a result of representations received during the public consultation period or by officers to provide greater clarity. *Please see Appendix 5 for a table outlining a summary of the representations received the responses to these and any other minor amendments officers have made.*

Infrastructure and Developer Contributions Manual

- 5.2 The aim of the 'Infrastructure and Developer Contributions Manual' Supplementary Guidance is to provide clear guidance to all stakeholders, in particular the development industry, on the infrastructure and developer contributions likely to be required to support development from an early stage in the preparation of the LDP. 10 representations were received. The comments received came from Turnberry Planning, Dundas & Wilson CS LLP, Turley Associates, Scottish Enterprise, Aberdeenshire Council, Aberdeen City Council, Scottish Water, SEPA, SportScotland and SNH. The comments ranged from wholesale disagreement with the idea of the Manual to minor amendments requested to the text. Following detailed analysis of all the responses changes have since been made to the Infrastructure and Developer Contributions Manual.

Bats and Development

- 5.3 The aim of the 'Bats and Development' Supplementary Guidance is to provide information that will be needed for developers and planning officers so that they can consider the effects of planning and development on bats which are a European Protected Species. Changes have been made to provide further clarity to all potential user groups of the guidance, whilst the policy direction has remained as before.

Children's Nurseries and Sports Facilities

- 5.4 The aim of the 'Children's Nurseries and Sports Facilities' Supplementary Guidance is to provide clear guidance for developers and planning officers so that they can ascertain the main

considerations involved in assessing the change of use of any land or building into a Children's Nursery or Sports Facility'. One representation was received in relation to this document from SportsScotland. This, the officer response, and amendment can be seen in Appendix 5.

Gypsy and Travellers Sites

5.5 The aim of the 'Gypsy and Travellers Sites Supplementary Guidance is to provide guidance on the provision of new Gypsy and Traveller sites in identified areas and how these are expected to be delivered. The Guidance also sets out the policy on how applications for private sites are to be dealt with and what facilities would be expected for permanent site and halting sites. Two responses were received from Turnberry Planning and Aberdeenshire Council. Changes made to the document have been to provide further clarity to the information currently displayed in the guidance.

6. IMPACT

6.1 The Local Development Plan and associated Supplementary Guidance contribute to the following Single Outcome Priorities:

1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2. We realise our full economic potential with more and better employment opportunities for our people; 10. We live in well designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.

6.2 The Local Development Plan and associated Supplementary Guidance supports the Council's Corporate Plan, in particular the aim of delivering high levels of design from all development, maintaining an up to date planning framework, promoting sustainable development and ensuring an appropriate open space provision.

6.3 The Local Development Plan and associated Supplementary Guidance contribute to "Aberdeen – A Smarter City" in terms of providing a clean, safe and attractive streetscape and by promoting biodiversity and nature conservation, encouraging wider access to green space in our streets, parks and countryside, improving access to affordable housing in the private sectors by working with developers to maximize the effective use of developer contributions.

6.4 The proposal is consistent with the Planning and Sustainable Development Service Plan, in particular engaging the community in the planning process, and the delivery of Masterplans/Development Frameworks in line with the Aberdeen Masterplanning Process.

6.5 Given the wide range of policy areas the Aberdeen Local Development Plan and associated Supplementary Guidance covers, an Equality and

Human Rights Impact Assessment was carried out on the Proposed Plan, it showed that there will be some positive impacts of the Plan on a range of equalities groups. The Equality and Human Rights Impact Assessment is available to view on the City Council's website at http://www.aberdeencity.gov.uk/Planning/ldp/pla_aldp_tech_appendix.a_sp or a hard copy can be obtained from the Local Development Plan Team.

7. BACKGROUND PAPERS

Appendices 1 to 4 are available on the Council's Website at:

http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_committee_reports.asp

Appendix 1 - Infrastructure and Developer Contributions Manual

Appendix 2 - Bats and Development

Appendix 3 - Children's Nurseries and Sports Facilities

Appendix 4 Gypsy and Travellers Sites

Appendix 5 - Summary of representations received, officers' responses and a list of all other minor amendments

Aberdeen Local Development Plan

<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=42278&SID=9484>

Aberdeen Local Development Plan –Action Programme

http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_action_programme.asp

Aberdeen City and Shire Strategic Development Planning Authority: Aberdeen City and Shire Structure Plan

<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=423&SID=149>

Planning etc. (Scotland) Act 2006

http://www.opsi.gov.uk/legislation/scotland/acts2006/asp_20060017_en_1

Scottish Planning Series: Planning Circular 1/2009: Development Planning

<http://www.scotland.gov.uk/Resource/Doc/261030/0077887.pdf>

The Town and Country Planning (Development Planning) (Scotland) Regulations 2008

http://www.opsi.gov.uk/legislation/scotland/ssi2008/ssi_20080426_en_1

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**APPENDIX 5: Supplementary Guidance Consultation
Respondents, Representations, Responses and Changes**

- Infrastructure & Developer Contributions Manual
- Bats & Development
- Children's Nurseries & Sports Facilities
- Gypsy and Traveller Sites

Respondent	Summary of comment	Response to comment	Changes (if any)
Infrastructure & Developer Contributions Manual			
Sport Scotland	<ol style="list-style-type: none"> 1. Welcomes the provision of the guidance 2. Respondent feels that sports facilities could be placed under "Open Space and Green Network" or "Community Facilities". They would like this to be clarified. 3. There is reference to indoor and outdoor sports facilities but no clear indication of what this means for developments. Some new development may result in new sports facilities being needed where existing ones are at capacity. 4. Respondent makes aware that Council is currently updating Pitch Strategy and looking at indoor sports facilities strategy. Completion of these studies should provide clarity to future requirements and reference to this may be useful. 	<ol style="list-style-type: none"> 1. Noted. 2, 3. Outdoor Sports Areas are covered within the Infrastructure and Developer Contributions Manual at present in the section on "Open Space and Green Space Network". Outdoor Sports Areas are defined within the Open Space Supplementary Guidance (SG) as being 'natural or artificial surfaces used for sport and recreation, for example playing fields, pitches, tennis courts, bowling greens, athletics tracks and water sports facilities.' <p>In this section the open space requirements for new residential development are set out and how the open space required will be calculated.</p> <p>Overall the character of Sports Facilities in general means that the topic is covered by both sections 'Open Space and Green Network' and 'Community Facilities'. Outdoor Sports Areas are defined within the Open Space SG and requirements for new or upgraded facilities are also described within this document. Therefore the logical place to be located was in the main 'Open Space and Green Network' section.</p> <p>Indoor Facilities are also important and it is felt that they are best covered within the Community Facilities section of the document as the general nature of indoor facilities are covered within</p>	<p>2,3</p> <p>Some additional wording has been added to the section on Open Space and Green Space Network for clarity.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>Community Facilities such as an indoor hall then it seems logical that they remain classified in that section.</p> <p>The upgrading of Indoor Facilities are also covered alongside Outdoor Facilities within the 'Open Space and Green Space Network' section. This aims to provide flexibility should the size or viability of a development mean that the open space provided would be of limited function and usefulness, and where there is already adequate provision in the surrounding area, then contributions towards the improvement of quality and accessibility of surrounding open spaces may be more appropriate instead. This may allow contributions to upgrade of differing facilities such as Indoor Facilities.</p> <p>4. It has been noted that the Pitch Strategy is currently being updated and members of the Development Plan Team have had discussions regarding this.</p>	
<p>Turnberry Planning</p>	<p>1. Welcome the production of this guidance however the respondent feels the guidance requires further development.</p> <p>The respondent states that the guidance must be assessed against the tests in Circular 1/2010.</p> <p>Scale & Time Test</p> <p>Respondent has two key concerns: 1) Believes the level of infrastructure</p>	<p>1. Note the respondents comments regarding welcoming the guidance however we do not feel that the guidance requires further development. The guidance has previously been through consultation alongside the Proposed Plan and the principles and mechanism of the guidance has been ratified by Reporters at examination. The guidance has now been through further consultation and it is our opinion that the guidance is complete.</p> <p>Notes that the respondent states that the guidance must be assessed against the tests in Circular</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>and contributions are excessive especially with existing and emerging S75 and Strategic Transport Fund and forthcoming water infrastructure fund. Respondent questions how asking for further developer contributions on top of the previous does not represent “attempts to extract excessive contributions...” as stated in Circular 1/2010. The respondent continues by writing that the requirements set out in SG appear arbitrary and conceived in isolation of the STF and water infrastructure fund. They feel that the Guidance is passing responsibility on to the developers.</p> <p>2) The proposal for the cumulative fund suggests that additional funds may be sought to upgrade the existing infrastructure which Circular 1/2010 forbids. Respondent believes that without further details of this requirement then it should be removed from the document.</p> <p>Relationship to Proposed Development</p> <p>Lack of precision around the purpose of some of the contributions and exact schemes and infrastructure that will be</p>	<p>1/2010. In identifying infrastructure requirements and formulae for developer contributions we have taken account of the tests contained in Circular 1/2010. We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>The need for new or improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional development, as well as the forecasted impact of developments based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>In addition to this, it was the opinion of the Reporter in the Reporters Examination Report that the Council’s approach to this complex issue is appropriate and conforms with guidance in Circular 4/1998, Circular 1/2009 and Circular 1/2010 (Aberdeen LDP Examination Report page 407, paragraph 26.)</p> <p>Scale & Time Test</p> <p>1 & 2)</p> <p>The identification of infrastructure requirements related to new development in the Local Development Plan (LDP) has been based on the</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>delivered leaves the SG exposed on the issue of the relationship of some of these contributions to the development. STF and Green Space Network contributions in particular are unfocussed on the above. Many of the requirements are based on the application of a formula without regard to the current usage of facilities in the area and whether there is a need for new facilities. The respondent believes that unless greater clarity is provided then the SG will contravene Circular 1/2010 which states that planning agreements should not be used to address existing deficiencies or to achieve wider planning objectives that are unrelated to the development in question. The respondent also writes that further detail is required on what will be taken into account in considering cumulative impact. Is this just within Masterplan Zones or is it wider, in which case the link to infrastructure becomes too tenuous to comply with the tests in the Circular.</p> <p>Reasonableness Test</p> <p>The SG does not provide enough clarity on the obligations of the Council and statutory authorities such as NESTRANS/Scottish Water etc. The SG shows no apparent link between allowing development to proceed and delivery of</p>	<p>following principles:</p> <ul style="list-style-type: none"> - assessing the spare capacity of existing services/facilities/infrastructure and their ability to accommodate new development; - measuring the likely impact of new development on these services in different areas of the City; and - identifying where new infrastructure, or improvements to existing infrastructure, would be required to mitigate the impact of new development. <p>This work was carried out in partnership with colleagues from across Aberdeen City Council and key agencies involved in the planning and delivery of infrastructure, through the Aberdeen Future Infrastructure Requirements for Services Group. This process led to the identification of infrastructure specified in the LDP as being required to support particular developments across the City. The information gathered was published as a list of infrastructure requirements in the Proposed and Adopted Plans, which served to share information with the development industry and other consultees as soon as possible in the plan preparation process.</p> <p>In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes. Any infrastructure or financial contributions sought will be proportionate to the impact of development and also take account of the capacity of existing services and infrastructure.</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>infrastructure. In particular it is not clear when or how these agencies would invest contributions in a timescale that satisfies the requirements of the private sector developer. The respondent's main concern is the development would proceed with no certainty or clarity as to when the necessary infrastructure facilities will be delivered and by whom. The respondent suggests that this issue would be resolved should the SG look at the role and responsibility of the Council in drafting and monitoring the agreement, and crucially reporting back to contributors. The respondent thinks that the SG should include a statement emphasising direct provision/delivery of infrastructure by the developer and that the extent of directly delivered infrastructure will be taken into account in determining contributions under the different categories.</p> <p>Transport Infrastructure</p> <p>Respondent feels that the inclusion of the STF in the SG raises a number of issues. One issue was that the original consultation undertaken by the SDPA was not responsive to many of the concerns raised, and that these are equally applicable to this incarnation.</p>	<p>Masterplans will need to demonstrate how supporting infrastructure will be delivered, along with the phases of development proposed, and provide appropriate evidence to support any request by a developer to diverge from the infrastructure identified. For example, this could be as a result of a change in the scale and/or impact of development proposed from the allocation in the LDP. Subsequent planning applications will need to reflect the agreed masterplan and mitigation measures, and developer contributions will be finalised through a Planning Agreement. This will ensure that infrastructure requirements and developer contributions are necessary and proportionate to mitigate the impact of development. It should also ensure that infrastructure provision and developer contributions do not unduly restrict the implementation of development proposals or affect the viability of development, whilst ensuring that new development will be accompanied by an appropriate level of services to support new communities.</p> <p>The principles outlined above are reflected in the formulae for developer contributions identified in the Infrastructure and Developer Contributions Manual.</p> <p>The Council will not be seeking contributions to resolve existing inadequacies.</p> <p>The Council's approach recognises that infrastructure is necessary to mitigate the impact of development and that without the necessary infrastructure development would not function</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>The SG also gives the impression that the initiatives listed as Strategic Transport Infrastructure are clearly defined. However it is the respondents understanding that these schemes have only been identified at conceptual level to date. This means that there can be no certainty about the adequacy of final proposals to deliver the benefit being identified, nor about the costs attributed to their implementation.</p> <p>There is no clear definition between 'Strategic' and 'Local' measures. This is particularly significant for the larger scale projects on which much of the LDP relies. By definition many of these projects have direct impacts on the Strategic networks. Mitigation of impacts requires modifications to these networks, in some cases involving locations identified through Cumulative Transport Appraisal (CTA) process as requiring Strategic intervention. There is currently no set procedure to identify the distinction in these circumstances.</p> <p>It appears the Council has not taken consideration of how phased development can be implemented ahead of significant infrastructure upgrades. The CTA analysis assumes that this infrastructure is in place whilst the LDP</p>	<p>effectively. It is therefore necessary to identify infrastructure requirements. By identifying these early in the plan preparation process, and applying those to specific sites and/or groups of sites, costs for infrastructure provision can be factored into land costs and shared more equitably, thereby avoiding any disproportionate cost to, or affecting the viability of, specific developments. The requirement to prepare Masterplans will provide developers with the opportunity to demonstrate how development and accompanying infrastructure will be delivered.</p> <p>The Reporters Examination Report to the Aberdeen LDP sets out that the belief is that the Council's approach has involved working with infrastructure providers, other local authorities and consultees and they found that a robust assessment of infrastructure requirements, the funding implications and the timescales involved has been undertaken.</p> <p>The Report continues that without investment in infrastructure the level of development proposed would put an unacceptable burden on existing facilities and communities and would not provide an appropriate level of services to support the new communities.</p> <p>The Reporter finds that the list of contributions relates in scale and kind to the proposed developments and do not regard them as excessive. The Reporter also notes the council's assertion that it has taken into account the capacity of existing services in its calculations and will not be seeking</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>requires delivery of development in advance of it provision. We are concerned that this will only lead to requests from Officers for further conventional network-wide assessment which defeats the objective of the STF procedure, and potentially will delay development and therefore the flow of contributions to STF.</p> <p>The respondent believes that the STF penalises mixed developments. The LDP encourages sustainable new communities yet the STF is applied to both residential and commercial development within a development. This discourages developers from providing both employment and residential uses on a site.</p> <p>Finally the respondent believes that the current draft of the SG does not provide assurances to developers that their contribution will be used directly for their site if implementation will be managed by a NESTRANS committee and that the infrastructure will be implemented within the required timescale to deliver a development proposal.</p> <p>The Respondent recommends that:</p> <ol style="list-style-type: none"> 1) The Council demonstrates that the SG conforms to circular 1/2010 and 	<p>contributions to resolve existing inadequacies and is satisfied with this.</p> <p>The Reporter concludes that he believes the process carried out by the council to identify the infrastructure required to implement the level of development proposed in the structure plan to be robust.</p> <p>The City and Aberdeenshire Council, in partnership with NESTRANS, Transport Scotland and the Strategic Development Plan Team have explored options for delivering strategic infrastructure as development comes forward for implementation across the region.</p> <p>Cumulative Transport Appraisal (CTA) demonstrates that new development across the north-east will have an impact on transport infrastructure and that movements rely on a network of road, rail and public transport with a high degree of interdependency between the two council areas. A package of defined transport projects are identified by the CTA to mitigate the impacts of new development. The purpose of the Strategic Transport Fund (STF), as set out in the Supplementary Planning Guidance document 'Delivering Identified Projects Through a Strategic Transport Fund', is to provide a mechanism for securing contributions from development to fund the delivery of the infrastructure needed to support development.</p> <p>The projects include road and public transport interventions in a variety of locations where the cumulative impact of new housing and employment uses</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>should be subjected to an assessment of its cumulative impact on development when accounting for STF and the forthcoming water infrastructure fund;</p> <ol style="list-style-type: none"> 2) The reference to the water cumulative impact model is removed given that no detail has been advanced at this stage, 3) Greater precision is provided as to how generic contributions will be applied to the direct benefit of the site, 4) Direct provision by the developer should be recognised as the preferred route rather than contributions, 5) Protocols and procedures for negotiating, monitoring and reporting S75 agreements should be included within the document; 6) Further discussions need to take place around the administration and implementation of STF. 	<p>is likely to cause increased congestion. By sharing the financial burden widely across the region, no one development will be liable for the cost of a specific strategic project or delayed by its implementation. By being upfront about the mechanism for making contributions, developers will have greater certainty over strategic transport requirements.</p> <p>All developments, regardless of Strategic Transport Fund (STF) requirements, will be assessed in terms of their impact on the local transport network and may be required to mitigate these impacts.</p> <p>Developments may be required to make an appropriate contribution towards one or several of the mitigation measures outlined in the Infrastructure and Developer Contributions Manual or any other measure that the Council deem to be appropriate.</p> <p>Overall the Council do not consider the infrastructure requirements to be excessive. The STF has been devised through transport modelling to assess the cumulative impact of development proposed and the scale of strategic transport interventions likely to be required to support this development. Further contributions as stated within the Manual will be identified through negotiation with the Council.</p> <p>Relationship to Proposed Development</p> <p>All development sites will have to provide the infrastructure and developer contributions necessary to mitigate the impact of development. The need for new or</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional development, as well as the forecasted impact of developments based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>The Reporter to the Examination Report to the Aberdeen LDP has also noted that the council's approach has involved working with infrastructure providers, other local authorities and consultees and found that a robust assessment of infrastructure requirements, the funding implications and the timescales involved was undertaken.</p> <p>Circular 1/2010 – Planning agreements, recommends that methods and exact levels of contributions should be included in supplementary guidance. The Reporter to the Examination Report is of the opinion that the council has attempted to do this. He continues that he feels that opportunities for discussion about the precise need for infrastructure and the process for delivery are also identified and notes that in certain instances further work is required to finalise the mechanisms or precise costings involved particularly with regard to the cumulative impact of proposals. Contrary to some of the arguments put forward the Reporter does not regard this as a fatal flaw in the council's approach and writes that given the early stage of development on many of the sites proposed it is inevitable that the exact level of contributions may change through time.</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>In relation to the STF, the approach taken by both Aberdeen City and Aberdeenshire Councils reflects the government guidance set out in Circular 1/2010 Planning Agreements (paragraph 18) which states that: <i>“Planning agreements must be related in scale and kind to the proposed development. In assessing any contributions planning authorities may take into account the cumulative impact of development over time. The effect of such infrastructure investment may be to confer some wider community benefit but contributions should always be proportionate to the scale of the proposed development.”</i></p> <p>In addition to this the Cumulative Transport Appraisal takes account of the capacity of the transport network to accommodate new development and provides evidence that a number of transport-related mitigation measures are required but can not be attributed to the impacts of specific developments. On this basis, the Strategic Development Planning Authority, both councils and key partners took a joint decision to find a mechanism for applying the costs of these cumulative items of infrastructure to new development allocated through the structure plan and subsequent LDPs. Fundamentally, this approach seeks to ensure that development in the future does not have a detrimental impact on the transport network.</p> <p>Where developments are being brought forward for implementation through masterplan and planning applications, the Council will discuss with developers the exact level of contribution and scale of intervention required to mitigate any impacts. Such discussion will be informed by more detailed information submitted by</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>the developer, to ensure that any contributions sought are directly related to the impact of the development.</p> <p>All development sites, whether listed or not within the Masterplan Zones table within the Manual, LDP or Action Programme, will have to provide the necessary infrastructure and developer contributions necessary to mitigate the impact of development.</p> <p>Reasonableness Test</p> <p>The need for new or improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional developments, as well as the forecasted impact of developments based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>Whilst the Council recognises there are challenges to delivering infrastructure in the current financial climate, it will be important for the Council to work closely with the development industry to find solutions to delivering development and the necessary infrastructure to mitigate any impacts. The Council can utilise mechanisms to spread costs through staged payments where appropriate, and we are exploring opportunities for up-front funding.</p> <p>The Reporter to the Examination Report to the Aberdeen LDP notes that in line with the structure plan spatial strategy the local development plan proposes</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>major development within the Aberdeen City area. This will require a network of new infrastructure such as roads, public transport and sewers together with social infrastructure such as schools, doctor's surgeries, dentists, open space and other community facilities. Without investment in this infrastructure the level of development proposed would put an unacceptable burden on existing facilities and communities and would not provide an appropriate level of services to support the new communities. At the same time a number of transport related projects to improve the regional transport network are being developed by the council in partnership with other key agencies and Aberdeenshire Council. These schemes are not expected to be subject to developer contributions but will bring significant benefits to the transport infrastructure of the entire structure plan area.</p> <p>While acknowledging the concerns about the impact on the viability of developments due to the extra expense which will be incurred, Scottish Planning Policy (SPP) clearly recognises in paragraph 16 that investment in infrastructure may be required as a consequence of existing under provision and/or planned growth. The structure plan states in paragraph 5.7 that considerable investment in existing and new infrastructure will be required in order to maintain the quality of life in communities given the level of growth which it proposes. Paragraph 5.8 confirms that developers will have to accept the need for contributions towards necessary infrastructure, services and facilities within their own site as well as extra contributions in cases where their development has wider effects.</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>The local development plan emphasises the need for developers to take into account the likely requirement for a contribution when preparing and costing proposals and early discussion with the council is encouraged. The proposed supplementary guidance states that the precise level of infrastructure provision and developer contribution required from any development will be agreed with the council in consultation with other statutory agencies. It goes on to say that the intention has been to avoid deterring development by making unreasonable demands and to seek infrastructure or developer contributions which are proportionate to the development proposed.</p> <p>The Reporter considers that the council's approach includes ample opportunities at both the masterplanning and planning application stages for negotiations to take place to ensure that the cost implications as a result of infrastructure requirements do not unnecessarily impede the effective delivery of sites.</p> <p>Transport Infrastructure</p> <p>The projects are identified at the high level, as are the estimates, which will be subject to review as per the policy and detailed assessment. This is consistent with other strategies where high level projects are identified at an early stage, and through further assessment will be refined and subject to processes such as consultation. The Cumulative Impact Assessment document gives the appropriate level of detail at this stage, and following agreement on prioritisation, will</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>then be subject to the more detailed assessment and development.</p> <p>The transport assessment process will assist in identifying the differences between the local and strategic impacts.</p> <p>The prioritisation process which is currently on-going will seek to deliver the STF in accordance with the pace of delivery of LDP development - bearing in mind both will take several years to build out. Information has therefore already been sought from the development industry to inform this process. The Transport Assessment and more detailed phasing of development via masterplanning and subsequent planning applications will help inform the level of transport infrastructure which must be in place for the development, and what will be progressed in parallel by the local authorities in terms of delivery of the STF.</p> <p>STF contributions are for the cumulative impact measures - not specifically for infrastructure proximal to their development. The policy also outlines the governance of the STF which will be transparent and reported regularly. This, in addition to the prioritisation process which is currently on-going, will all be subject to consultation and regular review after agreement.</p> <p>1) The transport cumulative impact assessment for the STF has already been undertaken. 2) Reference to a potential Cumulative Fund will remain as this is an option which is currently being investigated and may prove a useful tool in the future.</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>3) The STF looks to deal with the cumulative impact of all development.</p> <p>4) Direct provision by the developer should be recognised as the preferred route rather than contributions, the reason we are doing the STF is that this route hasn't worked particularly well in the past to deal with cumulative impact of development</p> <p>5&6) Protocols and procedures for negotiating, monitoring and reporting S75 agreements should be included within the document; STF is not a negotiation - it is about mitigating the actual impacts of development on the strategic transport network, protocols and procedures re the legal agreements are in place and are being refined as we continue, as I said before the governance and transparency of the STF will be reported regularly via NESTRANS</p>	
<p>Dundas & Wilson on behalf of ASDA Stores</p>	<ol style="list-style-type: none"> 1. Generally support the role of developer contributions and think that they are appropriate where contributions are sought to address the impacts of the proposed development. 2. In terms of the STF, the respondent would like it to clearly state that STF contributions will only be sought from newly allocated sites and windfall sites when the relevant criterion set out in the SG is satisfied. 3. The respondent would support a clearer recognition of the principle set out in Policy I1 and the tests from Circular 1/2010 in all sections. 	<ol style="list-style-type: none"> 1. Welcome the comments made supporting the role of developer contributions and the manual. 2. Process for identifying opportunity sites within the LDP that will contribute towards the STF: <ul style="list-style-type: none"> - If the application site was allocated in the 2008 Local Plan, strategic contributions would not be required, unless an alternative use or larger scale of development is proposed which generates a greater transport impact than that anticipated from the 2008 Local Plan allocation. In that case, the precise level of Strategic Transport Contributions would need to be identified in order to mitigate the wider impact of the development on the transport network. - For any new developments allocated for the first time in the Local Development Plan, these sites would make a strategic transport contribution based on the 	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>4. The SG should recognise that a need for new or improved services, facilities or infrastructure, relating directly to the proposed development, must be clearly identified before contributions are sought.</p> <p>5. Respondent thinks that bullet point two and three of the General Principles for All Development could be interpreted as suggesting that all development will automatically have an impact on existing services which will require to be mitigated by way of developer contributions. The suggestion is to include the words “where impacts requiring mitigation have been clearly identified” at the end of the second and third bullet points would give recognition to the requirement for developer contributions to be necessary to mitigate the impacts of the development to which they relate.</p> <p>6. Respondent welcomes early engagement.</p> <p>7. Respondent welcomes the increased use of draft heads of terms for Section 75 agreements prior to committee meetings to reduce the likelihood of lengthy negotiations post committee and allow the decision notice to be issued in a timely manner.</p> <p>8. Respondent notes that apart from the</p>	<p>corresponding standards for housing and employment land set out in the Supplementary Planning Guidance ‘Delivery Identified Projects Through a Strategic Transport Fund’.</p> <p>3. We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>In identifying infrastructure requirements and formulae for developer contributions we have taken account of the tests contained in Circular 1/2010.</p> <p>Circular 1/2010 – Planning agreements, recommends that methods and exact levels of contributions should be included in supplementary guidance. The Reporter to the Examination Report is of the opinion that the council has attempted to do this.</p> <p>The Reporter to the Reporters Examination Report to the Aberdeen LDP also concludes that he believes the process carried out by the council to identify the infrastructure required to implement the level of development proposed in the structure plan to be robust.</p> <p>Therefore it is believed that a clear recognition has been given to the principles set out in Policy I1 and all the tests from Circular 1/2010.</p> <p>4. The identification of infrastructure requirements</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>STF the guidance does not note a time limit for which contributions will be held. They would like an extra bullet point within the guidance stating this and would welcome a short time period of time which would be more appropriate to ensure that the developer contribution is associated clearly with the impacts of the relevant development and thus satisfying the tests of the Circular.</p> <p>9. Core Paths Network The respondent feels that it is unfair and there is no justification provided as to why food retailing is assigned the highest weighting factor for contribution towards Core Path Network. The respondent would expect to see the justification for this within the document.</p> <p>10. Development Viability Respondent welcomes the continued and improved collaboration and feel that viability to development is critical to its delivery. The respondent writes the when seeking developer contributions the Council needs to take wider pressures and viability assessments into account to ensure that Aberdeen remains a competitive location in which developers are keen to invest. The commitment in the SG</p>	<p>related to new development in the Local Development Plan (LDP) has been based on the following principles:</p> <ul style="list-style-type: none"> - assessing the spare capacity of existing services/facilities/infrastructure and their ability to accommodate new development; - measuring the likely impact of new development on these services in different areas of the City; and - identifying where new infrastructure, or improvements to existing infrastructure, would be required to mitigate the impact of new development. <p>This work was carried out in partnership with colleagues from across Aberdeen City Council and key agencies involved in the planning and delivery of infrastructure, through the Aberdeen Future Infrastructure Requirements for Services Group. This process led to the identification of infrastructure specified in the LDP as being required to support particular developments across the City. The information gathered was published as a list of infrastructure requirements in the Proposed and Adopted Plan, which served to share information with the development industry and other consultees as soon as possible in the plan preparation process.</p> <p>In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes. Any infrastructure or financial contributions sought will be proportionate to the impact of development and also take account of the</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>to treating financial viability as an essential element in the determination of planning applications is therefore welcomed. The respondent would like the SG to clearly state that the Council's review of development viability will be undertaken on a confidential basis.</p>	<p>capacity of existing services and infrastructure.</p> <p>The Reporters Examination Report to the Aberdeen LDP sets out that the belief is that the Council's approach has involved working with infrastructure providers, other local authorities and consultees and they find that a robust assessment of infrastructure requirements, the funding implications and the timescales involved has been undertaken.</p> <p>The Report continues that without investment in infrastructure the level of development proposed would put an unacceptable burden on existing facilities and communities and would not provide an appropriate level of services to support the new communities.</p> <p>The Reporter finds that the list of contributions relates in scale and kind to the proposed developments and do not regard them as excessive. The Reporter also notes the council's assertion that it has taken into account the capacity of existing services in its calculations and will not be seeking contributions to resolve existing inadequacies and is satisfied with this.</p> <p>The Reporter concludes that he believes the process carried out by the council to identify the infrastructure required to implement the level of development proposed in the structure plan to be robust.</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>5. Point 5 has been noted and the section on General Principles for All Development rearranged for clarity.</p> <p>6. Similarly to the respondent the whole ethos of the LDP and associated Supplementary Guidance has been for early engagement and this is something which we will look to continue with.</p> <p>7. Noted</p> <p>8. Within the current version of the Infrastructure and Developer Contributions Manual there is no time limit given to how long the Council will hold contributions towards mitigation measures. Certain mitigation measures, such as the development of Schools, may require an unknown time frame for collection of developer contributions as items such as this tend to be developed over a long period of time. Any holding of developer contributions will be within the terms of Circular 1/2010 – necessary, serving a purpose, related to the development, in scale and kind and reasonable.</p> <p>9. Food retailing has the highest weighting factor when calculating Core Path Network contributions. The reasoning behind this is that the methodology takes account of use factors, where at one end of the scale there is a warehouse for example with a weighting factor of 0.05. This weighting reflects a warehouse operation where there is likely to be a small number of staff using the facility. At the other end of the scale is a supermarket which has a weighting factor of 1. This is because a supermarket employs a considerable amount of staff and also attracts an even greater amount of customers which is reflected in the weighting factor. The reasoning behind this will be added to the document to provide greater</p>	<p>5. The section on General Principles for All Development has been amended for clarity. The bullet points have been removed and some words added for further clarity.</p> <p>9. The reasoning behind the Core Path Network contribution calculations will be added to the document to provide greater clarity.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
		<p>clarity.</p> <p>10. We welcome the positive response regarding the inclusion of the section on Development Viability.</p> <p>All parties should be aware of the provisions of the Freedom of Information Act and Environmental Information Regulations and also mindful of any conflicts of interest that could taint their advice. Therefore the Council can confirm that the review of development viability will be undertaken on a confidential basis should the following text be included within each development viability submission.</p> <p>“This viability report is provided on a confidential basis to the Council. We therefore request that the report should not be disclosed to any third parties (other than consultants instructed by the Council to review this report) under the Freedom of Information Act 2000 (sections 41 and 43(2)) or under the Environmental Information Regulations.”</p>	<p>10. Include text to set out that the review of Development Viability will be undertaken on a confidential basis should the correct statement be included. Statement to include has been added.</p>
<p>Turley Associates on behalf of Sainsbury's Supermarkets</p>	<p>1. General Principles The respondent would like the General Principles of the manual to refer specifically to the five policy tests set out in the Circular 1/2010.</p> <p>2. Development Viability The respondent supports the approach taken to the consideration of development viability when determining</p>	<p>1. Notes that the respondent would like the General Principles of the manual to specifically refer to the five tests set out in Circular 1/2010. We consider that the proposed approach to planning and delivering infrastructure is appropriate and complies with Scottish Planning Policy and the corresponding five tests contained within Circular 1/2010 Planning Agreements.</p> <p>In identifying infrastructure requirements and</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>developer contributions.</p> <p>3. Core Paths Network Respondent questions whether a contribution towards the core path network is appropriate in all circumstances and in particular where the development is not enhancing or linking in to it.</p> <p>Request that the second criterion for determining if a contribution is required is amended to the following: “Where there is no provision of core paths or links to the Core Path Network, developers will may (depending on the particular circumstances of the case) be required to make a financial contribution at a rate of £371 per dwelling or per house equivalent for non-residential developments.”</p> <p>The respondent seeks clarity on the Council’s justification for applying weightings in the calculation of core path contributions. How have the individual site usage weighting factors been calculated? It is unclear as to why food retail has the highest weighting factor. In calculating the core path contribution, the manual states that an individual site usage weighting factor (WF) is then applied to reflect the probable occupation</p>	<p>formulae for developer contributions we have taken account of the tests contained in Circular 1/2010.</p> <p>The need for new or improved infrastructure has been identified following detailed analysis of the capacity of existing services to cope with additional development, as well as the forecasted impact of developments based on the information available at that time. As such, these requirements are necessary in planning terms to make development acceptable, and to avoid any detrimental impact on services and infrastructure.</p> <p>Circular 1/2010 – Planning agreements, recommends that methods and exact levels of contributions should be included in supplementary guidance. The Reporter to the Examination Report is of the opinion that the council has attempted to do this.</p> <p>The Reporter to the Reporters Examination Report to the Aberdeen LDP also concludes that he believes the process carried out by the council to identify the infrastructure required to implement the level of development proposed in the structure plan to be robust.</p> <p>Therefore it is believed that a clear recognition has been given to the principles set out in Policy I1 and all the tests from Circular 1/2010.</p> <p>2. We welcome the comments made regarding the Development Viability section of the Manual.</p>	

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	<p>level of the site. The Council should define what probable occupation means as this is not clear in the manual.</p> <p>The respondent would like further information as to how the Council have calculated the £371 per dwelling equivalent.</p>	<p>3. Enhancement of, or links into the Core Path Network are appropriate and in particular where a development is not currently linking in to, or enhancing the core path network this is even more crucial. Contributions will be sought to enable off site improvements. Policy NE9 Access and Informal Recreation sets out that development's should include new or improved provision for public access, permeability and/or links to green space for recreation and active travel. Further guidance within the Transport and Accessibility and Open Space SG add weight to this. This is conveyed by the ability to access, move around and through the built and natural environment by walking and cycling is a major contributor to quality of life and, in particular, an individual's ability to freely access the services and facilities they need without using a vehicle. New development will be required to protect and enhance existing access rights including core paths, rights of way and paths within the wider network.</p> <p>Guidance continues that new development must be permeable to walkers and cyclists and should ensure that new routes are planned in accordance with the 5 C's – connected, convenient, comfortable, convivial and conspicuous – as referred to in the Aberdeen Local Transport Strategy 2008 - 2012.</p> <p>Connecting our urban and rural green spaces to each other and to the communities around them, offers a wide range of social, health, economic and environmental benefits. Green networks can provide an enhanced setting for development and other land uses and opportunities for outdoor recreation, nature</p>	

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		<p>conservation, landscape enhancement and providing a sense of place and local distinctiveness. Therefore it is considered that it would not be appropriate to make the suggested change.</p> <p>Food retailing has the highest weighting factor when calculating Core Path Network contributions. The reasoning behind this is that the methodology takes account of use factors, where at one end of the scale there is a warehouse for example with a weighting factor of 0.05. This weighting reflects a warehouse operation where there is likely to be a small number of staff using the facility. At the other end of the scale is a supermarket which has a weighting factor of 1. This is because a supermarket employs a considerable amount of staff and also attracts an even greater amount of customers which is reflected in the weighting factor. The reasoning behind this will be added to the document to provide greater clarity.</p> <p>The £371 per dwelling equivalent is calculated through the cost of a range of past schemes and the average path lengths.</p>	
<p>Scottish Enterprise</p>	<p>1. Respondent broadly welcomes the guidance and welcomes the openness as good planning practice, provided a flexible approached is adopted and planning applications are not delayed.</p> <p>2. Flexible Approach Concerned that if document becomes too prescriptive in its application then</p>	<p>1. Response is welcomed from the respondent. The Council will endeavour to continue to progress planning applications within the set timescales. The Manual should help to hasten the process where the purpose is to provide clear, open and transparent upfront information that will allow developers and stakeholders to see what requirements should be taken into account from an early stage when preparing and costing proposals.</p>	

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	<p>developers and inward investors could be discouraged from pursuing planning applications, particularly on sites which are constrained by inadequate infrastructure. Respondent requests a flexible approach when applying the policy in practice.</p> <p>3. Development Viability Respondent notes and welcomes the process outlined for assessing development viability. Respondent would like the applicant and advisors to remain involved in discussions to ensure that there is no mis-interpretation of information or assumptions.</p> <p>Welcome the fact that income (including void periods etc) will be based on comparable evidence. Respondent request that realistic adjustments are made to reflect current economic conditions in particular localised market and physical factors which may need to be taken account of against market rates.</p> <p>Respondent requests that a flexible approach is adopted for reassessing finance costs on a regular basis. In the current climate borrowing costs and lending criteria are subject to abrupt change and can have a big impact on viability.</p>	<p>2. In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes.</p> <p>The masterplanning and planning application processes, and particularly technical assessments presented to the Council during these stages, will provide an opportunity for verification of impacts of development and mitigation measures proposed and will inform the negotiation of Planning Agreements. This approach will ensure that any infrastructure requirements and Planning Agreements are in scale and kind with the proposed development.</p> <p>The Reporters Report to the Examination of the Aberdeen LDP notes that without investment in infrastructure the level of development proposed would put an unacceptable burden on existing facilities and communities and would not provide an appropriate level of services to support the new communities.</p> <p>The Reporter finds that the list of contributions relates in scale and kind to the proposed developments and does not regard them as excessive. The Reporter also notes the council's assertion that it has taken into account the capacity of existing services in its calculations and will not be seeking contributions to resolve existing inadequacies and is satisfied with this.</p>	

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	<p>4. Planning Timescales Respondent requests that this process does not hinder or delay the planning process, and we trust this documents will assist and speed up applications and the assessment of developer's contributions in particular.</p> <p>Respondent requests that the 3rd party dispute resolution process step is identified as a measure of last resort at the end of the flowchart. Respondent feels that it is a useful diagram, however there is a risk it sends out the wrong message that the dispute resolution route will be take too early or automatically and the overall process is at risk of being interpreted as unduly costly and time consuming.</p> <p>5. Additional Comments Request that developer contributions are held and used within a shorter more reasonable period up to a potential maximum of 10 years. This will encourage the Council to spend the contribution within a timescale where the development and the community can meaningfully benefit from the infrastructure coming forward.</p> <p>To assist and make reading of the</p>	<p>The Reporter concludes that he believes the process carried out by the council to identify the infrastructure required to implement the level of development proposed in the structure plan to be robust.</p> <p>3. We welcome the positive response to the Development Viability section of the SG. Viability appraisals, if required, will be led by a qualified independent practitioner and suitably qualified surveyor. Professionals conducting such work will have the knowledge and reasoning to assess the viability of a site reflecting the current economic conditions locally and nationally.</p> <p>3. In relation to the request from the respondent for a flexible approach to be adopted for reassessing finance costs on a regular basis the Manual does states that the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes. In relation to Section 75 agreements, there is scope for variation of these agreements under the Planning etc. (Scotland) Act 2006.</p> <p>4. The Council will endeavour to continue to progress planning applications within the set timescales. The Councils approach to the planning and delivery of infrastructure has been open and transparent. The aim is to provide clear guidance to all stakeholders, in particular the development industry, on the infrastructure required to support development. It is</p>	

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	<p>document easier there should be a reference in the back of the Manual which provides links to all the relevant related documents where available.</p> <p>Respondent welcomes the collaborative approach taken to the preparation of this document.</p>	<p>hoped that by providing this information early and up front will help to streamline the process.</p> <p>Third party dispute resolution is one of the final steps within the flowchart relating to Development Viability. It is our opinion that there are several steps and opportunities for discussion with development management and planning gain officers to take place before a third party is brought in to try and resolve a dispute. The dispute resolution route will not be taken automatically and every opportunity will be given to discuss any issues arising. Viability Assessments are thought of as good practice and have been adopted as such by professional bodies RTPI and RICS.</p> <p>5. We welcome the suggestion that a reference section should be included at the back of the document. This will be added to the document before publication.</p> <p>Within the current version of the Infrastructure and Developer Contributions Manual there is no time limit given to how long the Council will hold contributions towards mitigation measures. Certain mitigation measures, such as the development of Schools, may require an unknown time frame for collection of developer contributions as items such as this tend to be developed over a long period of time. Any holding of developer contributions will be within the terms of Circular 1/2010 – necessary, serving a purpose, related to the development, in scale and kind and reasonable.</p>	<p>5. Addition of a reference section to the rear of the document.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
Aberdeenshire Council	<ol style="list-style-type: none"> 1. Respondent recognises the importance of, and supports the provision of, clear guidance to stakeholders on the methodology used to identify infrastructure requirements. 2. Respondent feels that by identifying specific figures within the document that they will become out of date quickly. This could be remedied by stating that up-to-date figures are available via contact with Planning Gain. 3. Respondent believes that the format of the document makes it difficult to identify the topic areas and that there seemed to be a considerable amount of contextual information dating back a number of years. 4. Respondent believes that the SG contains a duplication of information to that within the LDP which could be removed and replaced with cross-referencing to reduce the size of the document. 	<ol style="list-style-type: none"> 1. We welcome the comments made supporting the provision of clear guidance to stakeholders to identify infrastructure requirements. 2. Within the section 'Methodology for Infrastructure Requirements and Developer Contributions' it states that "Any costs for infrastructure will be reviewed annually and adjusted according to an agreed indexation." Identifying figures early allows stakeholders and the development industry to get an idea of likely costs and plan accordingly. <p>Paragraphs 23 and 29 of Circular 1/2010 Planning Agreements state that infrastructure requirements should be set out within the LDP and associated Action Programme. Policy I1 also states that "the level of provision will be commensurate to the scale and impact of development". Policy I1 includes a cross-reference to the Infrastructure and Developer Contributions Manual, which is published as Supplementary Guidance to the LDP. In compliance with Circular 1/2010 the LDP (at Appendix 4) and Action Programme list items for which contributions are likely to be sought and, alongside, the Supplementary Guidance is intended to provide as much information as possible on the contributions likely to be sought through conditions and/or Planning Agreements. In all cases, the precise level of infrastructure requirements and developer contributions will need to be agreed with the Council and other statutory agencies through the masterplanning and planning application processes.</p>	

Respondent	Summary of comment	Response to comment	Changes (if any)
<p>Aberdeen City Council</p>	<p>1. Page 9 and bus passes section - the text names of the bus operators should be changed. Could the wording in both sections recognise that these are the City's two main operators but there are smaller operators out there too – smaller bus operators may not necessarily offer annual bus passes at the moment but they may offer similar duration passes and may go on to offer annual in the future and it would be inappropriate to exclude these possibilities.</p>	<p>3. We do not believe that the format of the document makes it difficult to identify the topic areas. Each area is headed clearly for example “Schools” and “Community Facilities”. Pages and paragraph numbers have now been added to the document as this was an oversight from the consultation document. The contextual information is important as it sets where the SG document sits in relation to the LDP and planning system.</p> <p>4. We recognise that there are short paragraphs of information that are similar to information given in the LDP, however it is felt that they are important in setting out what the document is trying to achieve and will therefore be retained. It is not thought beneficial to remove this information and its removal would not shorten the document dramatically.</p>	
	<p>1. Page 9 and bus passes section - the text names of the bus operators should be changed. Could the wording in both sections recognise that these are the City's two main operators but there are smaller operators out there too – smaller bus operators may not necessarily offer annual bus passes at the moment but they may offer similar duration passes and may go on to offer annual in the future and it would be inappropriate to exclude these possibilities.</p>	<p>1. Comment has been taken onboard and the name and wording around the bus operators changed to reflect that First Bus and Stagecoach are the two main operators but there are smaller operators present too.</p>	<p>1. Sentence has been changed to recognise that First Bus and Stagecoach are the main operators, however also note that there are other smaller operators present within the City.</p>

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Scottish Water	<p>The respondent clarifies several points within the water section of the SG.</p> <ol style="list-style-type: none"> At present water and drainage infrastructure is not funded via Developer Contributions and this point should be made clearer. <p>Strategic assets are termed as Part 4 Assets. Scottish Water is funded to upgrade Part 4 Assets when there is growth requirement, initiated by a development meeting our 5 Growth Criteria.</p> <ol style="list-style-type: none"> The local bulk infrastructure, such as trunk mains and trunk sewers, water service reservoirs and some SUDS are Part 3 Assets. Should these Assets require to be upgraded as the result of new development then the responsibility will be with the Developer, however they will be entitled to a Reasonable Cost Contribution from Scottish Water. The respondent provides a slight change to the water section text within the SG. 	<ol style="list-style-type: none"> 2 & 3. Scottish Water have provided a reworded Water section which clarifies all the points which they have highlighted. Water section has been rewritten to clarify the role of Scottish Water and what will be expected in terms of Infrastructure and Developer Contributions. 	<p>Water and Drainage section has been amended to reflect the comments from Scottish Water. This context and direction of the section has not changed and the amendments provide further clarity.</p>
Scottish Natural Heritage	<ol style="list-style-type: none"> Generally support the production of this guidance. The respondent welcomes the clear statement that greenspace is a 	<ol style="list-style-type: none"> Welcome the support for the document from the respondent. The suggested rewording of the last sentence of the first paragraph relating to Green Space Network on 	<ol style="list-style-type: none"> The final sentence will be reworded to read:

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>fundamental part of the City's infrastructure from the 1st paragraph of page 19. However the respondent feels that there is also a function of greenspace missing from the sentence. The phrase should be re-written to read the following: "...providing an enhanced setting for development, opportunities for adapting to climate change, a facility for the people living or working in the developments and to help the Council meet its biodiversity duty."</p> <p>It is thought that the suggested addition emphasises the importance of the connectedness of the habitats that comes from the network, rather than just the habitats on their own.</p> <p>3. Respondent recommends that the final sentence of the second paragraph is amended to read "Such mitigation and enhancement measures..."</p> <p>4. It is suggested that the fourth bullet point is amended to read "strategic habitat networks, wildlife corridors and habitat development project."</p> <p>5. Finally the respondent asks if the Council foresees any situations where</p>	<p>page 20 is acceptable and the manual will be altered to reflect.</p> <p>3. The suggested amendment has been considered and adapted to fit the needs of the document. The respondent sought the final sentence of the second paragraph to read "Such mitigation and enhancement measures..." However it is thought more appropriate that the sentence reads as "Such mitigation and/or enhancement measures..."</p> <p>4. The suggested amendment to the fourth bullet point on page 20 to read "strategic habitat networks, wildlife corridors and habitat development project" is accepted and will be changed within the document.</p> <p>5. There are potentially situations where strategic landscaping would be on such a scale where it would require developer contributions. The strategic landscaping would have to be related to the development and setting of the site in context to the surrounding area. In this situation it would be a part of the Green Space Network and would not require its own separate category in the manual.</p>	<p>"providing an enhanced setting for development, opportunities for adapting to climate change, a facility for the people living or working in the developments and to help the Council meet its biodiversity duty"</p> <p>Additional text is in italics.</p> <p>3. Addition of "...and/or enhancement measures..."</p> <p>4. Amend fourth bullet point on page 20 to read "strategic habitat networks, wildlife corridors and habitat development project"</p>

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	<p>strategic landscaping would be on such a scale that it would require developer contributions. If so, would that be part of the green space network or require a separate category in the manual?</p>		
SEPA	<p>1. We have no additional comments to make to those previously made in relation to the Infrastructure and Developer Contributions Manual. We support the recognition in the LDP and supporting documents of the need to consider infrastructure delivery at an early stage.</p>	<p>1. Comments welcomed and noted.</p>	
Bats & Development Supplementary Guidance			
SNH	<p><u>Permitted development</u> The recent expansion of householder permitted development rights means that a lot of development in houses that could affect bats will now be permitted development, so there is no direct role for the planning authority. We recommend that a sub-heading could be added about 'Permitted Development Works' and then the point made that even if proposed works (e.g. alteration or extension of a dwelling house) are permitted development, the same principles of this supplementary guidance should be followed, and that if it is suspected that</p>	<p>Agreed.</p>	<p>Change made.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
SNH	works could not proceed without an offence being committed, SNH should be contacted prior to commencement.		
SNH	<u>Bats and Legislation (section 3)</u> It is also an offence to deliberately or recklessly disturb a bat while migrating or hibernating.	Agreed.	Change made.
SNH	<u>Bats and Licensing (section 4)</u> SNH is now the main licensing authority for bats for all purposes. We recommend that the text is amended to reflect this, for instance, by removing the second sentence.	These changes were already made.	No changes made.
SNH	We recommend that the text on the first licensing test is amended so that the second sentence reads 'For example, a licence may be granted'. This is because there are other purposes for which licences for bats can be granted.	Agreed.	Change made.
SNH	<u>Bats and Land Use Planning (section 5)</u> In table 1 it would be useful to clarify that 'priority' species refers to the UKBAP, as this means something very different under the Habitats Directive.	Agreed.	Change made.
SNH	<u>Establishing if a survey will be required (section 7)</u> The guidance on when a survey may be required is not precise and so could be open to interpretation. It is not clear how the councils GIS would be used to help decide if a survey is required, nor what other factors would be taken into account. It is also unclear what is meant by 'close'	Agreed. Most changes were made prior to the consultation period. Leaving out the reference to 200 metres as there is a separate flowchart which refers to GIS layers indicating freshwater and other relevant habitats.	Changes made.

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	<p>to freshwater and other habitats. Bats will forage at least 200m from their roost and so roosts could be this distance, or more, from these habitats. In Aberdeen, the corridors of the rivers Dee and Don are some of the key areas where bat roosts are more frequently found.</p> <p>The guidance in its current form could lead to it being applied inconsistently and cause confusion amongst developers and planners. This might result in situations where it is decided at a relatively late stage in the planning process that a bat survey is required, resulting in a delay to the planning decision. We recommend making it clearer when a survey will, may or will not be required. We have already provided a suggested approach to your environmental planner in an email on 6 June 2012 (copy attached) and understand that this section of the guidance is currently being amended.</p> <p>Please note that we are aware of a recent case where a bat roost was found in a castle on a sea cliff and so while bats are not generally found by the sea, this is not always the case.</p> <p>Similarly bat roosts are now sometimes found in trees of less than 1m girth.</p>		
SNH	<p><u>Survey standards (section 8)</u> For paragraph 9, beginning 'If winter surveys...' we recommend it is amended to read 'further survey' rather than 'a</p>	Agreed.	Changes made.

Respondent	Summary of comment	Response to comment	Changes (if any)
SNH	<p>further survey' as sometimes more than one additional survey is required. It may be helpful to provide links to the guidance referred to in this section: Bat Mitigation Guidelines: http://publications.naturalengland.org.uk/publication/69046 Bat Surveys Good Practice Guidelines: http://www.bats.org.uk/pages/batsurveyguide.html</p> <p><u>Once a survey is complete (section 10)</u> The second bullet point might be worded more clearly as 'if a survey has been conducted during the winter (outwith mid-May to mid-August) and has reported suitable habitat for bats but no evidence of bats has been found...' The fourth bullet point says that where a direct impact on bats cannot be avoided, the developer will be advised to apply for a licence from SNH. It may be helpful to set out more accurately the role of the council. We therefore recommend amending this bullet point to read: where a direct impact on bats cannot be avoided, the council will only grant planning permission if it is satisfied that all three tests for a EPS licence can be met, and it would then remind the applicant of the need to apply for a licence from SNH prior to any commencement of works.</p>	Agreed.	See next section for changes made.

Respondent	Summary of comment	Response to comment	Changes (if any)
SNH	<p><u>SNH suggested approach for when bat surveys are required</u> Built Structures Survey will usually be required for demolition/alteration of:</p> <ul style="list-style-type: none"> underground and other structures such as tunnels, kilns, cellars, ice houses and fortifications which can provide hibernations sites with stable temperatures; and/or buildings or other structures with known bat roosts or where bats are known to be present; and/or buildings or other structures within the area shown on the Council's GIS where bats have been noted in the city; and/or buildings or other structures within 200m of freshwater and wetland habitats such as rivers, burns, ponds or wet grassland or from woodland, hedgerows and/or lines of trees. <p>The buildings that may support bat roosts include traditional stone and slate buildings with an intact or almost intact roof structure (eg churches, steadings, mill buildings) and also any constantly heated buildings such as houses, nursing homes, schools and swimming pools. Other structures such as bridges can also contain bat roosts. Surveys may be required for: Buildings and other structures more</p>	<p>Have taken on some of the suggestions but not all of them. It is felt that some of the suggestions are still vague and could lead to misinterpretation.</p>	<p>Some changes made.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>than 200m from suitable habitats where there is evidence to suggest they may be present. Surveys will not be required for: buildings in a very exposed location by the sea with no suitable foraging habitat within 1km</p> <p>Trees Surveys may be required for developments proposing tree work (felling or lopping) of: old and veteran trees older than 100years; and/or trees with obvious hole, cracks or cavities; and/or trees with a girth greater than 1m at chest height.</p> <p>Note that trees other than these can sometimes contain bat roosts. Young plantations of conifers are not likely to have bat roosts. Trees are more likely to be used by bats if they are linked by other trees or hedgerows to woodland or other habitat suitable for bats.</p>	<p>Some suggested changes here have already been carried out, while others have now been made.</p> <p>Changes relating to trees already made.</p>	<p>Some new changes made.</p>
SEPA	<p>We have no additional comments to make to those previously made in relation to the Infrastructure and Developer Contributions Manual (SEPA Ref: PCS/110085). We support the recognition in the LDP and supporting documents of</p>	<p>Comments noted.</p>	<p>No changes required.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
Historic Scotland	the need to consider infrastructure delivery at an early stage.		
Historic Scotland	While welcoming the preparation of this guidance I can confirm that we have no detailed comments to offer on this occasion.	Comments noted.	No changes required.
Bat Conservation Trust (BCT)	<p>Like the use of photos but have a minor suggestion. For improvement Use 4 photos to illustrate;</p> <ol style="list-style-type: none"> 1. Bat Habitat 2. Roosts – traditional buildings 3. Roosts – modern buildings 4. Bats <p>EG</p> <ol style="list-style-type: none"> 1. Good bat habitat (water and woodland/ park/ gardens/ riparian woodland) in aberdeen 2. A traditional/ posh property that is a roost 3. A modern council estate type environment (great for pipistrelles) 4 Bats in roost (use photo on RHS) 	Do not have access to such photos, plus, it is felt that these photos would not add any further value to the guidance. Adding this number of photos would also make the guidance quite a large document.	No changes required.
Bat Conservation Trust (BCT)	Suggest items 6 and 7 should be “swapped” so Establishing if a survey is required comes as item 6 and identifying roosts is 7.	Section 8 ‘survey standards’ and section 9 ‘minimum survey details’ etc, naturally follows section 7 ‘establishing if a survey is required. Therefore, it is felt that there is no value in swapping section 6 and 7 around.	No changes required.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	Suggest including a sentence to say who the guidance is aimed at.	Agreed.	Sentence now added to introduction.
Bat Conservation Trust (BCT)	Lisa Kelly this is fine for activity surveys but I'm not sure what to say about identifying hibernacula. My problem is that a winter survey may NOT show bats using a structure but a week later the bats may be there. Is it better to identify potential winter sites in summer and assume bats might be there in winter, rather than ask developers to wait for a winter survey which does not find bats using the site at the time ???	The guidance does not suggest that developers wait for winter surveys but only suggests them if summer surveys cannot be carried out and that there is access to all parts of the building. If a winter survey has to be conducted, then it is stated that further summer surveys may be required if the evidence is inconclusive or there is potential for bats. That said, a new sentence has been added stating that if a summer survey suggest possible evidence of a hibernation roost, then further winter surveys will be required. Changes made.	Changes made.
Bat Conservation Trust (BCT)	Include Natterer's bat in section 2.	Agreed.	Change made.
Bat Conservation Trust (BCT)	Section 3 – Do we need the word “wild “in?	The inclusion of the word ‘wild’ helps to distinguish between wild and captive bats.	No change made.
Bat Conservation Trust (BCT)	Section 3 - Suggest adding – Bats will roost in a variety of structures including; trees, caves, rock faces, bridges and buildings both old and modern. Perhaps put this text in a box to make it stand out.	This is covered in section 6 ‘Identifying Bat Roosts’.	No changes required.
Bat Conservation Trust (BCT)	Section 3 – Perhaps illustrate with pictures of suitable sites; tree, bridge, modern building Eg	Do not feel that this would add value to this section which covers ‘Bats and Legislation’.	No changes required.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	<p>school and an older traditional property.</p> <p>Section 5 – Check with Lisa and Kelly – Does it have to be a roost? I suspect so but want to check – would the use of the area as a foraging site of commuting route count a material consideration (albeit with less protection)?</p>	<p>The use of an area as a foraging site or commuting route is not a material consideration. Section 3 clearly states what is included in the law.</p>	No changes required.
Bat Conservation Trust (BCT)	<p>Section 5 - Suggest using the Planning and development for bat surveys from the BCT Bat surveys – Good practise Guidelines as a tool for identifying when a bat survey should be carried out.</p>	<p>This is covered in section 7 ‘Establishing if a Survey is Required’ which tailors various guidance into concise guidance for an urban environment. Reference to this guidance has now been made in section 8 ‘Survey Standards’.</p>	No changes required.
Bat Conservation Trust (BCT)	<p>Section 5 – And must show how FCS will be safeguarded.</p>	<p>Local authorities are not the regulators for felling. FCS regulates their own work.</p>	No changes required.
Bat Conservation Trust (BCT)	<p>All resident bat species (in the Aberdeen area) are identified as “species of principal importance “under the Nature Conservation Act (Scotland) 2004. Also under this act local and regional authorities have a duty to further the conservation of biodiversity so far as is consistent with the proper exercise of those functions</p>	<p>This is already covered under the Natural Heritage guidance and the Council’s Nature Conservation Strategy 2010-2015. This information would not add any further value to this guidance.</p>	No changes required.
Bat Conservation Trust (BCT)	<p>Add Natterer’s Bat to Table 1.</p>	<p>Agreed.</p>	Change made.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	Add to Table 1 - SPI: species of principal Importance (on Scottish biodiversity list.	Agreed.	Change made.
Bat Conservation Trust (BCT)	'The Local Planning Authority should request a bat survey to establish the impacts on bats before the planning application is determined.' Excellent that this is made so clear.	Comment noted.	No changes required.
Bat Conservation Trust (BCT)	Section 6 – Stress that bats use a variety of roosts, different roosts in winter and summer. In addition they will use both modern and old/ traditional buildings or structures.	This is covered in section 6 and also in sections 2 under 'General Information about Bats' and 7 'Establishing if a Survey is Required'.	No changes required.
Bat Conservation Trust (BCT)	Section 6 – Caution – droppings may NOT be visible even with large number of bats. Pipistrelle bats in particular may use tiny spaces eg between slates/ tiles and sarking. Their droppings may not be visible inside a roof space and thinly scattered outside a structure or too high up on a wall to be noticed.	This is covered further on this section.	No changes required.
Bat Conservation Trust (BCT)	Section 6 - Delete "where light enters" I think this is a typo. BLE bats will roost in spaces that are quite light but this is not a prerequisite.	Agreed.	Change made.
Bat Conservation Trust (BCT)	Section 6 last paragraph - Suggest putting the text in bold also in a box – to make it stand out.	This section is already in bold.	No changes required.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	<p>Section 6 - Suggest adding a diagram to show the actual size / dimensions of a gap that a Pipistrelle bat could use as an entry/exit point into a roost. Draw torpedo shape 2cm wide by 1cm high.</p>	Trying to keep this guidance as brief and concise as possible. Adding further information would make the guidance too long, plus don't think that this would add value to the guidance.	No changes required.
Bat Conservation Trust (BCT)	Section 7 – See BCT trigger list for further info.	Links to the BCT guidance is already on section 8, plus this section has a more concise trigger list developed for the urban Aberdeen City.	No changes required.
Bat Conservation Trust (BCT)	<p>Section 7 – <i>Buildings which are in a very exposed location by the sea with no suitable foraging habitat within 1km are unlikely to be used by bats and will not require a survey.</i></p> <p>Check – not sure about this at all. I really don't know if this is true or not. Would bats use an ice house / fishing station as a hibernation site if it lacked good feeding habitat but had suitable temp and humidity for hibernation? Bats will feed over seaweed.</p>	Agreed. SNH have recently reported that a bat roost was found in a castle by the sea.	This sentence has now been removed.
Bat Conservation Trust (BCT)	Section 7 - Remember, bats can be found in any structure and/or building both old and new if it is in the correct environment! YES – suggest putting this in a box and in even bigger type.	This section is already in bold. Do not think putting in a box in bigger font size would add any value to the guidance.	No changes required.
Bat Conservation Trust (BCT)	Section 8 - b. Bat Surveys Good Practice Guidelines – Bat Conservation Trust.	Point noted.	No change required.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	<p>(note now on edition 2 , July 2012)</p> <p>Section 8 - For householder applications and other small scale developments such as steading conversions, winter surveys may be acceptable to rule out the presence of bats but these will only be accepted where all relevant parts of the building can be adequately inspected.</p> <p>NO!! Note a Winter survey MAY confirm the presence of bats / use of the building as a bat roost but it cannot be relied on to show bats will not be present in summer .</p> <p>Section 8, Diagram 1 – Add to text, for active bats . Hibernation surveys – from October to March.</p>	<p>The Council is satisfied that the guidance stipulates that it requires confirmation that there is access to all parts of the buildings before agreeing to a winter survey. The guidance goes on to say that if findings are inconclusive or there is potential for bats, the Council will request further surveys in the summer months. This is also repeated in Section 10.</p>	No changes required.
Bat Conservation Trust (BCT)	<p>Section 9 - Minimum Survey Details Note – see Template p28, BCT Bat survey = good practice guidelines.</p> <p>Section 9 – Minimum Survey Requirements – item 2. Add 's' to the word 'survey'.</p> <p>Section 9 – Minimum Survey Requirements – item 4. Add to sentence – '(Including timings /</p>	<p>This guidance focuses on the best time to survey for bat activity which is during the summer months. However, reference to the best time to survey for hibernation roosts has been included in the main text of section 8 and below the Bat Activity Calendar. It has also been added to Section 1 – Introduction.</p> <p>Reference to this literature is already included in Section 8.</p>	Changes made.
Bat Conservation Trust (BCT)	<p>Section 9 – Minimum Survey Details Note – see Template p28, BCT Bat survey = good practice guidelines.</p>	<p>Doesn't add any further value to sentence. Whether there is one or more survey conducted, the instruction is the same.</p>	No changes required.
Bat Conservation Trust (BCT)	<p>Section 9 – Minimum Survey Requirements – item 2. Add 's' to the word 'survey'.</p> <p>Section 9 – Minimum Survey Requirements – item 4. Add to sentence – '(Including timings /</p>	<p>Agreed.</p>	Change made.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	Section 9 – Minimum Survey Requirements – item 6. Add to sentence – ‘grid reference/s, photos’	Item 14 already mentions photographs but not grid references. Will add grid references to item 14.	Change made.
Bat Conservation Trust (BCT)	Section 9 – Minimum Survey Requirements – item 7. Add to sentence – ‘(Field surveys’, roost surveys, activity surveys)’	The Council does not want to specify either activity, field or roost surveys, but a general survey that covers all.	No changes required.
Bat Conservation Trust (BCT)	Section 9 – Minimum Survey Requirements – item 10. Add to sentence – ‘Equipment used’.	This is already covered in item 8.	No changes required.
Bat Conservation Trust (BCT)	Section 9 – Minimum Survey Requirements – item 11. Add to sentence – ‘and /’	Agreed.	Change made.
Bat Conservation Trust (BCT)	Section 9 – Minimum Survey Requirements – item 12. Add to bullet points - Foraging areas – safeguard / enhance Flight lines - safeguard / enhance	Disagree. Local authorities do not by law have to protect foraging areas of flight lines. Local authorities are concerned with bats and bat roosts.	No changes required.
Bat Conservation Trust (BCT)	Section 9 – Minimum Survey Requirements Add ‘summary’ at the end of section.	Although this does not constitute minimum survey details, it is useful to have and will include this in the guidance.	Changes made.

Respondent	Summary of comment	Response to comment	Changes (if any)
Bat Conservation Trust (BCT)	Section 10 – Second bullet point.	Agreed.	Change made.
Bat Conservation Trust (BCT)	Remove the word 'probably'.	Agreed.	Change made.
Bat Conservation Trust (BCT)	Section 11 – Conditions or obligations Add to bullet points 'Restrictions/prescriptions on lighting'	This section already suggests placing bat boxes in buildings plus bat bricks and slates which will allow bats into the roof space.	No changes required.
Rachael Thwaites	Section 12 – What you can do to Help Bats Add to sentence – 'Bat boxes can also be built into walls or roof spaces.' P 8 Not sure it's a good idea to provide too much information which will help remove evidence... I do know this has happened. Although this would help the applicant determine the need for a survey, it may also give them the opportunity to remove it prior to a survey.	As well as developers, this information is required by case officers determining applications. This information assists the Council in fulfilling its legal obligation for EPS and particularly bats. Both planners and developers/homeowners need to be informed of the law and what is required as per the law when it comes to bats. If a developer or homeowner chooses to remove evidence of bats prior to a request for planning permission, the council cannot be held responsible for any unlawful action taken by them. If caught, those responsible will be dealt with by the appropriate authority according to the law.	No changes required.
Rachael Thwaites	P 8 and 10. I doubt there would be many who would carry out a survey unless specifically asked to do so by the Council.	This information helps case officers understand what is required in a bat survey and what they should do next once it has been received. It also informs developers and homeowners of the process.	No changes required.
Rachael Thwaites	Minimum survey requirements do not strictly adhere to those in BCT's Bat Surveys Good Practice Guidelines, as these recommend that all of the roof should be seen at all times (p61 of the	As stated, this is the 'minimum' requirements requested by the Council. There is reference to the BCT's Bat Survey Good Practice Guidelines for more detailed requirements. Given that this is an opinion and one that is not shared by all surveyors.	No changes required.

Respondent	Summary of comment	Response to comment	Changes (if any)
Rachael Thwaites	<p>2nd edition). This is often not achieved where only one individual carries out the survey. However, this is an opinion not shared by all surveyors.</p> <p>BCT's Bat Surveys Good Practice Guidelines - Also recommends that 2 or 3 (surveys) should be carried out at different times of the summer (p 62/63). This is rarely done in Aberdeenshire and the practical and financial consequences are obvious, however, bats do use a number of roosts and their use can be missed by a single visit.</p>	<p>The council does not request as standard 2 or 3 surveys throughout the summer as this would add further delays to the planning process. That said, if no bats have been found but there is enough evidence to suggest that bats may be there at a different time, then the Council will request a further survey as noted in Section 8 and repeated in Section 10.</p>	No changes required.
Rachael Thwaites	<p>P 12. I'm always banging on about this, but it may be appropriate to use the term 'potential' when dealing with winter surveys rather than inconclusive. Evidence of bats is not always present when bats use hard to find spaces. This again is an area which not all surveyors agree upon.</p>	<p>This is actually page 10. Agreed.</p>	Change made.
Children's Nurseries and Sports Facilities			
Sport Scotland – Lorraine Jones	<p>The respondent previously made comment in relation to this SG in the context of the LDP. As noted at that time, it is considered that the wording of the SG is largely covered within the policies of the LDP itself. Also, it is our view that the wording of the guidance is unduly negative. Sports facilities are a vital part of the life in villages, towns and cities and</p>	<p>Although the two subjects appear unrelated, both are concerned with protecting the local environment and residential amenity. Because of this we would wish to retain the Supplementary Guidance as it is.</p> <p>A further sentence will be added to the Guidance which will make reference to the positive contribution which sports facilities make to the quality of life in Aberdeen.</p>	<p>The further sentence 'Sports Facilities are a vital part of life in villages, towns and cities and can make a positive contribution to the quality of life to the citizens of Aberdeen.' Has been added to the Guidance.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
	<p>while we appreciate that there are amenity and other planning considerations which need consideration in relation to their location, these appear to be covered in other parts of Plan.</p> <p>Reference is made to the Pitch Strategy. This is currently being revised and it would make sense to include reference to the 'updated' Pitch Strategy.</p> <p>In conclusion, we would request that a more positive form of wording is used in this SG, or that an additional sentence is contained within it which makes reference to the positive contribution which sports facilities make to the quality of life in the LDP area.</p>		
Gypsy and Traveller Sites Supplementary Guidance			
Chris Pattison Turnberry Planning	<p>We make the point that specifying detailed requirements around the delivery of such sites, often in absolute terms, is counter-intuitive where few sites have been delivered in previous years when the approach has been less rigorous.</p>	<p>It is important that planning authorities identify suitable locations for meeting the needs of Gypsies and Travellers. Through research undertaken the sites identified would deliver the minimum requirement. This policy is contained in the Adopted Aberdeen Local Development Plan and through the examination into the Plan the Reporters agreed that the approach was reasonable and appropriate.</p> <p>The larger development sites were chosen to contribute towards the development of Gypsy and Traveller sites as the impact on deliverability will be minimised. If there are alternative proposals that deliver the required provision these will be considered either through the</p>	<p>No Changes are suggested.</p>
Chris Pattison Turnberry Planning	<p>The provision is likely to be a barrier to the delivery of sites. Instead, a more pragmatic and flexible approach needs to be adopted if the flow of sites which need to provide on site provision should be.</p>		<p>No Changes are suggested.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
Chris Pattison Turnberry Planning	<p>Include the following at Paragraph 1.7: “This Guidance provides a framework through which Gypsy/Traveller sites can be located, developed and managed, but successful delivery is a key priority and the overarching policy framework and detailed provisions of this Guidance should be seen in this context.”</p>	<p>Masterplanning Process or Development Management process.</p> <p>The purpose of policy H6, H7 and the Supplementary Guidance is to assist in the successful delivery of sites for Gypsies and Travellers. Additional text has been included at paragraph 1.5 to make this clear.</p>	<p>Insert the following text: Therefore, the successful delivery of Gypsy and Traveller Sites through the Local Development Plan is a key priority.</p>
Chris Pattison Turnberry Planning	<p>We suggest that paragraph 2.2 of the Guidance is amended to make it clear that the nature of the contribution from the LDP sites will be reviewed at the time a planning application is made, having regard to the up to date position across the City.</p>	<p>The provision of Gypsy and Traveller sites is an ongoing issue that requires to be addressed. There is no alternative strategy that will deliver additional pitches in Aberdeen City. If there is a change in circumstances then the taken into account when the plan is reviewed. It is outwith the scope of Supplementary Guidance to make changes to this policy requirement.</p>	<p>No Changes are suggested.</p>
Chris Pattison Turnberry Planning	<p>Who or how the site will be managed will be delivered in not discussed. It is assumed that the land required for the pitches will be transferred to the Council or a registered social landlord who will take forward the proposal in accordance with the guidance.</p>	<p>It would most likely be the responsibility of the Council, but there may be a registered social landlord that would be interested in such a project.</p>	<p>To clarify additional information has been provided at Paragraph 2.3.</p>
Chris Pattison Turnberry Planning	<p>Greater clarity on the issue of long term management is required, particularly in respect of temporary halting sites which are potentially susceptible to abuse and as such will impact on the surrounding development.</p>	<p>It is a requirement of Policy H6 – Gypsy and Traveller Caravan Sites that it can be demonstrated that the site will be properly managed.</p>	<p>To make it clear that this is a requirement on new sites paragraph 2.3 makes it clear that sites will need to meet the criteria in Policy H6.</p>

Respondent	Summary of comment	Response to comment	Changes (if any)
Aberdeenshire Council	The supplementary guidance provides a clear statement of the process of identifying suitable sites and requirements that need to be considered.	This is Noted.	No Changes are suggested.
Aberdeenshire Council	<p>Whilst it is for Aberdeen City to decide the criteria for new sites and site management (using Good Practise guide as a basis) within the local authority boundary it appears to be quite prescriptive in places and it might be difficult for new sites to comply with all of the requirements.</p> <p>Both Aberdeen City and Aberdeenshire should be consistent with the level of provision recommended within the respective LDPs for gypsy and traveller sites. The supplementary guidance seeks a higher level of provision within Aberdeen City than is being sought within Aberdeenshire.</p>	<p>The level of provision will vary greatly depending on the type of site that is to be provided. In the circumstances that a permanent site is to be developed it is considered appropriate that higher standards are expected. The expectation would be for any new permanent sites to be integrated with the local community and provide greater access to services and facilities. For the temporary halting sites a the services expected will not be the same and these have been amended to ensure that they are consistent with the approach being taken by Aberdeenshire Council.</p>	<p>The services and facilities required for halting sites detailed at paragraph 3.16 has been amended. The requirement for waste water facilities as a minimum has been removed.</p>
Aberdeenshire Council	It is clear what level of contribution is required from the sites listed under Policy H7 e.g. equivalent of approx 15 affordable housing units. However, further clarity is suggested with regard to the method of contribution for the sites (Greenferns and Countesswells) which are to provide off site e.g. is a commuted sum for the equivalent of 15 affordable housing units being sought?	For sites where the delivery is to be off-site it is expected that a commuted payment equivalent to 15 affordable units is made towards the provision of Gypsy and Traveller sites.	To clarify this point make it clear that this is a requirement on new sites paragraph 2.3 makes it clear that sites will need to meet the criteria in Policy H6.

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Supplementary Guidance: Wind Turbine Development in Aberdeen City
REPORT NUMBER:	EPI/12/217

1. PURPOSE OF REPORT

- 1.1 The Planning etc. (Scotland) Act 2006 paragraph 22 (1) states that a planning authority may adopt and issue guidance in connection with a local development plan.
- 1.2 The purpose of this report is to obtain Committee approval to consult on Supplementary Guidance to provide further detail on applications for Wind Turbines. A copy of the Supplementary Guidance is contained in Appendix 1.

2. RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
 - (a) Approve the draft Wind Turbines Supplementary Guidance document for a six week public consultation.
 - (b) Agree that following completion of the relevant consultation, any comments received and subsequent amendments to the draft Guidance be presented to a future meeting of this Committee.

3. FINANCIAL IMPLICATIONS

- 3.1 There are no direct financial implications arising from this report, other than costs incurred through consultation and publicity related to the proposed guidance. Any such expenses incurred can be met through existing budgets. The implication for the priority based budgeting is positive. Detailed topic-based guidance has value in reducing officer time spent on pre-application discussions, and will therefore prove beneficial in reducing costs. This relates to PBB option EPI PSD02 - Rationalise planning application management.

4. OTHER IMPLICATIONS

- 4.1 There are no known legal or equipment implications arising from this report.
- 4.2 As a major landowner in the city, proposals for the development of land and assets owned by Aberdeen City Council will be subject to assessment in line with the principles and standards set out in the guidance, where applicable.
- 4.3 The progression of the guidance document will provide a clear framework for decision making, allowing comprehensive guidance for both applicants and officers, thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development.
- 4.4 The proposed Supplementary Guidance on Wind Turbines will provide clear instruction on the level of information that is required with planning applications for wind turbines. The document also gives map based guidance on the known environmental and safety constraints to the development of wind turbines. This does not guarantee that planning permission will be refused within these areas, or that planning permission will be approved out with these areas. The guidance will work towards helping to achieve the Scottish Government's target of for 40% of Scotland's electricity to be generated from renewable sources by 2020. This will help to mitigate the impact of Climate Change, and provide long term environmental benefits to Aberdeen City and beyond.

5. BACKGROUND/MAIN ISSUES

- 5.1 This Supplementary Guidance was reported to Enterprise, Planning and Infrastructure Committee on 15 November 2011 for public consultation. At this point in time it was considered that there would be no requirement to undertake a full strategic environmental assessment of the Supplementary Guidance. However, following consultation with the consultation authorities it was recommended that a strategic environmental assessment would be required. This process, to-date, has involved consultation on a Scoping Report and carrying out a strategic environmental assessment of the Guidance and producing an Environmental Report. Through this assessment three specific impacts on the environment have been identified and mitigation measures are required to be incorporated into the Supplementary Guidance. The changes made to the guidance are the reason that it is being reported to Enterprise, Planning and Infrastructure for a second time.
- 5.2 Through the environmental assessment it was identified that there is the potential for the Guidance to:
 - impact on birds from designated sites close to Aberdeen, namely, Loch of Skene and the Ythan
 - impact on bats, a European Protected Species
 - impact on peat land and the loss of carbon as a result of development

5.3 Mitigation measures have been incorporated into a revised Supplementary Guidance to minimise the environmental impact. A copy of the full Environmental Report is available

5.4 A copy of the Environmental Report can be viewed online at: http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_committee_reports.asp

6. IMPACT

6.1 The Local Development Plan and associated Supplementary Guidance contribute to the following Single Outcome Priorities: 1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2. We realise our full economic potential with more and better employment opportunities for our people; 10. We live in well designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.

6.2 The Local Development Plan and associated Supplementary Guidance supports the Council's Corporate Plan, in particular the aim of delivering high levels of design from all development, maintaining an up to date planning framework, promoting sustainable development.

6.3 The Local Development Plan and associated Supplementary Guidance contribute to "Aberdeen – A Smarter City" and the Smarter Environment by maximising the use of low carbon technology and materials and increase energy efficiency and introduce carbon reduction measures in our processes and our housing and non housing assets to reduce our carbon footprint, save money and to bring people out of fuel poverty.

6.4 The proposal is consistent with the Planning and Sustainable Development Service Plan, in particular Adoption of Supplementary Guidance to Local Development Plan and production of Technical Advice Notes

6.1 An equalities and human rights impact assessment (EHRIA) has been carried out in relation to the proposed Supplementary Guidance documents, with the results included as Appendix 3 to this report.

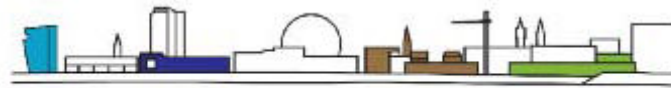
7. BACKGROUND PAPERS

- Aberdeen Local Development Plan – Proposed Plan http://www.aberdeencity.gov.uk/Planning/ldp/pla_aldp_document_map.asp
- Appendix 1 - Supplementary Guidance: Wind Turbine Development in Aberdeen City

- Appendix 2 – Environmental Report (available online at: http://www.aberdeencity.gov.uk/planning_environment/planning/local_development_plan/pla_aldp_committee_reports.asp)
- Appendix 3 – Equalities and human rights impact assessment

8. REPORT AUTHOR DETAILS

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aberdeen local development plan

Draft Supplementary Guidance

Topic: Wind Turbine Development
in Aberdeen City

November 2012

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1. Introduction

The development of renewable energy technologies, on all scales, is supported by Aberdeen City Council. A positive approach to renewable developments will help to meet the Scottish Government's target for 40% of Scotland's electricity to be generated from renewable sources by 2020. Renewable technologies are becoming more common place within Scotland, and the range of technologies available includes wind power, solar power, heat pumps and biomass boilers. The guidance in this document focuses on wind power renewable energy technologies. Scotland has a good wind resource which should be taken advantage of however this should not be achieved at the detriment of built and natural heritage or air safety. There needs to be a balance between a commitment to conserve and enhance our built and natural heritage, and supporting and promoting the growth of renewable energy generating developments.

2. Aim of the document

The aim of this document is to provide concise information and guidance to assist in the positive planning for wind powered renewable energy developments of different scales in Aberdeen. This guidance highlights the key issues that must be considered when planning for wind energy developments. Planning for wind energy development is complex. Variables in site location, turbine heights, styles and scale of development make it difficult to provide certainty on definitive solutions for wind turbine developments without detailed assessment and appraisal.

The document is divided into two distinct parts. The first part outlines the level of information that will be required with each application to ensure a well informed decision can be made. This list is not exhaustive but does outline the main considerations that need to be addressed with any application. The second part of the guidance is map based and highlights areas within Aberdeen City Council's boundary where wind turbine development may be significantly constrained.

3. What this guidance covers

The typical wind energy renewable generating technologies proposed in Aberdeen are likely to fall within the micro-renewable category. The term micro-renewable is used to describe a non-commercial renewable energy development, which provides heat and/or electricity to a single end user, be it a single dwelling house, office or community facility. Permitted development rights are in place for the installation, alteration or replacement of a free standing wind turbine within the curtilage of a dwelling. For further guidance on this please contact the Planning and Sustainable Development Department.

This document gives guidance for both micro-renewable and renewable energy turbine developments. Please see appendix 1 for further guidance on definitions of micro-renewable and renewable energy developments. The material considerations planning officers will regard when determining or recommending a wind turbine application are consistent no matter what the scale of the application is; however, the level of detail required for each

application is dependent on what is proposed and a number of other material considerations, as outlined below.

4. Information to be submitted with any planning application for wind turbine developments

In determining applications for one or more wind turbines Aberdeen City Council will expect the supporting information each application to address the issues listed below. Further information such as an Environmental Impact Assessment may be required. If more than two turbines are proposed, or if turbines are more than 15m in height, they are classed as Schedule 2 developments under the Environmental Assessment Regulations. It is then a matter for Aberdeen City Council to decide whether the turbines are likely to have significant environmental effects and therefore require an Environmental Impact Assessment screening option. It is strongly recommended that applicants submit a request for a screening opinion before any such application is submitted to avoid delay in determining the subsequent application.

Aberdeen City Council is required to consult the British Aviation Authority, National Air Traffic Services and Ministry of Defence on all applications for wind turbines. The Civil Aviation Authority also required to be consulted if the turbine is over 90 meters. Applicants are encouraged to submit a Wind Farm Developers Proforma with their application. The proforma can be found at http://www.bwea.com/docs/developers_proforma.doc
A copy is also attached in appendix 2 of this guidance.

As a design principle, it is desirable that, where a wind energy development already exists nearby, a similar scale and design of turbine should be proposed in any new developments. This however is heavily dependent on topographical features and other material considerations outlined below.

4.1 Technical information

The detail and specification of the proposed wind turbine(s) will need to be provided. Detail will need to be given of the:

- type of turbine proposed,
- rated generating capacity of the turbine,
- materials and colour of the wind turbine components,
- foundations material, depth and size,
- separation distances between turbines (if more than 1 turbine proposed),
- ancillary equipment/structures (if proposed),
- construction and operational access requirements including details of access tracks, transmission cable routes and borrow pits,
- proposals for decommissioning, and
- landscaping works proposed.

4.2 Planning statement

The following documents set the baseline policy on which applications will be judged. This does not form an exhaustive list. A planning statement needs to

be submitted with the planning application which evaluates the proposal against the following documents:

- Scottish Planning Policy
- Aberdeen City and Shire Strategic Development Plan
- Aberdeen City Council Local Development Plan
- Scottish Government's Planning Advice Note 45 Annex
- Scottish Government's Specific Advice Sheet - onshore wind turbines
- Scottish Natural Heritage's Siting and designing wind farms in the landscape
- Aberdeen City Councils Supplementary Guidance on Wind Turbine Developments

4.3 Ecological assessment

The impacts of proposed turbines on wildlife, habitats, ecosystems and biodiversity will need to be considered. Ecological assessments of proposed wind turbine sites will need to:

- outline any natural heritage designations for the site or that may immediately adjoin it such as Special Protection Areas, Special Areas of Conservation, Sites of Special Scientific Interest, Local Nature Reserves and Local Nature Conservation Sites.
- classify and evaluate the natural habitat and species,
- assess the potential affect(s) on protected species including bats and birds;
- classify and evaluate the agricultural context,
- outline any hydrological impacts,
- evaluate the impact of a wind turbine(s) on these, and
- discuss the scope of mitigation on the possible and proposed impacts.

4.4 Landscape assessment

The key natural heritage issue relating to turbines is likely to be that of landscape particularly in rural areas and on the urban edge. The ability of the landscape to absorb development often depends on the features of the landscape. This can also be influenced by careful siting and design. A cautious approach is necessary in relation to landscapes which are rare or highly valued. Aberdeen City Council's Technical Appendix on Landscape Characteristics is available on the Aberdeen City Council website from the link below:

(<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=31730&slD=14344>).

Applicants must:

- consider the character of the landscape, and outline if the proposed site is a ridge, hill, valley, coastal area and the vegetation present,
- evaluate the landscape on quality, value and scale terms,
- consider the impact of the wind turbine(s) on the landscape,
- consider the cumulative impact of the proposed application with regard to wind turbines that are already in existence or where planning permission has been approved. This will include proposed and approved turbines located in Aberdeenshire.

4.5 Visual assessment

A visual assessment should be submitted. This assessment should be carefully scoped so that it is appropriate to the size and scale of the development and the likelihood of significant landscape and visual impacts, including cumulative effects. The assessment should include:

- a viewpoint analysis,
- determination of the zone of theoretical visibility of the proposed development,
- evaluation of the visual impact,
- the scope for mitigation of those impacts, and
- details of the location, visual impact and the restoration of borrow pits,
- consider the cumulative impact of the proposed application with regard to wind turbines that are already in existence or where planning permission has been approved. This will include proposed and approved turbines located in Aberdeenshire.

Evaluation of impacts should include consideration of alternative siting for the turbine(s) (as well as alternative colouring), borrow pits and ancillary equipment. Represented viewpoints of the proposal should cover both long and short range visibility and presentation by 'photomontage' or 'videomontage' is recommended.

Individual circumstances will dictate the optimum position for wind turbines. This will be influenced by the size of the installation and its surrounding environment. The potential siting of wind turbines close to, on, or integrated with buildings means special attention must be given to the need to protect amenity.

4.6 Noise assessment

There are two distinct types of noise sources within a wind turbine, the mechanical noise produced by the gearbox, generator and other parts of the drive train; and the aerodynamic noise produced by the passage of the blades through the air. The level of detail required will depend on the scale of the proposal and the separation distance between wind turbines and noise sensitive properties. A noise assessment is not required for systems which are less than 20m to the hub and/or less than 32m to the tip of the blade.

A noise assessment will have to take into account:

- the individual effects of both the noise sources,
- the cumulative effects of both the noise sources,
- the character and sensitivities of the area (including the prevailing winds, landform and particularly noise sensitive receptors such as dwellings).

4.7 Shadow flicker assessment

Shadow flicker is the term used to describe the impact of shadows cast by rotating wind turbine blades through constrained openings, such as the windows of neighbouring properties. The small diameter and likely location of micro-renewable turbines greatly reduces the probability of shadow flicker. For larger turbines, shadow flicker can be mitigated by simple measures. These range from planting tree belts between the affected window and the

turbines through to shutting down the turbines during periods when shadow flicker could theoretically occur.

An assessment of potential shadow flicker and shadow throw throughout the year should be provided for all dwellings within a 10 rotor diameter of the proposed location of the wind turbine.

4.8 Ice throw

Turbines, under special meteorological conditions, may be covered by ice. If a wind turbine operates in icing conditions, two types of risks may occur if the rotor blades collect ice. The fragments from the rotor may be thrown off from the operating turbine due to aerodynamic and centrifugal forces or they may fall from the turbine when it is shut down or idling without power production. When icing occurs the turbines' own vibration sensors are likely to detect the imbalance and inhibit the operation of machines.

Locating turbines a safe distance from any occupied structure, road, or public use area will mitigate the risk of ice throw. Warning signage may be a useful precaution.

4.9 Built and cultural heritage assessment

Any built and cultural heritage assets will have to be noted, and an assessment of any known or potential impacts carried out. Assets which need to be considered are:

- archaeological sites,
- listed buildings,
- conservation areas,
- historic gardens,
- designated landscapes, and
- local sites of cultural importance.

There may be opportunity to site micro wind turbines in conservation areas or within the curtilage of listed buildings. It will not normally be possible to site turbines on scheduled ancient monuments and it will be difficult to site them on listed buildings. Care must be taken to ensure respect is paid to the site and setting and to important views and vistas to and from these buildings, monuments and sites. It is important that consideration is given to assets outwith Aberdeen that may affect their setting.

4.10 Tourism and countryside access assessment

The draw of Scotland as a tourist destination is well known. Turbine developments will need to assess any visual and amenity impacts on tourist and recreational facilities or tourism and countryside access.

Assessment will need to include the impacts on:

- core path network,
- footpaths,
- cycleways,
- bridleways, and
- public paths.

4.11 Public safety

To inform the potential public safety risk of a wind turbine development an informal risk assessment of the proposed development should be submitted. This should take particular account of

- proximity of surrounding buildings and roads, and
- risk of injury to humans through catastrophic equipment failure or ice throw.

4.12 Electro-magnetic interference (aviation and communication)

The British Aviation Authority, Civil Aviation Authority and Ministry of Defence should be consulted on proposed wind turbines in accordance with their guidance 'Wind energy and aviation interim guidelines', please see <http://www.bwea.com/pdf/Wind-Energy-and-aviation-interim-guidelines.pdf>

These bodies will in turn consult other organisations that could have an interest in wind turbine proposals in terms of flight paths, radar and navigation aids, television and radio transmissions. Details of possible adverse effects and appropriate measures to alleviate effects should be submitted with the application.

The proforma in Appendix 1 of this document was compiled by the Civil Aviation Authority, the Ministry of Defence, the National Air Traffic Service and the British Wind Energy Association to assist in the processing and assessment of wind turbine applications and applicants are encouraged to complete it.

4.13 Wind regime

The power produced by wind turbines depends on two key factors – the strength of the wind, and the area swept by the rotor. Assessing whether a particular site will harness sufficient wind power usually entails using historical meteorological data, with annual mean wind speed data available from the Meteorological Office, and obtaining information from anemometers on site.

The applicant must demonstrate that the proposal is viable. It needs to be shown that there is enough wind speed and this can reasonably be predicted after monitoring the site. For micro-renewable turbines evidence and data from four months of monitoring will be required. Renewable turbines will require a longer monitoring period; typically 12 months will be necessary.

4.14 Grid network

Access to the power electricity transmission and distribution system is required for commercial wind turbines. Micro-renewable turbines can be connected to the grid. Detail would be required on the proposed grid connection or supply to local user, if relevant.

4.15 Peat Land

Peat land is an important habitat and acts as a carbon sink and the development of wind turbines on peat land will result in the loss of carbon. To ensure that the carbon balance savings of the scheme is maximised developments should be designed to minimise soil disturbance when building roads and tracks, turbine bases and other infrastructure.

4.16 Other issues

A number of other issues will need to be considered when proposing development. These include:

- community consultation for renewable energy developments,
- local employment/business considerations,
- any associated community benefits, and
- cumulative impacts.

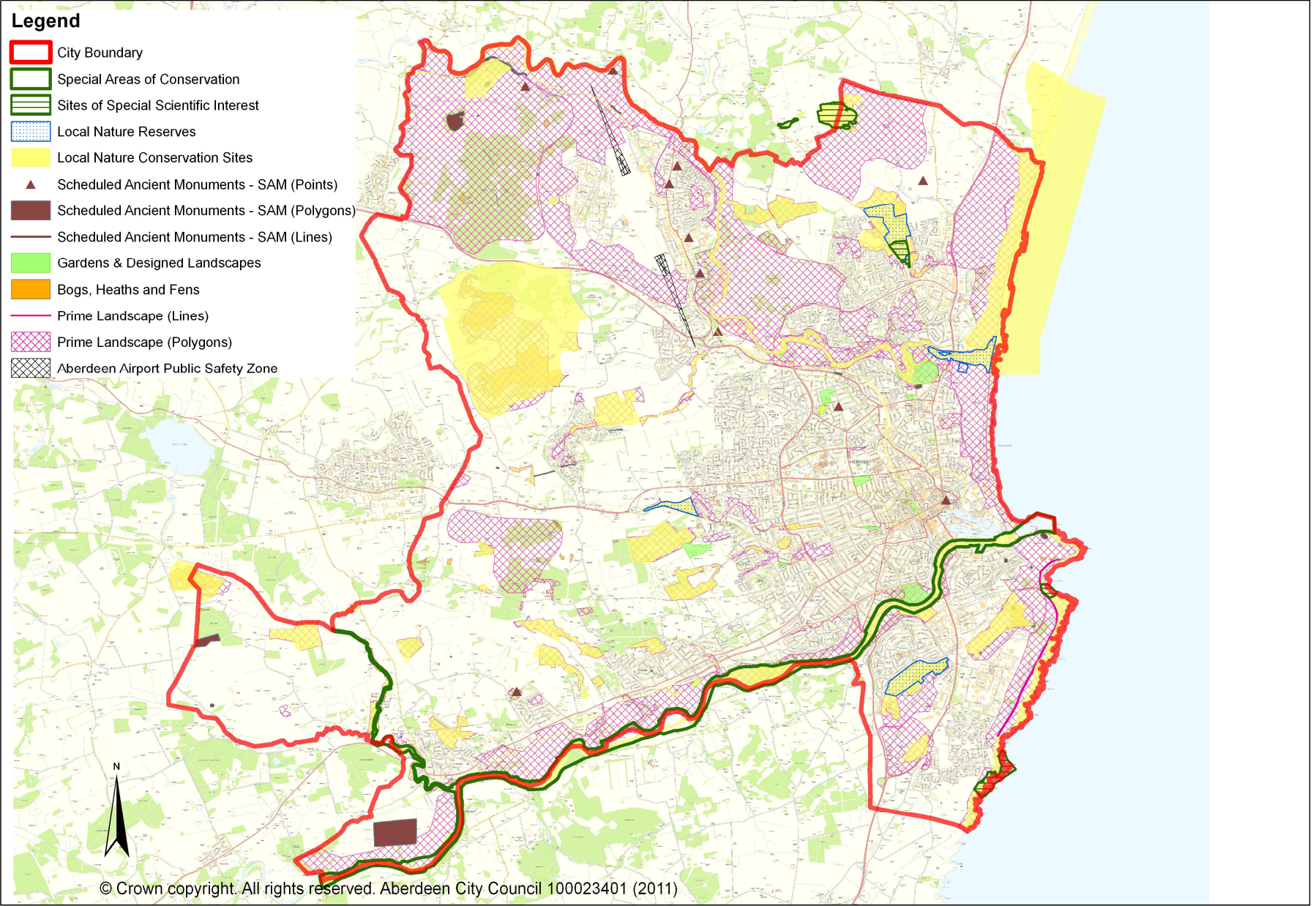
5. Map of constrained areas for wind turbine development

The following maps highlight areas the following information:

- Special Areas of Conservation
- Sites of Special Scientific Interest
- Local Nature Reserves
- Local Nature Conservation Sites
- Scheduled Ancient Monuments
- Gardens and Designated Landscapes
- Bog land, Heath land and Fens
- Areas of Local Landscape Significance – Primary Landscapes
- Airport Safety Exclusion Zone

It is important to note that this map does not identify areas of search and there is not a presumption in favour of those areas free from the identified constraints.

These must be considered as constraints to the development of wind turbine developments. In these areas proposals will be required to demonstrate that the proposed development offers exceptional benefits and that these outweigh any adverse environmental impacts. There is no available information on areas within the Radar Exclusion Zone and the impact of wind turbines on air safety is assessed by the relevant bodies, these being the British Airports Authority, the National Air Traffic Services, the Ministry of Defence and in some instances the Civil Aviation Authority.



For further information please see:

Scottish Planning Policy

Aberdeen City and Shire Strategic Development Plan

Aberdeen Local Development Plan

Scottish Government's Planning Advice Note 45 Annex

Scottish Government's Specific Advice Sheet - onshore wind turbines

Scottish Natural Heritage's 'Siting and designing wind farms in the landscape'

Scottish Natural Heritage's 'Natural Heritage assessment of small scale wind energy projects which do not require formal Environmental Impact Assessment'

Scottish Natural Heritage - Micro renewables and the natural heritage - Guidance Note. October 2009

Scottish Natural Heritage - Windfarm impacts on birds guidance

Natural England Technical Information Note TIN051 - [Bats and onshore wind turbines](#) *Interim guidance*

The British Wind Energy Association 'Wind energy and aviation interim guidelines'

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APPENDIX 1: Micro-Renewable and Renewable Energy Developments

To clarify the distinction between micro-renewable and renewable wind turbine technology is it helpful to classify turbines depending on the size and number proposed in one development.

Classification	Description	Approximate hub height/blade diameter
Single micro-renewable	A single turbine which produces less than 0.05MW capacity for the production of electricity.	<20m to hub and/or <32m to tip
Cluster micro-renewable	A cluster of turbines which produce less than 0.05MW capacity for the production of electricity.	<20m to hub and/or <32m to tip
Single renewable	1 turbine of installed capacity between 0.05MW and less than 3MW	20-40m to hub and/or 32-65m to tip
Cluster renewable	2-3 turbines or installed capacity between 3MW and less than 6MW	>40m+ to hub and/or > 65+ tip
Small scale renewable	4-10 turbines or installed capacity between 6MW and less than 16 MW	>40m+ to hub and/or > 65+ tip
Medium scale renewable	11-20 turbines or installed capacity between 16MW and less than 31MW	>40m+ to hub and/or > 65+ tip
Large scale renewable	21 or more turbines or installed capacity greater than 31MW	>40m+ to hub and/or > 65+ tip

To date most applications have fallen within the single micro-renewable, cluster micro-renewable or single renewable category.

Civil Aviation & Ministry of Defence Safeguarding

NOTICE TO WIND FARM DEVELOPERS

Please submit a completed application form for all new or revised onshore and offshore wind farm plans. This form has been compiled in consultation with the British Wind Energy Association. Its purpose is to standardise the information provided and to expedite the assessment of your proposed wind farm development. Assessment is made against air safety and defence interests, through evaluation of the possible effects on air traffic systems, defence systems and low flying needs.

NOTICE TO PLANNING AUTHORITIES

This form has been compiled with the assistance of the Civil Aviation Authority (CAA), the Ministry of Defence (MOD), the National Air Traffic Service (NATS) and the British Wind Energy Association (BWEA), to assist in the processing and assessment of wind farm applications. It is important that copies of this form are forwarded within the planning consultation process. This will help these organisations trace their records of any earlier consultations, as well as provide them with the relevant information for their assessments.

WHAT TO DO WITH THIS FORM

Please provide as much detail as possible by **filling in the shaded areas**. If the specific turbine and/or exact positions have yet to be established then fill in the likely turbine size (hub height, rotor diameter) and boundary points as a minimum. On completion send copies to both the following addresses.

deopsnorth-lmswind@de.mod.uk

or -

St George's House
Kingston Road
Sutton Coldfield
B75 7RL

windfarms@caa.co.uk

or -

Directorate of Airspace Policy
K6 Gate 3
CAA House
45-49 Kingsway
London, WC2B 6TE

It is important that a copy of this form is retained for inclusion with subsequent planning applications at the same site. If no application has been made prior to a planning application, please include a completed form in your planning application.

Wind Farm Name	
Also known as:	
Developers reference	
Application identification No.	
Related/previous applications (at or near this site): Provide reference names or numbers	

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Developer Information	
Company name:	
Address:	
Contact:	
Telephone:	
Facsimile:	
e-mail:	

Relevant Wind Turbine Details	
Wind turbine manufacturer:	
Wind turbine model:	
Wind farm generation capacity (MW)	Number of turbines
Blade manufacturer	
Number of blades	
Rotor diameter	Meters
Rotation speed (or range)	Rpm
Blade material including lightning conductors	
Wind turbine hub height	Metres
Tower design (* delete as required)	* Tubular * Lattice
Tower base diameter/dimensions	Metres
Tower top diameter/dimensions	Metres

Comments
Are there any details or uncertainties that may be helpful to add ?

--

Turbine Locations

Please provide as much information as you can. The position and height above sea level of every machine if available, the site boundary if not. The height above sea level is the above ordinance datum (AOD) used to specify all heights on OS maps. Please note grid references **and** latitude/longitude and must be included. For co-ordinate conversion: www.gps.gov.uk

An Ordinance Survey (OS) map, or maritime chart, should be submitted with this pro-forma, showing locations of proposed turbine/turbines or scheme boundaries. Please number the turbines or boundary points on the map, to correlate with the information provided below.

Copy this page as necessary to account for all turbines or boundary points, or attach an excel spreadsheet with wind turbine coordinate information.

Wind farm Name & Address:	
--------------------------------------	--

Turbine no.		Height AOD (m) of tower base	
Grid Reference		100 km square letter(s) identifier	
Easting (10 m)		Northing (10 m)	
	Degrees	Minutes	Seconds
Latitude			
Longitude			

Turbine no.		Height AOD (m) of tower base	
Grid Reference		100 km square letter(s) identifier	
Easting (10 m)		Northing (10 m)	
	Degrees	Minutes	Seconds
Latitude			
Longitude			

Turbine no.		Height AOD (m) of tower base	
Grid Reference		100 km square letter(s) identifier	
Easting (10 m)		Northing (10 m)	
	Degrees	Minutes	Seconds
Latitude			
Longitude			

Turbine no.		Height AOD (m) of tower base	
Grid Reference		100 km square letter(s) identifier	

Easting (10 m)					Northing (10 m)				
	Degrees			Minutes		Seconds			
Latitude									
Longitude									

CONTACT US

Please get in contact if you wish to discuss your proposal with us:
 Planning and Sustainable Development
 Enterprise, Planning & Infrastructure
 Aberdeen City Council
 Business Hub 4
 Ground Floor North
 Marischal College
 Broad Street
 Aberdeen
 AB10 1AB
 Tel: 01224 523470
 Fax: 01224 636181
 Email pi@aberdeencity.gov.uk

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Equality and Human Rights Impact Assessment - the Form

There are separate guidance notes to accompany this form – “Equality and Human Rights Impact Assessment – the Guide.” Please use these guidance notes as you complete this form. Throughout the form, **proposal** refers to policy, strategy, plan, procedure or report.

STEP 1: Identify essential information

1. Committee Report No.

2. Name of proposal.

3. Officers completing this form.

Name	Designation	Service	Directorate
Daniel Harrington	Senior Planner	Planning & Sustainable Development	Enterprise, Planning & Infrastructure

4. Date of Impact Assessment.

5. When is the proposal next due for review?

6. Committee Name.

7. Date the Committee is due to meet.

8. Identify the Lead Council Service and who else is involved in the delivery of this proposal. (for example other Council services or partner agencies)

The intended proposal document is to be used primarily by officers within the Council's Planning & Sustainable Development Service as an important tool in the assessment and determination of planning applications. The guidance contained within the document may also be of use to other Council services when considering improvements/alterations to Council-owned residential property.

9. Please summarise this Equality and Human Rights Impact Assessment, (EHRIA). This must include any practical actions you intend to take / have taken to reduce, justify or remove any adverse negative impacts (if necessary continue on blank sheet of paper). **Please return to this question after completing EHRIA.**

There are no negative impacts outlined as a result of the EHRIA.

10. Where will you publish the results of the Equality and Human Rights Impact Assessment? Tick all that apply.

- Summary of EHRIA will be published in committee report under section "Equality Impact Assessment"
 Full EHRIA will be attached to the committee report as an appendix
 Summary of EHRIA to be published on Council website within relevant service pages

STEP 2: Outline the aims of the proposal

11. What are the main aims of the proposal?

The purpose of this supplementary guidance document is to provide further information and detail in respect of policies set out in the Local Development Plan, in accordance with the Scottish Government's intention that the Local Development Plan itself focuses on vision, the spatial strategy, overarching and other key policies, and proposals.

Subject to any representations received and amendments made as a result of the consultation process, it is intended that this documents be subsequently adopted as a technical advice note.

The progression of the technical advice note will provide a clear framework for decision making, allowing comprehensive guidance for applicants and thereby making a significant contribution towards the Council's aim of promoting and achieving sustainable development.

12. Who will benefit most from the proposal?

Residents and businesses in Aberdeen, including stakeholders who have been involved in the preparation of the Aberdeen Local Development Plan – the development industry, key agencies, citizens, Council planning officers and other Council services.

13. Tell us if and how the proposal will increase equality of opportunity by permitting positive action to redress disadvantage?

The document provides all citizens with the opportunity to contribute to the preparation of supplementary guidance on the development of wind turbines in Aberdeen. As part of the modernisation of the planning system in Scotland, public consultation plays a vital role in the preparation of development plans, meaning that people’s involvement can make a real difference to the content of the plan. There is no known disadvantage to any equality target groups or related equality strands, though the consultation process allows all parts of society equal opportunity to engage with the preparation of this guidance and to make representations accordingly.

The consultation process will enable local communities, groups and individuals influence the content of the document and have their say in the design of our built environment.

14. What impact will the proposal have on promoting good relations and wider community cohesion?

The supplementary guidance is intended to form a part of the Aberdeen Local Development Plan on its eventual adoption. The Proposed Plan and its accompanying documents set out Aberdeen City Council’s vision for the growth and development of the city. The opportunity to make representations on that vision allows citizens to influence the way in which their city will grow over time. It is hoped that this will foster a sense of belonging and involvement in those who have engaged with the process.

The consultation proposed for this document do not exclude or disadvantage any particular group or part of society. Documents are made available through a range of media sources, and representations can be accepted in a number of ways.

STEP 3: Gather and consider evidence

15. What evidence is there to identify any potential positive or negative impacts in terms of consultation, research officer knowledge and experience, equality monitoring data, user feedback and other?

Information from external sources indicates that proposal for wind farms prove contentious. There have been no planning applications for wind farms within Aberdeen City. Previous planning applications for single turbines within Aberdeen City have either drawn no objections or a low number of objections.

STEP 4: Assess likely impacts on equality strands

16. Which, if any, equality target groups and others could be affected positively or negatively by this proposal? Place the symbol in the relevant box.

(Positive +, neutral 0, - negative)

Equality Target Group					
Race*	0	Disability	0	Gender**	0
LGB***	0	Belief	0	Younger	0

Older	0	Others e.g. poverty	0	
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* Race includes Gypsies/Travellers

** Gender includes women, men, Transgender

*** LGB: Lesbian, Gay and Bisexual

17. Please detail the potential positive and/or negative impacts on the groups you have highlighted above? Detail the impacts and describe the groups affected.

<p>Positive impacts The publication of Technical Advice is an invitation for all groups to engage with planning issues within Aberdeen. Once we publish the Supplementary Guidance there will be a chance for everyone to make comments on the documents.</p>	<p>Negative Impacts (describe groups affected)</p>
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STEP 5: Apply the three key assessment tests for compliance assurance

18. Does this policy/procedure have the potential to interfere with an individual’s rights as set out in the Human Rights Act 1998? State which rights might be affected by ticking the appropriate box(es) and how. **If you answer “no”, go to question 22.**

<input type="checkbox"/> Article 3 – Right not to be subjected to torture, inhumane or degrading treatment or punishment <input type="checkbox"/> Article 6 – Right to a fair and public hearing <input type="checkbox"/> Article 8 – Right to respect for private and family life, home and correspondence <input type="checkbox"/> Article 10 – freedom of expression <input type="checkbox"/> Other article not listed above <p>How?</p>

Legality

19. Where there is a potential negative impact is there a legal basis in the relevant domestic law?

--

Legitimate aim

20. Is the aim of the policy a legitimate aim being served in terms of the relevant equality legislation or the Human Rights Act?

Proportionality

21. Is the impact of the policy proportionate to the legitimate aim being pursued? Is it the minimum necessary interference to achieve the legitimate aim?

STEP 6: Monitor and review

22. How will you monitor the implementation of the proposal? (For example, customer satisfaction questionnaires)

The impact of the supplementary guidance will be monitored through the planning application process

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23. How will the results of this impact assessment and any further monitoring be used to develop the proposal?

Where necessary, the Supplementary Guidance will be updated to take into account feedback from monitoring.

STEP 7 SIGN OFF

The final stage of the EHRIA is formally to sign off the document as being a complete, rigorous and robust assessment.

Person(s) completing the impact assessment.

Name	Date	Signature
Daniel Harrington	4 October 2012	Daniel Harrington

Quality check: document has been checked by

Name	Date	Signature
Andrew Brownrigg	4 October 2012	<i>A. Brownrigg</i>

Head of Service (Sign-off)

Name	Date	Signature
Maggie Bochel	4 October 2011	Maggie Bochel

Now –

Please send an electronic copy of your completed EHRIA - without signatures - together with the proposal to:

Equalities Team
 Customer Service and Performance
 Corporate Governance
 Aberdeen City Council
Business Hub 13
 Second Floor North
 Marischal College
 Broad Street
 Aberdeen
 AB10 1AB

Telephone 01224 523039 Email sandrab@aberdeencity.gov.uk

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	06 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Technical Advice Note: The Repair and Replacement of Windows and Doors
REPORT NUMBER:	EPI/12/189

1. PURPOSE OF REPORT

- 1.1 The purpose of this report is to obtain Committee approval for a revised draft of Technical Advice on the Repair and Replacement of Windows and Doors to be issued for a six week period of public consultation. A copy of the revised draft is available as Appendix 1 to this Report.

2. RECOMMENDATION(S)

It is recommended that the Committee:

- (a) Approve the Repair and Replacement of Windows and Doors Technical Advice Note document for a six week public consultation period.
- (b) Agree that, following completion of the public consultation, any comments received and subsequent amendments to the draft advice be presented to a future meeting of the Committee.

3. FINANCIAL IMPLICATIONS

- 3.1 There are no direct financial implications arising from this report with the exception of costs which may be incurred through the consultation and publicity of the proposed advice. Any such expenses can be met through existing budgets.
- 3.2 The implication for the Priority Based Budgeting is positive. Detailed topic-based advice has value in reducing officer time spent on pre-application discussions, and will therefore prove beneficial in reducing cost.

4. OTHER IMPLICATIONS

- 4.1 There are no known legal or equipment implications arising from this report.
- 4.2 As a major landowner in the city, proposals for the development of land and assets owned by Aberdeen City Council will, where this is applicable, be subject to assessment in line with the principles and standards set out in the Technical Advice Note.

5. BACKGROUND / MAIN ISSUES

- 5.1 The Technical Advice Note has been prepared in support of the Aberdeen Local Development Plan (LDP) (2012). The TAN provides a means for Officers to assess applications for the repair and replacement of windows and doors within Aberdeen. Technical Advice Notes do not carry the same weight as Supplementary Guidance but are still treated as material considerations in the decision making process. The TAN also provides general guidance and best practice advice for members of the public.
- 5.2 At present, two Technical Advice Notes contain advice on this subject, the 'Replacement Windows and Doors' and 'The Repair and Replacement of Timber Sash and Case Windows'. These two TANs have been merged to form this comprehensive Technical Advice Note on 'The Repair and Replacement of Windows and Doors'.
- 5.3 A previous version of the Technical Advice Note was subject to a public consultation exercise in late 2011 / early 2012. Comments on this previous version of the TAN were reviewed in early 2012. The revised draft of the TAN has been prepared to take into account the comments raised, and also takes into account changes to legislation and local planning policy guidance.
- With regards new legislation, the revised TAN has been prepared in the context of The Town and Country Planning (General Permitted Development) (Scotland) Amendment Order 2011 which came into force on 06 February 2012 and altered the permitted development rights of houses and flats with regards modifications to their properties, such as the replacement of windows and doors.
 - With regards new local guidance, the revised TAN has been prepared in the context of the Householder Development Guide which was formally adopted as Supplementary Guidance to the Aberdeen Local Development Plan on 27 August 2012.

- 5.4 The purpose of the TAN is to provide information on when planning permission and any other permissions (such as Listed Building Consent and/or Building Warrant) will be required with respect to the repair and replacement of windows and doors. The TAN is relevant to every residential property in the City, including flatted properties, and includes specific sections on Listed Building and properties within Conservation Areas.
- 5.5 Where any permission(s) are required, the TAN provides clear information on when the repair of windows and doors will be expected, and in which circumstances replacement may be acceptable. Where replacement is proposed, the TAN gives clear advice on what is deemed to be acceptable through both pictorial descriptions and text.
- 5.6 The progression of the TAN will provide a clear framework for decision making and allow comprehensive guidance for both applicants and officers. Implementation and enforcement of the TAN will ensure that there is a consistent approach relating to windows and doors within those areas where consent is required.

6. IMPACT

- 6.1 The Technical Advice Note contributes to “Aberdeen – The Smarter City” Vision by promoting Aberdeen as a great place to live and visit. It contributes to the Smarter Environment priorities by promoting energy efficiency measures and attractive street and townscapes.
- 6.2 The proposal also contributes to the following Single Outcome Agreement priorities:
1. We live in Scotland that is the most attractive place for doing business in Europe
 10. We live in well-designed, sustainable places where we are able to access the amenities and services we need;
 12. We value and enjoy our built and natural environment and protect and enhance it for future generations.
 14. We reduce the local and global environmental impact of our consumption and production.
 15. Our public services are high quality, continually improving, efficient and responsive to local people’s needs.
- 6.3 The proposal is consistent with the Council’s Corporate Plan in particular delivering high levels of design from all development and maintaining an up-to-date planning framework. The TAN also meets the vision of the Community Plan in promoting a strong image of the City and a sense of civic pride.

- 6.4 The proposal supports the Council's 5 year Business Plan in terms of protecting and enhancing our high quality natural and built environment.
- 6.5 The proposal is consistent with the Planning and Sustainable Development Service Plan, in particular promoting a customer focused service by engaging the community in the planning process.
- 6.6 An equalities and human rights impact assessment (EHRIA) has been carried out in relation to the proposed Technical Advice Note, with the results included as Appendix 2 to this report.
- 6.7 An assessment of the environmental impact of the Technical Advice Note was made alongside the Aberdeen Local Development Plan. Since the adoption of the Local Development Plan there has been additional consultation undertaken, additional detail added and amendments made. Therefore, this document will have to go through the Strategic Environmental Assessment Process. Given the assessment made at the Plan level, and the scope of the document, there would be no likely significant effect as a result of the Technical Advice. To ensure that this is properly documented a Screening Report has been prepared and submitted to the SEA Gateway and the Consultation Authorities. In the case that any unforeseen issues are raised this will be reported back to Committee.

7. BACKGROUND PAPERS

- Aberdeen Local Development Plan
<http://www.aberdeencity.gov.uk/nmsruntime/saveasdialog.asp?IID=42278&slD=9484>
- Appendix 1 – Technical Advice Note: The Repair and Replacement of Windows and Doors
- Appendix 2 – Equalities and Human Rights Impact Assessment

8. REPORT AUTHOR DETAILS

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Technical Advice Note

The Repair and Replacement of Windows and Doors



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aberdeen local development plan

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1. Introduction

This is one in a series of **Technical Advice Notes (TANs)** produced by Aberdeen City Council to help explain the Council's policies on various aspects of development in the City and who to contact for further information and advice.

This TAN addresses the **Repair and Replacement of Windows and Doors** and applies to all properties in the City. It:

- provides advice and guidance to residents and developers who are considering the repair or replacement of windows or doors;
- provides advice on whether any approvals are required before undertaking works (e.g. Planning Permission, Listed Building Consent, Building Warrant), and how to apply for these approvals;
- is a 'material consideration' and will assist Officers within Aberdeen City Council in determining applications;
- explains why windows, doors and doorways are important features of traditional buildings;
- outlines the Council's expectations for properties which are Listed Buildings and / or within a Conservation Area.;
- provides 'best practice' guidance on suitable replacement windows and doors where repair is not an option and,
- advises on measures to enhance the efficiency and security of older windows and doors without having to replace them.

A Further Reading list is provided at the rear of the guide (Section 11), along with a Glossary (Section 10) to explain terms which may not be familiar. The TAN does not include guidance on new dormer windows, as this is covered separately within the Householder Development Guide Supplementary Guidance, available at: www.aberdeencity.gov.uk/localdevelopmentplan.



Windows, doors and doorways can be important features of a building which are important to protect

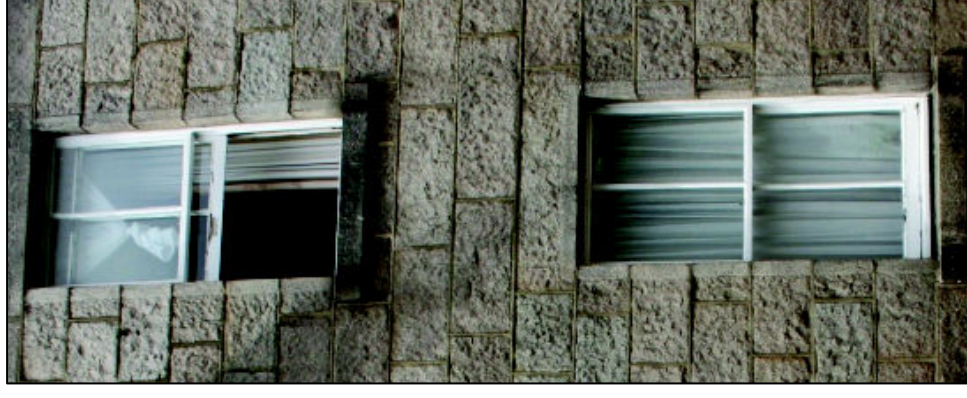
2. Thinking About Change

There are a number of reasons why owners might consider replacing their windows or doors, however the most common reasons are generally to do with concerns over heat loss, sound proofing, security and maintenance.

The general assumption is that these types of issues can only be resolved by completely replacing original windows and doors with something new – such as new ‘double glazing’. This is however often unnecessary, and can be extremely costly and unsustainable.

Below are a number of questions to consider before deciding whether replacing your windows or doors is right for you:

- Do my windows or doors actually need replacing or would it be more cost effective and sustainable to repair them?
- How old are my existing windows or doors? Are they of local or historic value or interest? Do they add value to my house?
- Are there upgrades which can be made to my existing windows to address heat loss, sound proofing and security?
- Can any parts of my existing windows or doors be recycled or reused if they cannot be repaired?
- Will new windows or doors really result in less maintenance?
- How long will it take for any savings in my heating costs to pay for the cost of new units, and how long will the new units last?



Repair of sash & case windows can often be much cheaper than replacement

3. Why Do We Need to Manage Change?

Windows, doors and doorways are distinctive features of a building which can often tell us a lot about local history and social change. In areas of Aberdeen where buildings have little decoration, traditional windows can make an important contribution to a building's architectural interest.

Changes to windows and doors which are well managed can make a positive contribution to the design and appearance of a building and to the quality and character of the surrounding area. Inappropriate changes can however have a significant negative impact on the character or appearance of a building which, when repeated over time, can dilute the appeal of wider areas.

For Listed Buildings and Conservation Areas inappropriate change can be particularly damaging, and may result in drops in property values. Unauthorised change may also lead to legal enforcement action.



Inappropriate change can negatively affect streets and wider areas as well as individual buildings

To check if your property is a Listed Building, visit: www.historic-scotland.gov.uk/historicandlistedbuildings.

To check if you live within a Conservation Area, visit: www.aberdeencity.gov.uk/masterplanning.

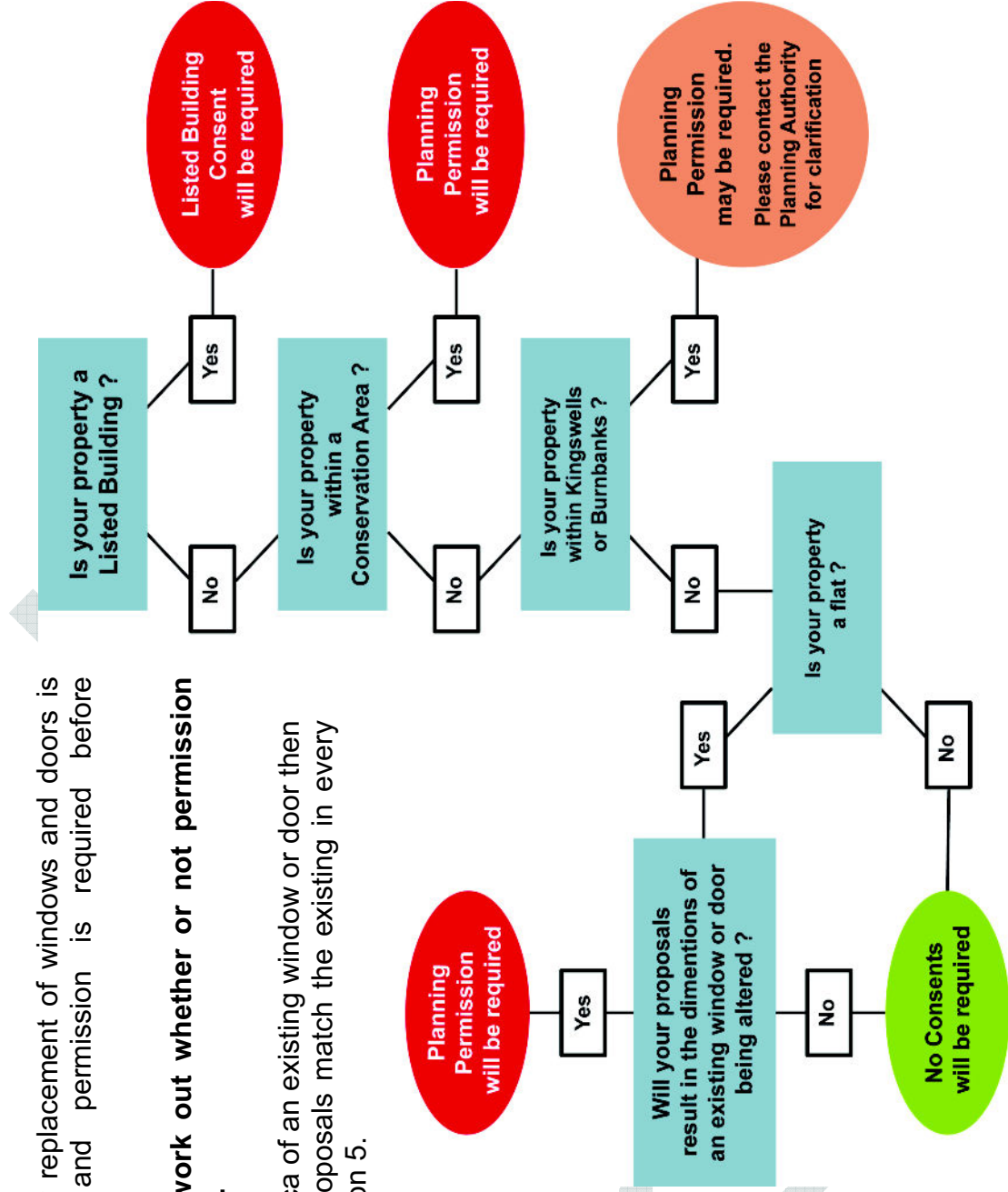
4. Do I Need Consent?

In some instances the amendment or replacement of windows and doors is controlled by planning legislation and permission is required before undertaking any works.

This diagram should help you to work out whether or not permission would be required for your proposal.

If your proposals are for an exact replica of an existing window or door then consent may not be required if the proposals match the existing in every way. This is discussed further in Section 5.

If you are unsure whether consent would be required you should always check with the Council's Development Management Section first, as unauthorised works can lead to costly enforcement action.



4.1 Building Regulations

Building Regulations exist to protect the public and for replacement windows and doors cover aspects such as ventilation, safety from collision, safe cleaning, prevention of falls, thermal performance and means of escape.

There is no need to obtain Building Warrant approval for replacing your windows or doors, but the work you undertake should meet the requirements of the Building Regulations, and the responsibility for compliance rests with the owner.

This guidance applies whether or not the new window or door is to be the same style and material as the existing.

Repairs to an existing window are slightly different and can be completed to return the window to the original standard without the need for the repaired window meeting current standards e.g. replacing the cill.

It is recognised that compliance with all Building Regulations may be difficult in buildings that have existing historical features or are Listed. Further advice on relaxations can be sought from the Building Standards Team at 01224 523470 or pi@aberdeencity.gov.uk.

Further guidance on Building Regulations can be found in Appendix 2.

5. Listed Buildings, Conservation Areas and Other Areas of Special Control

5.1 Listed Buildings

In Aberdeen, Listed Buildings range from grand villas on Queens Road, to tenement blocks on Rosemount Viaduct and fishermen's cottages at Footdee. Each Listed Building has its own character and style, be it individually, or as part of a wider group of buildings.

All Listed Buildings are protected, not just those in Conservation Areas, and all elevations of a Listed Building are protected, regardless of the Category of Listing.

Listed Building Consent will always be required for the alteration or replacement of windows or doors in Listed Buildings. In some instances repairs to traditional windows and doors using the original materials ("like for like") may not require Listed Building Consent. For proposals not to require consent they must include exact replication of the opening method and materials. For windows they must also include exact details of astragal dimensions and profiles, fixing of glass and the reuse of historic glass where this contributes to a building's character.

In considering applications for Listed Building Consent, the Council is required by law to: "*...have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses*" (Planning (Listed Buildings and Conservation Areas) (Scotland) Act 1997). If the Listed Building is situated in a Conservation Area, or is part of a flatted property, Planning Permission may also be required.

It is recommended that you check with the Council's Development Management section prior to undertaking any works to receive confirmation as to whether any consents would be required.



Building may exhibit a variety of window sizes and glazing patterns. This can provide important evidence of the history of the building and contribute to its character and interest

Original or historic windows, doors and doorways in Listed Buildings must be repaired and restored rather than replaced. Only in exceptional circumstances will replacement windows and doors be permitted, and these replacements must match the originals in terms of appearance and material as far as is reasonably possible. Consent to replace original windows or doors on Listed Buildings will only be given when it can be demonstrated to the satisfaction of the Council that they are beyond economic repair, and evidence of this will be required as part of any application. This evidence should take the form of a Professional Survey, further details on which are provided in Appendix 1.

5.2 Conservation Areas

Conservation Areas are areas of the City which have been designated by the Council for their special architectural or historic interest. Aberdeen has 11 Conservation Areas within the City Boundary, which include areas such as Old Aberdeen, Ferryhill and Rosemount / Westburn. Appraisals of Conservation Areas in Aberdeen are available at www.aberdeencity.gov.uk/masterplanning.

Even if your building is not Listed, if it is in a Conservation Area you will require Planning Permission to alter or replace windows or doors other than on a like for like basis, as described in Section 5.1. This requirement applies to both modern and older, traditional, properties.

In considering applications for Planning Permission in Conservation Areas, the Council is required by law to pay special attention “to the desirability of preserving or enhancing the character or appearance of that area”. (Planning (Listed Buildings and Conservation Areas) (Scotland) Act 1997). For Listed Buildings within Conservation Areas, Planning Permission will be required in addition to Listed Building Consent. For buildings which are located within Conservation Areas (but are not Listed Buildings), the Council’s preference is for original windows and doors to be repaired and restored wherever possible.



Details of the Conservation Areas in Aberdeen can be found on the Council's website, www.aberdeencity.gov.uk.

5.3 Other Areas of Special Control

Permitted development rights exist throughout the City so that small alterations to properties, such as replacing windows or doors, can be undertaken without the need for some permissions.

Burnbanks and Kingswells

In Burnbanks Village and Kingswells the City Council has taken the decision to remove these Permitted Development Rights via an Article 4 Direction, and so planning permission for replacing or amending windows / doors in these areas will be required.

The general preference will be for original or historic windows and doors in areas covered by an Article 4 Direction to be repaired and restored wherever possible. Where there is no original or historic fabric to be retained then any assessment of replacement proposals will principally be based on styling/type, materials and colouring.

Flats

If you live in a flatted property, planning permission will be required if you intend to alter the dimensions of an existing window or door opening.

6. Repair, Upgrade and Replacement

6.1 Retain and Repair

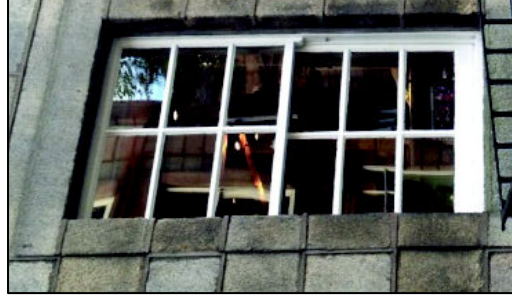
If you are considering new windows or doors, it is first worth checking whether replacing your existing windows/doors would be appropriate, or indeed necessary. Windows and doors can almost always be improved by being repaired, even if they are in very poor condition. Repair is also preferred in terms of sustainability as it reduces the requirement for new raw materials and energy.

Traditional Windows and Doors

The repair of original windows and doors is the best means of safeguarding the historic character of a traditional building, and in maintaining the visual character of wider areas. Repair will always be promoted over replacement, as new, modern units will very rarely be capable of matching the character and visual interest of the units to be replaced.

Traditional windows are nearly always timber sash and case and are found in a variety of forms that reflect changing glass technology and architectural fashion. Timber sash and case windows are a recognised feature of Aberdeen's traditional buildings, and are generally of a straightforward design and construction, meaning that they can usually be easily repaired.

Curved corner windows and rectangular "lying" panes of glass are features which are particularly special to Aberdeen must always be retained.



Examples of rectangular panes and curved glass

Traditional external doors and doorways are usually of solid timber frame construction with inset panelling retained by mouldings. Throughout the 18th and 19th centuries panelled doors became standard for the main entrance of most types of building. In Aberdeen this was typically reflected in 4, 5 and 6 panel varieties in a number of different configurations. A good joiner should be able to treat most faults with traditional doors, and again repair should be less expensive than replacement.

Traditional windows and doors are extremely durable and when properly maintained can last for centuries - many in Aberdeen are already well over 100 years old. In contrast, some modern windows can need replacing after just 20 years. Traditional windows and doors are generally made from good quality, durable, timber from mature trees which is of a much better quality than timber which is generally economically available today from sustainable sources. It therefore makes sense to retain and repair original windows, doors and doorways rather than to replace them with new timber which may not last as long and may be more susceptible to decay.

Original or historic windows, doors and doorways in Listed Buildings must be repaired and restored rather than replaced. Only in exceptional circumstances will replacement windows and doors be permitted, and these replacements must match the originals in terms of appearance and material as far as is reasonably possible. For buildings which are located within Conservation Areas, the Council's preference is also for original windows and doors to be repaired and restored wherever possible.

Original door ironmongery such as letterboxes, door knockers and handles should be retained and reused wherever possible, as should original window fittings such as cord clamps, sash lifts, sash fasteners and hooks.



4 panelled door with associated door ironmongery / furniture

6.2 Upgrading Traditional Windows and Doors

Replacing existing 'single glazed' timber windows and original timber doors with new 'double glazed' windows and uPVC / composite doors is often promoted as a means of fighting heat loss, draughts, dust ingress and providing improvements to sound insulation and security. Traditional windows and doors can however be upgraded at a lower cost to bring these same benefits, whilst still retaining the original features and style which gives character to your building.

Energy Efficiency

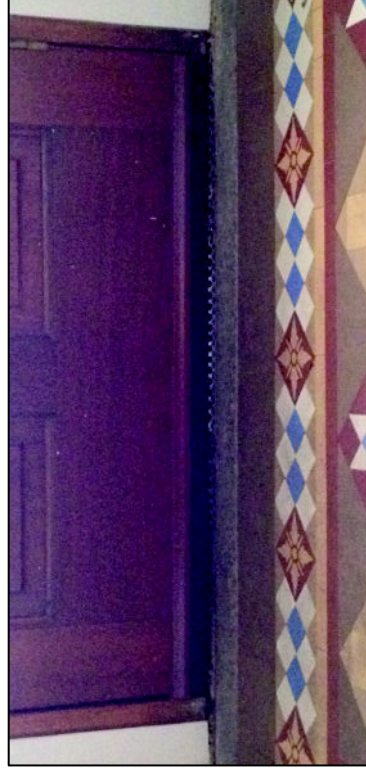
By the nature of their construction many older buildings are prone to heat loss through cracks and gaps which develop as building elements move and settle over time. Although this natural settlement helps to properly ventilate the building, often these draughts result in heat loss which can be uncomfortable for occupiers.

Draughts around older windows and doors (including key holes and letter boxes) can be a source of air leakage, and **draught-proofing** is one of the best ways to improve comfort and reduce energy use, with little or no change to a building's appearance.

Draught-proofing simply means blocking up any unwanted gaps which let cold air in and warm air out. Keeping warm air in the building means less energy spent heating it, therefore saving you money. A number of draught-proofing measures are widely available from DIY stores and can be fitted relatively cheaply.

Draught-proofing can also improve noise insulation, reduce dust ingress and make sash and case windows easier to slide up and down. Curtains lined with a layer of heavy material can also help reduce heat loss from a room and cut draughts.

Traditional timber doors are generally very effective in retaining warm air within a building, however additional insulation material can be added to the panels on the indoor side of the door to enhance the effect and fight additional heat loss whilst still maintaining the character of the door from the outside.



Draught-proofing strips can be easily applied to interior side of external door

Where installing draught-proofing to traditional windows is difficult, **secondary glazing** systems can be a good alternative. Secondary glazing consists of an additional pane of glass fitted on the inside of the existing window frame which can be removed during summer months and for cleaning. Providing similar insulation values to double glazing, it can also reduce dust ingress, provide good noise insulation, and be an extra security measure.

Secondary glazing design should seek to be as discreet as possible, particularly in Listed Buildings, with small frames concealed from view. Meeting rails and frames should be as small in section as possible to allow them to be disguised behind existing rails. If secondary glazing is installed, the original windows should not be draught-proofed to help avoid condensation.

For more information about improving energy efficiency in traditional properties a number of Historic Scotland's publications are included in the Further Reading List at the end of this document (Section 11).

Security

To improve the security of traditional sash and case windows additional sash locks can easily be fitted to the meeting rails to improve the security of the window when closed. Timber blocks and / or sash stops can also be fitted to restrict the size of openings.

Additional security measures on doors can also be easily incorporated without affecting the character of a door, for example extra mortice locks, rimlocks or bolts.



Secondary glazing should seek to be as discreet as possible

6.3 Replacing Windows and Doors

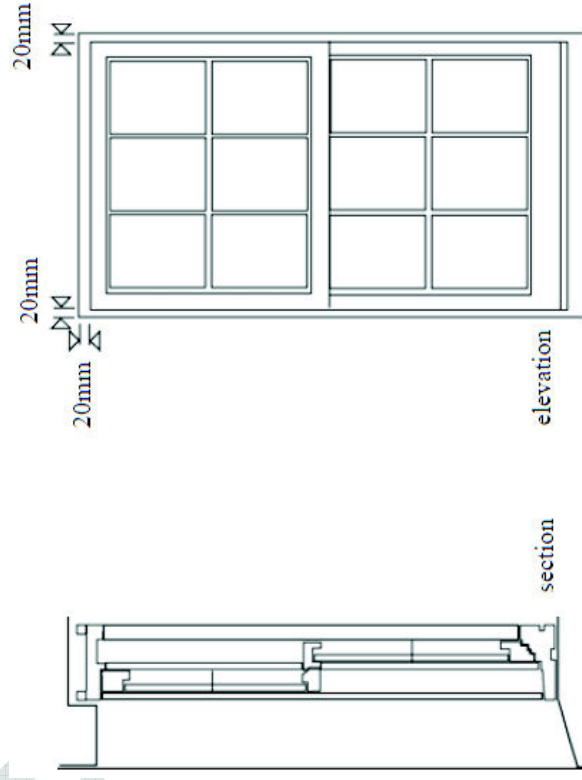
The most appropriate windows and doors for a property are likely to be those which were originally designed for it. Instances will however exist where original features will have deteriorated to such a state that replacement is the only viable option. Complete replacement of traditional windows and doors should however only be contemplated where the features to be replaced have deteriorated beyond economic repair, i.e. it would be unviable to repair and replacement is the only option remaining. The following principles are especially relevant where Planning Permission or Listed Building Consent is required, but should also be considered as 'best practice' for all properties throughout the City, even when consent is not required.

Windows

Where there is no alternative to replacement, new windows should be sensitively replaced in an environmentally sensitive way which is in keeping with the character of the original building and the quality of its design.

The original proportions of window openings should be retained to ensure the architectural integrity of the building is not compromised. Where any original glazing survives, every effort should be made to retain or to salvage as much as possible for re-use.

The relative proportions of the upper and lower sashes of new windows in Listed Buildings must be the same as in the original windows, as should the size of timber sections. Replacement windows should be fitted in the same plane, be made up from timber sections of the same profile and dimensions, and have the meeting rails in the same position. Generally, when the window is fully closed, the visible part of the sash stiles, top rail and meeting rails should be of a similar width/ depth. Where there is no existing original model on which to base a replica, bottom rails should be at least 75mm deep and 25mm deeper than the meeting rails, on elevation. No more than 20-25mm of the sash box should be visible in the window opening, the remainder being concealed behind the masonry window check.



Sash & case window illustration

Factory made standard windows, whether in timber, aluminium, galvanised steel, plastic (uPVC) or a composite are almost always damaging to the character and appearance of historic buildings and will not be appropriate for Listed Buildings. In Listed Buildings joiner-made timber replacements will generally be the only type of replacement window which will be accepted.

Ventilators cut through the glass or visible on the window frames will not be considered acceptable on Listed Buildings or on public elevations in Conservation Areas. Ventilators, where required, should be located unobtrusively in the meeting rail. Where trickle ventilation is required other more discreet means of achieving this should always be considered. Historic Scotland's publication "Sash & Case Windows: A Guide for Homeowners" provides further guidance on ventilation.

Consent for replacement windows on Listed Buildings or public elevations in a Conservation Area which reproduce the astragal pattern but open in a different manner will always be refused, as will consent for those where the astragals are merely applied to the surface of, or are sandwiched between, the panes of double glazing. Where astragals are required, they must be kept slender to match the thickness of the original astragals, particularly in multiple pane sashes.

Where glazing beads are required they should be wedge shaped to match a putty fillet, and taper from 10mm at the glass to less than 2mm at the outside. The edge of the bead must be flush with, or kept slightly back from, the face of the sash. It must never project out from the face of the sash, or an unacceptable shadow line will be created around each pane of glass.

The dimensions of replacement window astragals should use original sash windows as the model. Typically astragals may only be 17-19mm wide. In Listed Buildings, where the interior of the building can often be as important as the exterior, the internal profile of the astragals must not be over-simplified, but should reproduce traditional moulding appropriate to the period and detail of the building. Astragals must carry through the sash to completely separate each pane of glass.



Discreet means of achieving ventilation will be required for Listed Buildings and on public elevations in Conservation Areas

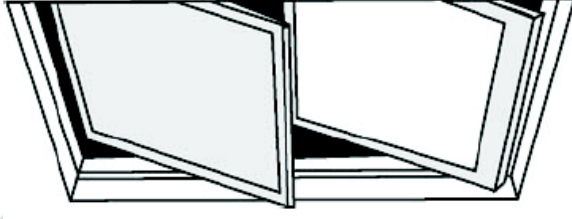
Glazing in sash windows on Listed Buildings should preferably be retained in position with traditional putty or modern butyl based putty, which is always preferred to timber beads.

In both Listed Buildings and Conservation Areas there will be a strong presumption in favour of retaining stained or decorative leaded glass and etched glass in replacement proposals. Proposals to use wired glass, obscured glass, louvered glass or extract fans in windows on Listed Buildings or on public elevations in Conservation Areas will not generally be considered acceptable.

On traditional buildings in Conservation Areas, modern window designs will generally be inappropriate on elevations of the building which are visible from public areas.

Sash and case “lookalike” windows, which closely match the detailing and appearance of a traditional sash and case window, but may employ a different opening mechanism, may be acceptable in Conservation Areas.

“Lookalike” windows will normally be formed in timber and will have upper and lower sashes of the same size as those in the window they are to replace. It is of vital importance that the upper sash is stepped out in front of the lower sash in profile, with the meeting rails fully overlapping as seen in elevation, such that the window when closed, is virtually indistinguishable from a traditional sash and case window. White uPVC vertical sliding windows may be acceptable as ‘lookalike’ replacements for windows in Conservation Areas provided that proposals with through / embedded astragals comply with original dimensions. No planted astragals will be accepted on elevations in Conservation Areas which are visible from public areas.



Acceptable type of 'lookalike' in a Conservation Area



Not acceptable as a 'lookalike' in a Conservation Area

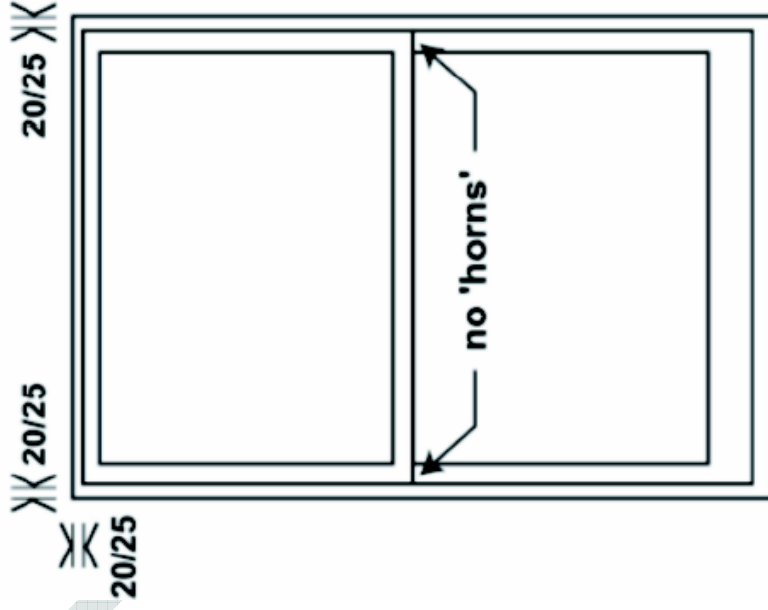
Where uPVC “lookalike” windows are to be used they should fully replicate the significant features of timber sash and case windows. Such features would include:

- a. no more than 25mm of the outer window frame should be visible at the top and sides, once the window has been fitted into the masonry opening.
- b. the meeting rails must fully overlap.
- c. the bottom rail of the lower sash must be at least 75mm high.
- d. the glass must be recessed from the front face of the sash by at least 10mm..

The Council may choose to be more flexible when considering applications for replacement windows and doors in Conservation Areas which are not visible from the street or are only visible from a private court.

On non-traditional, more modern, buildings in Conservation Areas a broader range of materials and designs may be permitted, depending on the individual building and surrounding area.

Elsewhere in the country most mid/late Victorian windows have ‘horns’ however this is not the case in Aberdeen. Along with original Georgian and early Victorian windows, most nineteenth century windows in Aberdeen do not have horns, and neither should the windows which replace them. The use of horns should only be contemplated only where there is clear evidence that they existed on the original windows, and in such instances the design of the horns should match the original.



Elevation of sash & case 'lookalike' windows with no 'horns'

Doors

Where there is no alternative to the replacement of an original door, new elements must match the original as far as possible when the property is a Listed Building or is visible from a public area within a Conservation Area. The new door should match the original in terms of proportion, profile and material, and reuse historic glass where this contributes to a building's character. If the property forms part of a group of uniform design, then any replacement should make reference to those of the neighbouring properties in style, design and size. Joiner-made replicas will be strongly encouraged in Conservation Areas and will be a requirement for Listed Buildings.

Composite door solutions may be appropriate on public elevations in Conservation Areas depending on the specified design proposed. Composite, aluminium and uPVC solutions will never be acceptable on Listed Buildings. Only on non-public elevations in Conservation Areas will uPVC, aluminium or doors from DIY chain stores be acceptable. Doors from DIY chain stores and flush plywood doors with mouldings applied to resemble panelling will not be acceptable substitutes on Listed Buildings or on public elevations in Conservation Areas. Replacement doors with a stained or varnished finish, and those which introduce asymmetrical elements, integral fanlights, inappropriate glazing or panelled patterns, will also be rejected.

The original proportions of doorways and door openings on street frontages must always be retained, and proposals to recess a door either less or more deeply within the door opening will not be supported. Original door ironmongery such as letterboxes, door knockers and handles should be retained and reused if the timber door is being replaced. Where this does not survive, the replacement of modern fittings with items appropriate to the period of the building will be encouraged.

Where the opportunity exists, modern doors which are badly proportioned, or of the wrong type or material, should be replaced with a more appropriate solution.



Historic glass should be reused where this contributes to the character of a building

Colour Palette

As white is the colour of most existing sash and case windows in Aberdeen, this is the colour which new windows will generally require to be in order to encourage uniformity.

This will be particularly important in tenements where the use of a uniform window colour helps give the building an architectural cohesion it may otherwise lack.

Other colours will only be agreed to in exceptional circumstances.

Doors should be painted in an appropriate dark and muted colour, and bright glosses and white paint avoided.



Replacement windows will generally require to be white

7 Inserting New and Blocking Up Old

Applications to insert additional windows in Listed Buildings or in Conservation Areas may be permitted where this does not detract from the character of the building or area.

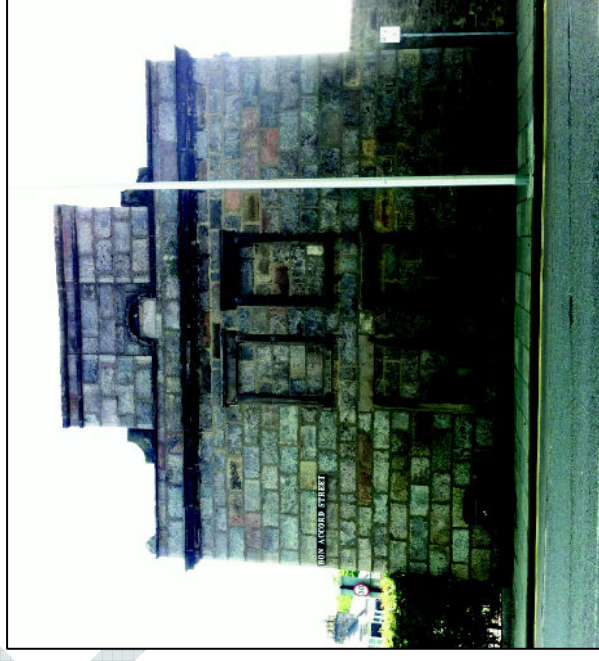
Location and design are the key considerations for new window openings which should be assessed in the context of the rest of the building and surrounding properties. The blocking up, in whole or in part, of original openings should only occur where the window makes little contribution to the building or area.

In principle the blocking up should be done in materials that relate to the surrounding building and evidence of the opening, such as window surrounds, be retained.

Proposals to increase the glazing area by removing stone or timber mullions which form the divisions in bipartite or tripartite windows will not be supported on Listed Buildings or in Conservation Areas.

The re-opening of blocked windows will be encouraged only where this will reinstate the intended elevational treatment of the building. It will not be permitted in cases where blocking up has taken place during later alterations to the structure and where the earlier window openings consequently no longer relate to present elevational form.

Doors in street frontages, even though no longer used, should always be retained



Example of blocked up windows in Ferryhill

8. Summary Charts - What Could Be Acceptable Where?

The following charts have been produced for general guidance purposes only. Proposals will still require to demonstrate satisfactory detailed designs and implementation methods. The charts should only be read in the context of the advice provided within the rest of this TAN. Any queries should be directed to the Council's Development Management Section.

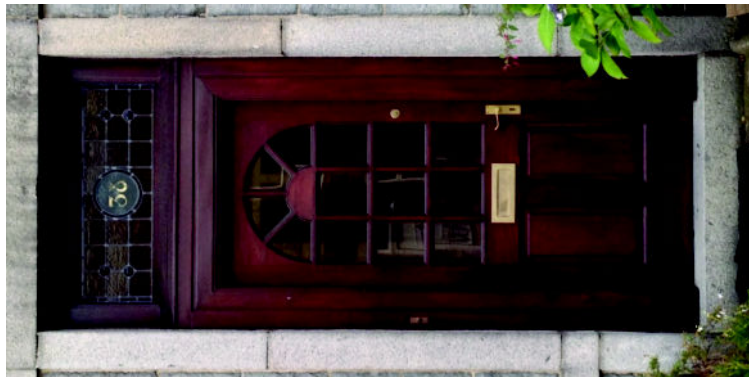
Windows

	Listed Buildings	Conservation Areas: Public Elevations	Conservation Areas: Non-Public Elevations
Joiner made timber sash and case	✓	✓	✓
Retaining historic or original stained / decorative leaded glass and etched glass	✓	✓	✓
uPVC sash and case 'lookalikes'	X	✓	✓
Through astragals	X	✓	✓
Planted astragals	X	X	✓

Doors and Doorways

	Listed Buildings	Conservation Areas: Public Elevations	Conservation Areas: Non-Public Elevations
Joiner made timber panelled door	✓	✓	✓
Composite Door	X	✓	✓
uPVC / aluminium / DIY chain store door	X	X	✓
Flush (e.g. plywood) doors with mouldings applied	X	X	✓

Examples of window and door designs to avoid on Listed Buildings and on public elevations in Conservation Areas



9. What To Do Next

Applications for Planning Permission and/or Listed Building Consent can be made online via the Scottish Government's e-planning website: www.eplanning.scotland.gov.uk

Or direct to Aberdeen City Council using the application forms available on our website: www.aberdeencity.gov.uk/planning_environment

For applications for Planning Permission an application fee will be required. There is no charge for applications for Listed Building Consent, however a fee may be requested to advertise the application in the local press.

Failure to obtain appropriate consents and permissions could result in enforcement action being taken to have unauthorised windows or doors replaced, which may entail considerable costs for the building owner. Unauthorised windows and other works are often identified by conveyancing solicitors and can affect property sales.

In the case of a Listed Building unauthorised work falls under criminal law, and so the person who executes the works, or causes them to be executed, is liable to prosecution or imprisonment in addition to standard enforcement proceedings.

Submission Requirements

Two (2.no) copies of the following scaled drawings will be required for applications for Planning Permission and three (3.no) for Listed Building Consent in addition to the relevant application forms:

1. An OS Plan which clearly identifies the location of the site
2. Plans and detailed elevations of both existing and proposed works.
3. Horizontal and sectional drawings with details at a larger appropriate scale.
4. Details of proposed materials and style of opening.
5. Any other relevant technical information and photographs

In addition, for applications for the replacement of windows/doors on a Listed Building or within a Conservation Area, the following will also be required:

6. A clear statement which sets out a justification for the proposals, including why the works are desirable or necessary.
7. Detailed justification (e.g. a professional survey – for more information refer Appendix 1) to demonstrate to the satisfaction of the Council that the windows/doors to be replaced are beyond economic repair. Within Conservation Areas this will only be required for proposals affecting front elevations or elevations which are visible from public areas.
8. Drawings will normally be required at a scale of 1:1 or 1:2 and should include sections through window head, meeting rails, bottom rail and cill, window jambs and astragals. In addition, an elevation of the window should be provided showing the position of the meeting rails and the arrangement of any astragals.

Sources of Assistance

If your property is a Listed Building or situated within a Conservation Area then you may wish to investigate whether any sources of funding assistance exist:

The Aberdeen City Heritage Trust is a limited company and charity that operates within Aberdeen City. The Trust has its own criteria for determining in what circumstances it may be prepared to offer assistance. Further details can be found on its website www.aberdeenheritage.org.uk or by calling 01224 522755

Historic Scotland also administers a programme of building repair grants throughout Scotland for Listed Buildings of outstanding architectural or historic importance, or key buildings within Conservation Areas. More information is available at www.historic-scotland.gov.uk/grants

10. Glossary

Article 4 Direction: Some types of development do not need planning permission by virtue of permitted development rights. An Article 4 Direction is an order made by Scottish Ministers which suspends (for specified types of development) the general permission granted under the Town and Country (General Permitted Development) (Scotland) Order 1992 (as amended), thereby removing permitted development rights. Article 4 Directions are currently in place in Burnbanks and parts of Kingswells.

Astragal: A glazing bar separating panes of glass within a window.

Conservation Area: Conservation Areas are areas of special architectural or historical interest, the character or appearance of which it is desirable to preserve or enhance. Such areas are designated by the local planning authority. Details of the Conservation Areas in Aberdeen can be found via www.aberdeencity.gov.uk/masterplanning.

Dwellinghouse: For the purposes of this guidance, the term “dwellinghouse” does not include a building containing one or more flats, or a flat contained within such a building

Fenestration: The arrangement / pattern of the windows in a building.

Flat: A separate and self contained residence, which is one of several within a larger building.

Lintel: A structural beam above an opening, such as a window or door,

Listed Building: Working on behalf of Scottish Ministers, Historic Scotland inspectors identify buildings which are worthy of statutory protection. These are ‘Listed Buildings’. The criteria by which the Scottish Ministers define the necessary quality and character under the relevant legislation are broadly; Age and Rarity; Architectural Interest; and Close Historical Association A list of listed building is available from Historic Scotland www.historic-scotland.gov.uk/historicandlistedbuildings.

Listed Building Consent: Although the listing of a building should not be seen as a bar to all future change, strict controls do exist to ensure that works undertaken to Listed Buildings are appropriate. Listed Building Consent is obtained through an application process which is separate from, but runs parallel to, applications for planning permission. This separate regulatory mechanism allows planning authorities to ensure that changes to listed buildings are appropriate and sympathetic to the character of the building.

Material Consideration: Any issue which relates to the use and development of land and is relevant to the planning process.

Mullion: A vertical piece of stone or timber dividing a window into sections.

Permitted Development: A term used for certain types of development which, by satisfying specified conditions, is automatically granted planning permission without the submission of an application to the planning authority. These Permitted Development Rights are removed for Listed Buildings, Conservation Areas, Article 4 areas, and flats.

Planning Authority: The term given to the Council in its role exercising statutory functions under Planning legislation. Authorities have three main planning duties: Development Management (assessing and determining planning applications); Development Planning (preparing, updating and monitoring the authority's Local Plan/Local Development Plan); and Enforcement (seeking to investigate and resolve breaches of planning control)

Sash and Case Window: A window that slides vertically on a system of cords and balanced weights.

Terrace House: A dwellinghouse situated in a row of three or more buildings

Transom: A horizontal glazing bar in a window.

11. Further Reading

- **Aberdeen Local Development Plan**
www.aberdeencity.gov.uk/localdevelopmentplan
- **Conservation Area Appraisals**
www.aberdeencity.gov.uk/masterplanning
- **Historic Scotland - Energy Efficiency Advice**
<http://www.historic-scotland.gov.uk/index/heritage/technicalconservation/energyefficiencyadvice.htm>
- **Historic Scotland - Managing Change in the Historic Environment: Doorways**
<http://www.historic-scotland.gov.uk/index/heritage/policy/managingchange.htm>
- **Historic Scotland - Managing Change in the Historic Environment: Windows**
<http://www.historic-scotland.gov.uk/index/heritage/policy/managingchange.htm>
- **Historic Scotland – Sash & Case Windows: A Short Guide for Homeowners**
<http://www.historic-scotland.gov.uk/maintaining-your-home.pdf>
- **Scottish Historic Environment Policy**
www.historic-scotland.gov.uk/index/heritage/policy/shep.htm

Appendix 1: Professional Survey

National planning policy only allows windows in Listed Buildings to be replaced where there is evidence that they cannot be repaired. It also requires that applications for Listed Building Consent are supported by sufficient information to enable the local planning authority to determine them. This is why we ask for a survey to demonstrate that existing window(s) are beyond repair and need to be replaced. This is in addition to the elevational drawings and sections required. Please note that comprehensive window replacement is unlikely to be justified in all but exceptional circumstances.

What should be in a survey?

The survey should be carried out by a joiner or similar and include each individual window. It should include an image of the whole elevation with the windows numbered and an accompanying proportionate amount of information about the type, age and condition of each window. For a straightforward residential property this could be an annotated photo with the windows numbered and a short description of the window condition. Additional information may be required for a more complex building or where significant historic windows are involved

Appendix 2: Building Control Information

Ventilation

Every apartment must have a window(s) or door(s) with an opening area of at least 1/30th of the floor area of that room. Where the opening area of the existing window is already less than 1/30th of the floor, the opening area of the replacement window must not be less than the original size. If the existing window had an integral trickle ventilator the replacement window must also have an integral trickle ventilator of at least the same size as the original.

Safety from Collision

Windows must not open over footpaths or any place to which the public has access, where they could form a hazard or obstruction.

Safety Glass

Glazing which is less than 800 mm above the floor must be toughened or laminated. Alternatively, a protective barrier could be installed to prevent collision.

Thermal Insulation

Windows must have a U-value (thermal insulation rating) of not more than 1.8 W/m²K. There are many types of window construction which meet the required degree of thermal insulation and this should be confirmed with the window installer.

Protective Barriers

Openable windows to a storey with a floor level of at least 600 mm above the outside ground level, and with a cill height less than 800 mm above floor level, must be provided with a protective barrier to minimise the risk of falling from the window. On the ground and first storeys, the barrier must be not less than 800 mm above the floor level. There must not be any gaps in the barrier which would allow a 100 mm ball to pass through. Alternatively, permanently fixed safety glass may be used instead of a barrier.

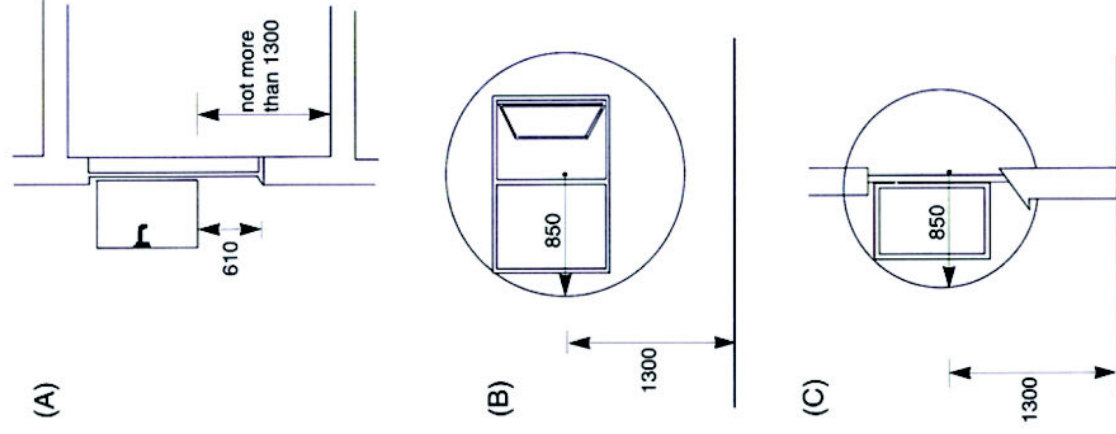
Emergency Escape Windows

An escape window must be provided in every apartment on the first floor of a dwelling. Escape windows must also be provided in rooms on the ground storey where the escape is through another room. Escape windows must have an unobstructed openable area that is at least 0.33 m² and at least 450 mm high and 450 mm wide. The bottom of the openable area must not be more than 1100 mm above the floor except in cases where the existing cill height is greater than 1100 mm.

Safe Cleaning

All windows must be safely cleanable. Windows, all or part of which are more than 4 m above the adjacent ground level must be designed so that any external and internal glazed surfaces can be cleaned safely from inside the building. These windows must be cleanable whilst standing on the floor and without over-reaching. The maximum reach must not exceed that shown in the diagrams over, and the maximum upwards reach is 2150 mm. If parts of the existing window are out with the maximum reach, the replacement window should be of a type which allows those parts to be brought within the safe reach in order to allow safe cleaning.

All dimensions are in mm.



(A) downwards reach through an opening light

(B) side reach through an opening light

(C) reach for cleaning an open window with easy-clean hinges

FEEDBACK

For Further Information Please Contact:

Planning and Sustainable Development
Enterprise, Planning & Infrastructure
Aberdeen City Council
Business Hub 4
Ground Floor North
Marischal College
Broad Street
Aberdeen
AB10 1AB

Telephone: 01224 523470

Fax: 01224 523180

Email: pi@aberdeencity.gov.uk

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Aberdeen City and Shire Strategic Development Plan
REPORT NUMBER:	EPI/12/234

1. PURPOSE OF REPORT

- 1.1 The purpose of this report is to present a draft of the Proposed Strategic Development Plan (SDP) to the Committee for information prior to its consideration by the Strategic Development Planning Authority (SDPA) on 9 November 2012.
- 1.2 After consideration and agreement by the SDPA, the Proposed Plan will be referred to both Aberdeen City Council and Aberdeenshire Council for ratification. It is anticipated that this will be at the Council meeting on 19 December 2012. There will then follow a period for representations before submission to Scottish Ministers.

2. RECOMMENDATION(S)

- 2.1 It is recommended that the Committee:
 - (a) Note the contents of the draft Proposed Strategic Development Plan (Appendix 1);
 - (b) Note the contents of the draft Minute of the Strategic Development Planning Authority on 21 September 2012 (Appendix 2);
 - (c) Agree to attach the Minute of this Committee to the report to Council following approval of the draft Proposed Plan by the SDPA.

3. FINANCIAL IMPLICATIONS

- 3.1 The financial implications arising from this report are met from within the SDPA's budget.

4. OTHER IMPLICATIONS

- 4.1 This is a significant piece of work that has involved and will impact upon many other council services, public bodies, the business and development industries and the citizens of Aberdeen.

5. BACKGROUND/MAIN ISSUES

- 5.1 The Aberdeen City and Shire Structure Plan was approved by Scottish Ministers in August 2009. It was the last structure plan to be approved in Scotland before the Planning etc (Scotland) Act 2006 came into force, introducing a requirement to prepare SDPs in the four largest city regions (including in Aberdeen and Aberdeenshire).
- 5.2 Both councils have now adopted local development plans consistent with the structure plan. This was a significant achievement given the scale of change required from the previous local plans.
- 5.3 Although not applying directly to the first SDP, there is a statutory requirement for future plans to be submitted to Scottish Ministers within four years of the approval of the previous plan. The current and previous development plan schemes have presented plan preparation timetables which set this as a target for the first strategic development plan. This means that submission is required to Scottish Ministers by 17 August 2013.
- 5.4 The SDPA started work on the preparation of a replacement plan in late 2010. This has involved the preparation of a range of studies and assessments as well as monitoring of the current structure plan and engagement with stakeholders and agencies. The SDPA approved a Main Issues Report at its meeting of 23 September 2011 and consultation took place between October 2011 and January 2012, with 85 responses received.
- 5.5 All members of both councils have been invited to two informal seminar sessions during the preparation of the plan to date. The first took place on 10 June 2011 on the proposed content of the Main Issues Report while the second took place on 10 September 2012 on the content of the Proposed Plan itself.
- 5.6 An early draft of the Proposed Plan was shared with the Key Agencies for comment and their feedback was useful in informing the final draft attached.

Proposed Plan

- 5.7 The preparation of a Proposed Plan is a key stage in the preparation of the SDP. The proposed plan builds on the monitoring of the previous plan, engagement with stakeholders and Key Agencies, responses to

the Main Issues Report and the various studies and assessments prepared in support of the plan.

- 5.8 The Proposed Plan presented in Appendix 1 represents a significant level of continuity with the current structure plan. This was the view clearly expressed by the majority of stakeholders, Key Agencies and those responding to the Main Issues Report.
- 5.9 The plan continues to present an ambitious strategy for growth for the area while also focusing regeneration activity in those parts of the area with particular challenges. A common view throughout engagement and consultation was that the focus should be on implementation rather than making changes to a plan which was widely welcomed and still only a few years old.
- 5.10 As identified in the Main Issues Report, while by no means immune from the consequences of the global economic turmoil since 2007/08, Aberdeen City and Shire has been one of the most resilient parts of the UK. Aberdeen City and Shire is consistently identified as one area in Scotland and the UK which has the potential to drive economic recovery. It is important that the SDP supports this and helps deliver on this opportunity.
- 5.11 Housing and employment allowances up to at least 2026 have already resulted in allocations being made by the two Local Development Plans so there is already a clearly defined pattern of growth. The focus is therefore rightfully on the implementation of these plans.
- 5.12 As indicated in the Main Issues Report, the opportunity has been taken to expand the 'Spatial Strategy' section of the plan and at the same time include more diagrams to explain the effect of the plan on different areas. At the same time the text retains the Crystal Mark from the Plain English Campaign for its clarity.
- 5.13 The main changes from the current structure plan relate to five additional proposals and the policy positions around waste management and the strategic transport fund. These are addressed below after a consideration of the housing land supply position.

Housing Land Supply

- 5.14 While housing completions in Aberdeen City and Shire have declined since the start of the global economic turmoil in 2007/08, they have been more resilient than elsewhere in Scotland. The 2012 Audit indicates that completions are likely to rise again over the next few years as the new LDP allocations are developed. The Proposed SDP does not require additional housing or employment land to be identified in the subsequent LDPs for the period to 2026.
- 5.15 The housing requirement and allowances in the Proposed Plan are based on the outputs of the 2011 Housing Need and Demand

Assessment (HNDA). Recent projections prepared by the National Records of Scotland (formerly the General Register Office for Scotland) suggest a significantly higher rate of population and household growth than was used as the basis for the HNDA. The Housing Market Partnership met earlier this year and recommended that the HNDA continue to be used as the basis for the SDP. The Proposed SDP continues to provide a generous supply of housing and employment land and is robust enough to accommodate higher levels of growth such as those projected by the National Records of Scotland in the medium-term (i.e. to 2026 at least and probably well into the final phase of the plan).

- 5.16 Monitoring of population and household growth as well as the rate of new house building will be important on an annual basis to inform the review of the Housing Need and Demand Assessment and the SDP in five years' time to ensure that the SDP continues to provide a robust plan for the future.

Waste Management

- 5.17 The publication of Scotland's Zero Waste Plan in 2010 set a different context for planning for waste management to that contained in the current structure plan. This was anticipated in the structure plan which stated that the new framework would be reflected in the first SDP.
- 5.18 The urgency and significant of the change to a more sustainable approach to waste management expressed in the structure plan remains. This plan seeks to facilitate the development of the required infrastructure, viewing waste as an important resource.
- 5.19 The MIR presented a series of options to deal with the area's waste and the preferred option has largely been carried forward into the proposed SDP. The proposed SDP emphasises the objective of meeting our waste management needs locally as far as possible, in line with the waste hierarchy, the proximity principle and the Zero Waste Plan.
- 5.20 While we currently have sufficient landfill capacity, there is a need for at least an additional 300,000 tonnes of capacity in new waste management facilities in the area. These are likely to be facilities such as material recycling and composting plants as well as energy from waste facilities. The plan directs most of this capacity to the strategic growth areas within the Aberdeen Housing Market Area to reduce the environmental and financial costs of transporting waste and maximise the opportunity for using the heat generated by such proposals. Given the nature of modern waste management facilities, sites identified for employment purposes in the two local development plans are likely to be potentially suitable, along with sites adjacent to existing waste management facilities. Sites outwith the strategic growth areas may also be suitable, particularly where they meet a more local need or require a rural location.

Strategic Transport Fund

- 5.21 The SDPA approved Supplementary Planning Guidance on a Strategic Transport Fund (STF) in December 2011 and this was subsequently ratified by both councils in early 2012.
- 5.22 Financial contributions from development have already been received and a number of planning agreements have been signed which require a contribution to be made at a future date. Most of the contributions to the fund will come from new sites allocated in the two recently approved local development plans so it can be expected that the number of planning agreements with STF contributions will rise significantly as sites come forward for development over the next year or two.
- 5.23 Engagement with the development industry is due to take place over the next few months around the timing of implementation of the individual interventions. This will also seek views on the implementation of the guidance and how it can be improved. A review of the guidance will be carried out over the next six to twelve months with reports to future meetings of the SDPA. Consultation will be carried out as part of this process and enable the guidance to be adopted as statutory supplementary guidance alongside the approval of the SDP. The existing guidance will continue in force until that time.

Proposals

- 5.24 All of the proposals contained in the current structure plan have been carried forward to the Proposed SDP with one exception. The proposal for a new prison at Peterhead has been removed as construction is well underway. All other proposals are still awaiting implementation and there is value in retaining the plan's support for them until that stage has been reached.
- 5.25 A number of additional proposals have been included in the Proposed SDP. Most of these were highlighted in the MIR, although the Scottish Government's Infrastructure Investment Plan gave a commitment to dualling the A96 north of Inverurie and this has also been included.

The new proposals include:

- Upgrading the A96 to dual carriageway north of Inverurie;
- Carbon capture and storage infrastructure at Peterhead as well as infrastructure for the transport of CO₂ to St Fergus from outwith the area (either by pipeline or ship);
- Electricity transmission infrastructure to the south of Peterhead including an extension to the electricity substation and converter stations required for offshore transmission (subject to a development framework for the area);

- Proposals to realise the potential of the ports at Aberdeen and Peterhead, particularly in the context of the National Renewables Infrastructure Plan; and
- Strategic transport interventions required to address the cumulative impact of new development in and around Aberdeen. These interventions were identified and will be funded through the Strategic Transport Fund.

6. IMPACT

- 6.1 The Strategic Development Plan contributes to the following Single Outcome Priorities:
 1 – We live in a Scotland that is the most attractive place for doing business in Europe; 2. We realise our full economic potential with more and better employment opportunities for our people; 10. We live in well designed, sustainable places where we are able to access the amenities and services we need; 12 – We value and enjoy our built and natural environment and protect and enhance it for future generations.
- 6.2 An Equality and Human Rights Impact Assessment was carried out on the Proposed Plan by the SDPA and is attached for information (Appendix 3).

7. BACKGROUND PAPERS

Appendix 1 - Aberdeen City and Shire Strategic Development Plan: Proposed Plan

<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=982&sID=556>
 Diagrams

<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=983&sID=556>

Appendix 2 – Draft SDPA Minute 21 September 2012
 Attached at end of this report.

Appendix 3 - Proposed Plan - EHRIA
<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=988&sID=556>

8. REPORT AUTHOR DETAILS

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 01224 523133

Appendix 2

DRAFT/

STRATEGIC DEVELOPMENT PLANNING AUTHORITY

ABERDEEN, 21 September, 2012 – minute of meeting of the STRATEGIC DEVELOPMENT PLANNING AUTHORITY. Present: Councillor Milne (Chairperson), Councillor Argyle (Vice Convener); and Councillors Cormie (as substitute for Councillor Corall), Donnelly (as substitute for Councillor Thomson), Graham (as substitute for Councillor Grant) and Taylor (as substitute for Councillor Finlayson) (Aberdeen City Council), Bellarby, Cox, Merson, Norman Smith and Topping (Aberdeenshire Council).

The agenda and reports associated with this minute can be located at the following link:

http://www.aberdeencityandshire-sdpa.gov.uk/AboutUs/Agendas/2012_09_21_Agenda.asp

APPOINTMENT OF CHAIRPERSON

1. With reference to article 1 of the minute of its previous meeting of 22 June, 2012, the clerk to the Authority invited nominations for a Chairperson, and explained that in terms of the constitution of the Authority, a Chair and Vice Chair would be elected by the members attending the first meeting of the Authority following local government elections, which would be this meeting, as the previous meeting had not been quorate; the Chair and Vice Chair could not be from the same Council and they would hold office for a period of two years from the date of today's meeting. At the end of the Chair's term of office the Vice Chair would automatically assume the role of Chairperson and a new Vice Chair would be elected.

The Authority resolved:

that Councillor Milne be appointed as Chairperson.

At this juncture, Councillor Argyle assumed the Chair.

APPOINTMENT FOR VICE CHAIRPERSON

2. The Chairperson invited nominations for a Vice Chairperson.

The Authority resolved:

to appoint Councillor Argyle as Vice Chairperson.

MINUTE OF MEETING OF 9 MARCH, 2012

3. The Authority had before it the minute of its meeting of 9 March, 2012. It was noted that the previous meeting of the Authority (22 June, 2012) had not been quorate, and therefore members had not been in a position to approve the minute of 9 March, 2012 as a correct record at that time. It was further noted that only the Vice Convener and Councillor Cox had been in attendance at the meeting of 9 March from the attendees on this day, and they confirmed that the minute was a correct record.

The Authority resolved:

to approve the minute as a correct record.

MINUTE OF PREVIOUS MEETING OF 22 JUNE, 2012

4. The Committee had before it the minute of its previous meeting of 22 June, 2012. It was again noted that this meeting had not been quorate.

In relation to article 7, the Strategic Development Plan Manager advised that the Scottish Government would like to meet with the Authority to hold a seminar on the National Planning Framework.

The Committee resolved:

- (i) to note that “planed” on article 7, resolution (ii) of the minute should have read “planned”;
- (ii) to note that Councillor __ apologies had not been recorded;
- (iii) to ratify the annual report, as per article 5, resolution (i) of the minute;
- (iv) to note that the recommendation regarding the £34,000 payback to Aberdeen City and Aberdeenshire Councils would be considered at a later point in the agenda (article 8 of this minute refers);
- (v) to agree to meet the Scottish Government jointly with Nestrans at a date to be agreed between the clerk and the Chairs to the SDPA and Nestrans in relation to the National Planning Framework; and
- (vi) to otherwise note the minute.

ORDER OF AGENDA

5. The Chairperson suggested that item 7 of the agenda (Aberdeen City and Shire Strategic Development Plan) be considered as the next item of business.

The Committee resolved:

to agree to consider the Strategic Development Plan as the next item of business.

ABERDEEN CITY AND SHIRE STRATEGIC DEVELOPMENT PLAN

6. The Authority had before it a report by the Strategic Development Plan Manager which presented a draft of the proposed Strategic Development Plan (SDP) for consideration.

The report provided a background to the Plan and a synopsis of the draft Plan, which was appended to the report along with (1) a number of indicative plans and diagrams, (2) a draft habitats regulation appraisal record, (3) a sea environmental report, (4) a strategic flood risk assessment, (5) an equalities and human rights impact assessment; and (6) a document outlining responses which had been received to the October, 2011 Main Issues report.

A letter officers had received from the Harbour Board, which presented suggested wording to be incorporated into the Plan, was circulated to members.

The Authority resolved:

- (i) to commend the draft of the proposed Plan (appendix 1) and the associated supporting documentation (appendices 2 – 6) for further consideration by elected members, subject to (1) the “detail maps” etc on pages 41 - 49 being renamed “indicative diagrams”, and (2) officers considering how the comments in the letter received from the Harbour Board on 20 September, 2012, could be reflected in the Plan;
- (ii) to defer approval of the Plan, and recommendations (b) – (e) for consideration at an additional meeting of the SDPA to be convened on 9 November, 2012.

HOUSING LAND AUDIT 2012

7. The Authority had before it a report by the Strategic Development Plan Manager which advised of the results of the 2012 Housing Land Audit (which was appended to the report), and explained its implications for the housing land supply position.

The Authority resolved:

to note the content of the 2012 Housing Land Audit and continue to monitor the land supply and rates of house building over the next few years as required by the Structure Plan.

AUDITED STATEMENT OF ACCOUNTS 2011/12 AND INDEPENDENT AUDITOR'S REPORT

8. The Authority had before it a report by the Treasurer which presented the audited Statement of Accounts for 2011/12 and the independent auditor's report on

the audit. The Statement of Accounts was appended to the report. The Authority heard Karlyn Watt (Audit Scotland) in relation to the independent auditor's report.

The Authority resolved:

- (i) to welcome the Statement of Accounts for 2011/12 and the independent auditor's report;
- (ii) to thank officers for their prudent financial management of the accounts over the past year;
- (iii) to return £34,000 excess contributions to Aberdeen City and Aberdeenshire Councils; and
- (iv) that a £10 charge for a printed copy of the Statement of Accounts be made, and to note that the Statements could be accessed and downloaded free of charge from the SDPA website.

BUDGET MONITORING

9. With reference to article 5 of the minute of its meeting of 9 March, 2012 the Authority had before it a report by the Treasurer which advised on the forecast position for 2012/13, as at 31 August, 2012.

The Authority resolved:

to note the budget monitoring position and forecast outturn as at 31 August, 2012.

DATES OF FUTURE MEETINGS

10. The Authority had before it proposed dates and times of future meetings.

The Authority resolved:

- (i) to note that the next meeting would take place on 9 November, 2012 (article 6, resolution (ii) of this minute refers); and

- (ii) to agree that the Authority would meet on 14 December, 2012 and 22 March, 2013 at 2pm, whilst noting that a date for the additional joint meeting with Nestrans and the Scottish Government would be confirmed at an early date.

- **COUNCILLOR MILNE; Convener**

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1 – Introduction

1.1 Aberdeen City and Shire is a region of global significance. As the energy capital of Europe it has the ambition, skills and resources to lead the move towards a more secure and sustainable supply of energy and lead Scotland forward on a path of economic recovery.

1.2 The area already has an enviable reputation as an attractive place to live and work, with a high-quality environment, above-average incomes, low unemployment and among the happiest people in the UK. However, there is no room for complacency. The effects of climate change, the need to use resources more efficiently, changes in North Sea oil and gas production and differences in wealth and opportunity between some of the region's communities will threaten everyone's wellbeing if we do not take action.

1.3 By 2035, Aberdeen City and Shire will need to have adapted to a very different world. We need to face up to the challenges ahead to be able to maintain and improve the attractiveness of the region as a place to live, visit and do business.

1.4 The purpose of this plan is to set a clear direction for the future development of the North East – recognising the importance of improving links and connections, adding to the already enviable quality of life and providing the opportunities for high-quality sustainable growth towards which the public and private sectors can work to deliver our vision.

1.5 This plan is the first strategic development plan to cover the area. It has been developed from the previous structure plan approved by Scottish Ministers in 2009 and reflects the widespread support that plan received.

1.6 The plan covers the whole of Aberdeen City and Aberdeenshire except that part within the Cairngorms National Park. It has been prepared by the Aberdeen City and Shire Strategic Development

Planning Authority on behalf of Aberdeen City and Aberdeenshire councils. It represents a shared vision for the future of the area and has been developed in a way which aims to include everyone in the process, including communities, public-sector organisations and businesses.

Layout and content of the plan

1.7 This plan focuses on what we want to change. It does not cover all planning issues but only those which are nationally or regionally important or can best be dealt with by Aberdeen City Council and Aberdeenshire Council working together. Both councils prepare local development plans which cover a wider range of issues in more detail.

1.8 A key part of this plan is our vision and spatial strategy, supported by a series of objectives and targets. Proposals are included where they are important to putting the plan into practice. We have tried to use as few technical terms as possible, but there is a glossary at the end of the plan which defines some words and phrases you may not be familiar with.

1.9 It is vital to recognise the links between the different parts of the plan. We have divided the plan into a number of themes rather than topic-based chapters because individual topics relate to some, if not all, of the themes. This emphasises that the plan should be read as a whole.

1.10 The plan is supported by a Proposed Action Programme which shows how we plan to put the plan into effect with our partners. Local development plans are one of the main ways of putting this plan into effect but there are others. Both councils will use this plan in appropriate cases to help decide whether a development proposal would take the area in the right direction. However, local development plans (and supplementary guidance) consistent with this plan will be the main plans we will use to assess applications for planning permission.

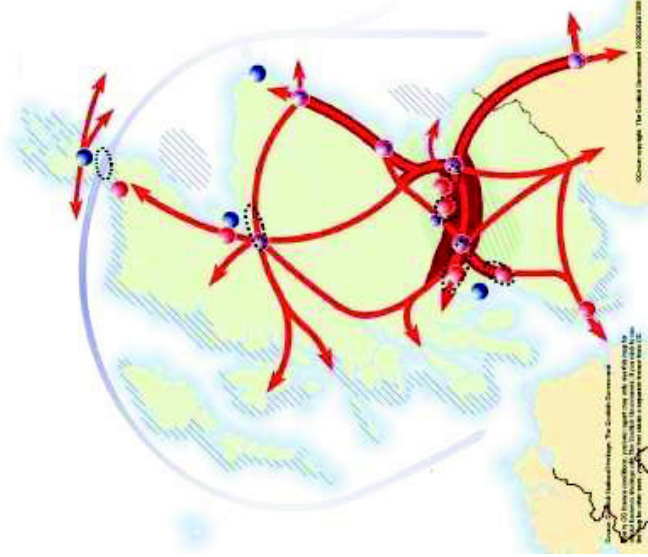
Influences on the plan

1.11 We have written this plan with a wide range of international, national and regional influences in mind.

1.12 At an international level, issues such as climate change, protecting the environment, securing energy supplies for the future and economic recovery all set an important context for this plan.

1.13 At the national level, National Planning Framework 2 was published by the Scottish Government in June 2009. The framework looks forward to 2030 with the main parts of the development strategy being shown in figure 2. The east coast corridor from Aberdeen to Newcastle is identified as a 'key economic corridor' offering opportunities to develop 'knowledge economy links' with the energy and offshore industries and the universities along the route. Peterhead is identified as a 'deep water opportunity' and Aberdeen as an international gateway.

The A90 and A96 are identified as strategic transport routes while Aberdeen is also shown as having 'strategic transport routes' beyond the strategic development plan area. A potential subsea energy network has been identified off the east coast, with a key onshore connection at Peterhead.



1.14 Along with the opportunities associated with the changing nature of the offshore oil and gas industry, the potential of the North East to build on its strengths is also recognised – growing and diversifying the economy. The National Planning Framework emphasises the opportunities offered by the deepest harbour in the North East at Peterhead, including a potential role in taking offshore equipment out of use. Three 'national developments' have been identified in the area – improvements (including access) to Aberdeen airport, the upgrading of Peterhead power station and reinforcing the electricity grid (East Coast transmission route).

1.15 Recent legislation and Scottish Government strategies, including the Climate Change (Scotland) Act 2009, Land Use Strategy, Zero Waste Plan, Economic Strategy, Regeneration Strategy and the Agenda for Cities, have been published since the previous structure plan. Forthcoming work includes marine planning at the national and regional level.

1.16 Over the last few years the Scottish Government has identified a range of improvements to the road and rail infrastructure in the area over the period of this plan. These include making the full length of the A96 a dual carriageway as well as reducing journey times and increasing the frequency of trains on the Aberdeen-Inverness railway line.

1.17 The regional strategies and plans which have an important influence on this plan include the North East Scotland Transport Partnership's (Nestrans) Regional Transport Strategy and Aberdeen City and Shire Economic Future's Economic Manifesto.



2 – The vision for the plan

2.1 We need to face up to the challenges and grasp the opportunities ahead to be able to maintain and improve the attractiveness of the region as a place to live, visit and do business. A vision for a successful and sustainable future is set out below.

Vision

“Aberdeen City and Shire will be an even more attractive, prosperous and sustainable European city region and an excellent place to live, visit and do business.

We will be recognised for:

- our enterprise and inventiveness, particularly in the knowledge economy and in high-value markets;
- the unique qualities of our environment; and
- our high quality of life.

We will have acted confidently and taken the courageous decisions necessary to further develop a robust and resilient economy and to lead the way towards development being sustainable, including dealing with climate change and creating a more inclusive society.”

2.2 To make this vision a reality, we have developed a series of aims. These provide a useful guide which both interprets the vision and focuses clearly on the role this plan will play in delivering it. This recognises that many different people and organisations will be involved in delivering this vision.

Aims

The main aims of the plan are to:

- provide a strong framework for investment decisions which help to grow and diversify the regional economy, supported by promoting the need to use resources more efficiently and effectively; and
- take on the urgent challenges of sustainable development and climate change.

To support these main aims, the plan also aims to:

- make sure the area has enough people, homes and jobs to support the level of services and facilities needed to maintain and improve the quality of life;
- protect and improve our valued assets and resources, including the built and natural environment and our cultural heritage;
- help create sustainable mixed communities, and the associated infrastructure, which meet the highest standards of urban and rural design and cater for the needs of the whole population; and
- make the most efficient use of the transport network, reducing the need for people to travel and making sure that walking, cycling and public transport are attractive choices.

2.3 We are committed to all the aims as a whole. This is the only way we will be able to achieve our vision.

Policy

In assessing development proposals, we will balance the importance given to each aim in coming to a decision, taking into account the spatial strategy, objectives and targets of the plan.

3 – The spatial strategy

3.1 All parts of the North East are important for the future of the area. However, different areas will take different roles as a result of their current characteristics and future opportunities. This plan will set a clear direction and be specific where necessary, while giving local development plans the opportunity to respond to local circumstances.

3.2 Identifying where development will take place is a vital part of delivering the changes needed in the Aberdeen City and Shire area. Being clear about where we want new homes, businesses and other developments to be provides long-term certainty and confidence for communities, businesses and organisations that provide services. It allows us to make best use of the investment that we will need to maintain and improve the quality of life in the area over the coming decades of growth.

3.3 We have developed a spatial strategy which promotes development in places that meet the needs of business and, at the same time, are sustainable and take on the challenges of climate change. The strategy also recognises the need to maintain and improve the area's communities and the services they depend on.

3.4 All parts of the strategic development plan area are either within a strategic growth area or a local growth and diversification area. However, some areas are also identified as regeneration priority areas.

3.5 This strategy (see figure 3) focuses development in places where there are clear opportunities to encourage people to use public transport, which itself can be provided in an efficient way. The strategy plans for significant growth in a limited number of places, where there can be significant public and private investment in schools, community facilities and transport infrastructure without affecting people's quality of life. It also provides the flexibility to

meet the needs of local communities. The strategy also identifies those areas that need investment to tackle important regeneration needs.

3.6 All areas will need significant upgrades to the communications infrastructure to allow for superfast broadband early in the period covered by this plan.

Targets

- Over the period to 2035, at least 75% of all homes built, and employment land developed should be in the strategic growth areas.
- Over the period to 2035, at least 50% of all homes built should be in Aberdeen City.
- For all developments meeting more than local needs to be in strategic growth areas, unless another area can be justified.

How to meet the targets

- Local development plans (and, where appropriate, supplementary guidance) will use the spatial strategy to set policies and identify land for development.
- Planning applications will be assessed against the spatial strategy to decide whether the location, nature and scale of development is in line with the plan.

Monitoring

Patterns in house-building and land used for new employment uses will be monitored each year.

Strategic growth areas

3.7 This strategic development plan identifies four strategic growth areas which will be the main focus for development in the area up to 2035.

3.8 Although these areas will carry out different roles, together they can be expected to account for around 75% of the growth over the next 20 years or more. The strategic growth areas are centred on Aberdeen and the main public transport routes. They aim to make housing, employment and services highly accessible by public transport and they will be relatively narrow, up to around 5km wide. However, the Aberdeenshire Local Development Plan will decide which settlements and sites are within a strategic growth area.

3.9 Development in these areas will bring about a significant need for new and improved infrastructure. This will include new primary and secondary schools, open space, improvements to roads and railways as well as walking, cycling and bus networks, and new water and waste-water systems.

3.10 We should explore opportunities to put in place a strategic walking and cycle network which builds on the current networks being developed in Aberdeen and Aberdeenshire towns by linking these together. We should do this in a way which creates opportunities for improvements to the natural environment by linking habitats and species in green networks. New tree-planting could play an important role in delivering these networks.

3.11 This investment in infrastructure is essential if we are to create successful sustainable mixed communities, and the cost is likely to be several hundred million pounds. While the public sector is committed to investing in the area, a significant part of the cost will have to be paid for by developers and landowners who should take account of this when negotiating the price paid for land.



3.12 We also need investment in new waste-management infrastructure such as recycling, composting and energy from waste. Most of this should be located in the strategic growth areas close to Aberdeen or in Aberdeen itself.

3.13 All four strategic growth areas should focus on creating sustainable mixed communities with the services, facilities and infrastructure necessary for the 21st century.

3.14 We discuss each of the four areas in detail below.

Local growth and diversification areas

3.15 Most of Aberdeenshire is identified in this plan for local growth and diversification. These areas are important assets to the North East and Scotland as a whole, providing areas of rich agricultural land as well as high environmental quality for tourism and outdoor recreation. The towns and villages reflect the different roles they have played over time.

3.16 Fishing, farming and forestry are important sectors of the economy. However, there is a need for diversification and growth in the economy in this area to meet local needs, with tourist-related developments having a role to play. Encouraging employment growth in these areas is a central part of making them more sustainable and reducing the need to commute, with its economic, social and environmental consequences.

3.17 Increasing tree cover can bring significant environmental, economic, social and community benefits if new planting respects the different functions of the land concerned and its role in a wider green network. There is clear potential to increase the tree cover of this area while respecting the competing interests for land. This will contribute to a wider Scottish Government target of increasing tree cover and recognising the role of woodland in reducing the causes of climate change (known as mitigation).

3.18 We need to improve some of the road infrastructure in the area, particularly those links to and from northern coastal communities such as Fraserburgh and Banff. This will support vital economic growth and regeneration in this area. This area will also play a part in supporting new sustainable energy infrastructure, and harbours along the north coast may play a role in supplying services to offshore renewable energy facilities.

3.19 Levels of growth in individual settlements should relate to local needs, although the scale of this growth will vary from place to

place. While we should aim to provide a mix of housing opportunities for everyone, we need to focus on providing smaller homes to buy or rent, and a significant amount of affordable housing. We need to give priority to mixed-use developments which respect the character of the landscape and local identity.

3.20 Housing allowances for this area are set out below, while employment land should be made available to meet local needs.

	Housing allowances		
	Existing LDP allocations to 2016	2017-2026	2027-2035
Local growth (AHMA)	1,000	1,350	1,350
Local growth (RHMA)	4,200	4,000	4,000

3.21 In this area, there will continue to be pressure for housing in the countryside which is not connected to existing settlements. However, local development plans, in line with Scottish Planning Policy, should approach this by focusing new housing in, or as an extension to, existing settlements, particularly those which are well served by public transport. This will help to create and maintain successful places and be more sustainable.

3.22 In some cases, developments which aim to meet more than local needs may need to be based in this area, such as mineral extraction. In these cases, developers will need to justify this against the aims, strategy, objectives and targets of this plan. Meeting regeneration needs could be one part of this justification.

Regeneration priority areas

3.23 Regeneration is a vital part of the strategy of this plan. Inequality exists in some parts of the area and these areas will not meet their full potential unless we take positive steps. These actions are needed to support ongoing community efforts and will involve forming partnerships across the public, private and voluntary sectors, and will focus on achieving clear results. Work is already going on but needs to continue and increase.

3.24 The areas in need of regeneration are identified broadly in this plan but are focused on the needs of existing communities. They include some of the coastal communities of north and south Aberdeenshire (including the two largest towns of Fraserburgh and Peterhead), as well as parts of Aberdeen (the City Centre and a number of its communities). Improving the economy, environmental quality, accessibility, employment opportunities and the competitiveness of business should play a particular role in these areas.

3.25 The challenges and opportunities in these areas are wide-ranging and demand a response appropriate to the particular area. We must do everything we can and explore funding to bring about positive change in these communities. This change must bring lasting benefits and have positive effects on their vitality and viability.

3.26 Opportunities for redeveloping brownfield sites will vary from place to place and over time but need to be a clear priority. Such redevelopment should respect the character of the local area, improve the quality of the environment, use high-quality design and include a mix of uses.

3.27 Aberdeen City Council is currently developing a new framework for regeneration to replace its strategy of 2007. This will integrate social, economic and area-based initiatives in a number of

parts of the city. As well as redirecting existing spending, the strategy will look at access to new funding sources and the use of its own asset base to stimulate new investment.

3.28 Aberdeenshire Council developed a 'Framework for Regeneration' in 2010 and this is supported by a series of action plans. These are based on the needs and opportunities of particular communities and cover improvements to the built and natural environment, actions to improve town-centre vitality, and measures to improve opportunities for employment.

3.29 Improvements to transport are particularly important to the coastal communities of north and south Aberdeenshire. The Aberdeen Western Peripheral Route and dualling of the A90 between Balmedie and Tippetry are important projects to help tackle these issues. However, further improvements to the A90 north of Aberdeen and better links to the trunk road network (such as improvements to the A947 and A952 from Banff and Fraserburgh) will also help improve access to markets. Access to services and employment in south Aberdeenshire is being supported through action by communities.

3.30 The quality of the environment varies from area to area, although it will be possible to further develop tourism in some coastal areas.

Aberdeen City

3.31 Aberdeen is Scotland's third-largest city and the regional centre for employment, retail, culture, health and higher education as well as being the region's transport hub – with road, rail, sea and air links.

3.32 Around half of all new development in the city region will be within Aberdeen City. While we prefer development to take place on brownfield sites, the scale of growth we expect will mean that more than half of the development will need to take place on greenfield sites. Reducing travel distances and making walking, cycling and public transport more attractive to people is vital. We will need to make sure that one or two secondary schools, associated primary schools and improvements to transport infrastructure are provided to serve new development in the city.

3.33 Housing and employment allowances for Aberdeen

	Housing allowances	
	Existing LDP allocations to 2016	2017-2026 2027-2035
Brownfield	4,500	3,000
Greenfield	12,000	5,000
	Employment land allowances (hectares)	
	105	70

3.34 We have planned a wide range of transport measures to either tackle existing problems or support the growth planned over the next 20 to 25 years. While congestion is a key factor, reducing the effect of transport on the environment (including improving air quality) is also important. Substantial public-sector funding will deliver the Aberdeen Western Peripheral Route and park-and-ride at its junction with the A96 as well as improvements to the Haudagain Roundabout and a new bridge over the River Don. Further proposals to support growth include:

- new bus services and action to give priority to buses and increase their frequency;
- junction improvements on the A96, A944, A956, A90, Kingswells North and Parkhill;
- safety improvements on the access road to the A93;
- improvements to the Parkway and Persley Bridge; and
- a link over the River Dee.

3.35 Communication and transport links are critical to the future prosperity of the city region, and Aberdeen Airport has a vital role to play in maintaining and improving these both domestically and internationally. Improvements at the airport are needed to improve customers' experience and increase the number of travellers it can support while keeping the effect on the environment to a minimum. Access to the airport also needs to be improved, including building the Aberdeen Western Peripheral Route and other related projects.

3.36 Aberdeen Harbour is a vital gateway for the regional economy and provides important passenger and freight links to the Northern Isles. The harbour has been identified as a key port in the National Renewables Infrastructure Plan. Work will be needed to set out in more detail the likely implications of this and the wider potential for growth it provides. Given its city-centre location, this work should take into account the wider city-centre regeneration as part of the current City Centre Development Framework and the competing demands for land.

3.37 Aberdeen City centre is an important asset for the region but its regeneration is vital for the economic future of the area and how potential investors and residents see it. Details on this are included in the City Centre Development Framework but we need to take determined action to create a city centre to be proud of and which reflects the global status and ambitions of the city and the wider region. The Business Improvement District (BID), set up in 2011, was a major initiative and showed that the private sector was willing to actively take part in this process. There needs to be a strong

focus on improving the quality of the city centre's shopping, leisure, commercial and residential environment – with partial pedestrianisation of Union Street having an important role.

3.38 Aberdeen has a number of communities which would benefit from regeneration. These are largely areas with a high percentage of social housing built in the 1960s which no longer provide satisfactory living conditions. There is more detail about this in the Regeneration Priority Area theme below.

3.39 Two possible locations are shown for a new Community Stadium to support the growing sporting infrastructure of the city.



Aberdeen to Huntly

3.40 This area is the key corridor linking Aberdeen and Inverness and contains several thriving Aberdeenshire communities. Significant development will take place in the 31-mile road and rail corridor stretching from Huntly to Blackburn. Development at the southern end of the corridor will be mainly focused towards the end of the plan period, beyond 2027, while further north it will be spread throughout the period of the plan.

Housing and employment allowances for this area

	Housing allowances		
	Existing LDP allocations to 2016	2017-2026	2027-2035
Inverurie - Blackburn	1,000	1,500	4,000
Huntly - Pitcaple	300	500	200
Huntly - Blackburn		42	28
		Employment land allowances (hectares)	
		42	28

3.41 South of Inverurie the corridor is served by a dual carriageway to Aberdeen, with the Scottish Government committed to completing the A96 dual carriageway between Inverurie and Inverness over the lifetime of this plan. However, short-term improvements will also be made (such as replacing the Inveramsay Bridge to allow two-way traffic) before the more comprehensive upgrade planned for later in the plan period. Significant improvements are also planned to the Aberdeen-Inverness railway line, with more services, shorter journey times and a new station at Kintore. We will also look at further improvements, in the form of a more local rail service to better serve commuters, shoppers, students and visitors. This will be supported by development in this corridor. Bus services on this corridor are already good, although we will need to make further improvements to make these services

an even more attractive choice. We will need to provide a secondary school and associated primary schools in this area. Several trunk road junctions will also need to be improved, including those at Inverurie, with opportunities considered for an eastern bypass for the town.

3.42 Development will be supported by new infrastructure within Aberdeen City such as the Aberdeen Western Peripheral Route, a new park-and-ride site at its junction with the A96 and other improvements on the A96 corridor.

Aberdeen to Laurencekirk

3.43 This area is the key corridor linking the region with central and southern Scotland as well as the rest of the UK by road and rail. Its main settlement is Stonehaven where the Highland Fault Line marks the boundary between Highland and Lowland Scotland, which can be seen in the geology and landscape of the area.

3.44 We are proposing significant new development in the northern part of this corridor early in the plan period. A significant new settlement is planned south of Portlethen at Chapelton of Elswick which has the potential to set the standard for future new development in Aberdeen City and Shire and beyond.

3.45 New housing and employment development is also proposed in the southern end of the corridor as well, but on a more limited scale. This will be supported by a new secondary school in Laurencekirk (Mearns Academy Community Campus) which will replace the existing school on a nearby site.

Housing and employment allowances for this area

	Housing allowances		
	Existing LDP allocations to 2016	2017-2026	2027-2035
Portlethen – Stonehaven	2,200	2,400	1,000
South of Drumlithie - Laurencekirk	500	400	100
	Employment land allowances (hectares)		
Portlethen - Laurencekirk	63		42

3.46 Improved junctions on the A90 will also be needed for development at Elswick and Laurencekirk (where existing safety issues also need to be taken into account) to go ahead.

3.47 The southern leg of the Aberdeen Western Peripheral Route passes through the northern part of this area and the Fastlink will significantly improve access from the south to the airport and northern Aberdeenshire. A park-and-ride will be built at Portlethen so that traffic heading for the city can use more sustainable travel for the last leg of the journey.

3.48 A new station opened at Laurencekirk in 2009 and has proved extremely popular, with twice the expected number of passengers already using it. We will look at the potential for developing a further station at Newtonhill alongside other possible locations within Aberdeen. Other rail improvements include improvements to journey times and electrification.

3.49 Development will be supported by new infrastructure within Aberdeen City such as a new link over the River Dee and improvements to the A956.

3.50 We will support proposals outwith the strategic development plan area which reduce journey times by road and rail south.

Aberdeen to Peterhead

3.51 This area has important strategic assets, high environmental quality and significant potential for development. Energetica is already helping to realise this potential and create a global hub for the energy sector in a first-class coastal location. This is being done through partnership between the public and private sectors.

3.52 Both employment and housing land will be needed over the period of the plan, with these allowances set out in figure sdf below. Current plans already identify land up to 2026, while land identified for the period beyond 2027 will need to be allocated in future local development plans.

Housing and employment allowances for this area

	Housing allowances		
	Existing LDP Allocations to 2016	2017-2026	2027-2035
Blackdog - Ellon	800	1,500	1,500
Hatton - Peterhead	800	800	600
Blackdog - Peterhead	Employment land allowances (hectares)		
	45ha		43ha

3.53 The northern end of the Energetica corridor has a unique opportunity to play an important role in the energy future of Europe.

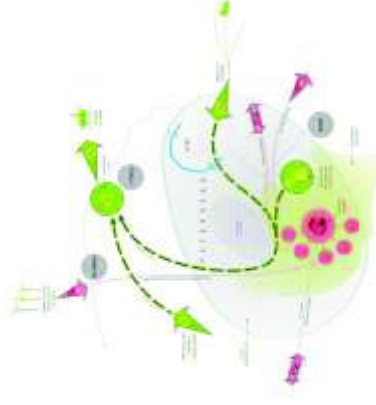
- We need to make the best use of the deepwater opportunities at Peterhead Port. It has been identified in the National Renewables Infrastructure Plan and we need to aid its diversification into oil and gas decommissioning as well as offshore renewables.
- The Peterhead and St Fergus area has been vital to the UK oil and gas industry over the last 40 years and will be well into the future. It now also has significant potential for capturing,

transporting and storing carbon dioxide under the North Sea (carbon capture and storage). This would make extensive use of existing infrastructure, help to deliver significant reductions in greenhouse gas emissions and provide a major competitive advantage in this emerging technology.

- Peterhead also has the potential to be an important hub in transmitting renewable energy both within the UK and more widely as part of a European network. Its coastal location and existing connections make it an attractive choice for subsea cables and their onshore infrastructure.

3.54 These nationally and internationally important developments need to be co-ordinated effectively (see figure sdf). To achieve this, a development framework will be needed for the southern gateway to Peterhead.

3.55 Further south, realising the economic potential of the Menie Estate golf resort will be important as well as delivering vital infrastructure projects in the first plan period to encourage growth. These include building the Aberdeen Western Peripheral Route, making the A90 between Balmedie and Tipperty a dual carriageway, improving the Haudagain Roundabout and delivering a new bridge over the River Don. Until this infrastructure is in place, development in the southern part of the corridor will be limited. The focus in the whole area needs to be on developing and diversifying the economy with a strong focus on the quality of development. We will also need to provide a new secondary school and associated primary school in this area.



Economic Growth

Objective

To provide opportunities which encourage economic development and create new employment in a range of areas that are both appropriate for and attractive to the needs of different industries, while at the same time improving the essential strategic infrastructure necessary to allow the economy to grow over the long term.

Issues

4.1 Aberdeen City and Shire has a healthy economy. While currently dominated by the oil and gas industries, it also has significant strengths in life sciences, food and drink and tourism. The oil and gas industry will continue to be very important into the future. Its changing focus over the next few decades (into supplying the export market and decommissioning) will mean that the area will need to sell itself more strongly to make sure growth happens here. Traditional industries, such as farming and fishing, must continue to be supported. To promote a diverse local economy we will have to build on our existing assets (particularly in the field of renewable energy) and put into practice new ideas, including those from our universities, research organisations and our skilled and innovative workforce, and improve our connections to the outside world through transport and high-speed digital communications networks.

4.2 A strong service sector is also important and in particular, retail and all forms of tourism, including business tourism, will have a role to play. It is very important that retail services throughout the region are promoted to guarantee the vitality and viability of our town centres, while creating opportunities for growth in the economy. It is particularly important that we continue to support the regeneration of Aberdeen city centre – making sure that this key regional asset

fulfils its potential by improving its shopping, leisure, commercial and residential environment as well as its visitor experience. Some of the building blocks are already in place (such as the City Centre Development Framework and the Business Improvement District) but there is still much to be done. In line with Scottish Planning Policy, a sequential approach will be taken when identifying sites for new retail development across the strategic development plan area.

4.3 Making sure that there is enough of the right type of land for business use, in the right places, will give Aberdeen City and Shire a competitive advantage, this includes mixed-use developments on appropriate sites. We expect that the Energetica initiative will help to deliver this in the Aberdeen to Peterhead strategic growth area, while we will encourage economic development in local growth and diversification areas at appropriate levels to meet local need. Aberdeen City and Shire already has a very well-developed role in the knowledge economy. Providing high-quality business space has an important role to play in building on this, as has support for high quality education and training opportunities from schools, colleges and universities.

4.4 The roll-out of high-speed broadband throughout the area is vital for the economy as well as bringing social and environmental benefits. World-leading internet access is essential in the strategic growth areas, while significantly improved speeds are also necessary in the rural growth and diversification areas. The public and private sectors will need to work together to deliver these improvements.

4.5 Connections, within Aberdeen City and Shire as well as to the rest of Scotland, the UK and the rest of the world are critical for the future health of the local economy. Aberdeen airport and the region's main ports are key gateways and have a vital role to play in accessing international markets as well as providing lifeline services to Orkney and Shetland. It is vital that these important

regional assets are supported in meeting their potential for the regional economy and that they reflect the aims of the area. Future development should not be allowed to limit the growth of the economy by making the region less attractive to business, particularly in relation to congestion and access to roads, ports, airports and rail facilities. This infrastructure needs to be protected and improved, including supporting freight being transported by rail and sea.

Targets

- To make sure there is at least 60 hectares of marketable land available to businesses at all times in a range of places within Aberdeen City.
- To make sure there is at least 60 hectares of marketable land available to businesses at all times in a range of places within strategic growth areas in Aberdeenshire.
- For at least 20 hectares of the above land available to businesses in the strategic growth areas to be of a standard which will attract high-quality businesses or be suitable for company headquarters.
- For Aberdeen to remain one of the top-20 retail areas in the UK.
- For all new development to have the use of modern, up-to-date high-speed telecommunications networks, such as fibre optics.

How to meet the targets

- Local development plans should, where possible, protect strategic reserve land from other uses (see figures 3 and 4).

Figure 3: Employment land allocations

	Established land 2011 (hectares)	Marketable land 2011 (hectares)	Minimum land available at all times (hectares)	LDP allocations to 2026 (hectares)	Strategic reserve land 2027-2035 (hectares)
Aberdeen City	210	87	60	105	70
Aberdeenshire					
Huntly to Laurencekirk	153	75	40	105	70
Blackdog to Peterhead	104	86	20	45	43
Total	467	248	120	255	183

- Identifying and maintaining an appropriate amount of business land in the local development plan for 'local growth and diversification' areas in Aberdeenshire.
- Put the City Centre Development Framework into practice and carry out a wider regeneration of the area.
- Work with partners to make significant improvements to provide world-class telecommunications networks.
- Working in partnership with universities, colleges and other research organisations to help them make as positive a contribution as possible to the area, including for example, the implementation of the Foresterhill Masterplan prepared by the University of Aberdeen and NHS Grampian.
- Using action programmes to identify and co-ordinate the actions needed to make land allocations available to business in line with the spatial strategy.

Monitoring

Monitoring will focus on the supply of employment land, the roll-out of high-speed telecommunications networks and retail rankings.

Sustainable development and climate change

Objective

To be a city region which takes the lead in reducing the amount of carbon dioxide released into the air, adapts to the effects of climate change and limits the amount of non-renewable resources it uses.

Issues

4.6 The challenges of sustainable development and climate change are some of the most serious we will face over the period covered by this plan. Reducing the causes of climate change (known as mitigation) is important. However, due to current and past actions, it is vital to put in place a number of measures to limit the consequences of climate change as well (known as adaptation).

4.7 To tackle climate change, energy efficiency will need to be improved in existing as well as new development. All new developments must be designed and built to use resources more efficiently and be located in places where they have as little an effect on the environment as possible. Focusing initially on using energy-efficiency measures is likely to be the best way of reducing the demand for energy as a step towards achieving carbon-neutral development, and this should be done as soon as practically possible. Joint supplementary guidance may be prepared to require new development to meet specific carbon-dioxide levels through the sustainability labelling mechanism. Developers will need to examine the scope for including combined heat and power schemes when preparing larger development proposals.

4.8 Significant changes will also be needed to reduce the effects of transport on climate change. As well as increasing energy efficiency, we will need to encourage the use of alternative fuels and take opportunities, such as providing electric-vehicle charging points, to encourage people to use them.

4.9 We also need to tackle the supply of energy during the plan period. This will involve increasing the supply of energy from renewable sources and reducing emissions of climate-change gases from existing power stations. As well as some extra capacity for onshore wind there is still considerable potential in offshore wind, energy from waste, solar and biomass, as well as ground, water and air source heat pumps. A more balanced mix of renewable energy sources will be needed if we are to meet our renewables targets.

4.10 One of our significant potential roles will be to put in place infrastructure so that Scotland can meet its renewable energy and carbon-emissions targets.

- The onshore electricity grid will need to be upgraded to allow new sources of renewable energy to be developed in the north of Scotland. This means upgrading the East Coast transmission route from 275kV to 400kV (identified as a 'national development' in the National Planning Framework) as well as the line from Rothienorman to Peterhead. However, the volumes of electricity likely to be generated mean that offshore transmission will also be necessary, with Peterhead acting as a main transmission hub with links to England as well as other European countries to balance renewable energy generation and overcome the intermittency challenges associated with the widespread use of wind energy.
- Carbon capture and storage has significant potential to reduce greenhouse gas emissions from conventional power stations and help the change to a low-carbon future. We are well placed to play a key role in this, building on our world-leading expertise, existing infrastructure and nearness to potential storage sites. The technology has huge global-market potential over the period of this plan and beyond. The power station at Peterhead offers an ideal opportunity to demonstrate the technology and

significant storage potential that lies in old oil and gas reservoirs under the North Sea, with St Fergus the established gateway to this potential. Using existing infrastructure will be the key to making the technology commercially viable, with existing high-pressure gas pipelines already in place between St Fergus and central Scotland. Peterhead also has the potential to act as a key import point, with carbon dioxide being shipped there from elsewhere in the UK or further afield before being transported in existing pipelines up to St Fergus and onwards for storage under the North Sea.

4.11 Areas of Aberdeen and Aberdeenshire are already at risk from flooding, but increased risk comes about through changing and more unpredictable weather patterns and rising sea levels. Avoiding flood risk is an important measure to adapt to climate change. We will need to take account of the scale of these changes when we consider what new developments should be built and where they should be located.

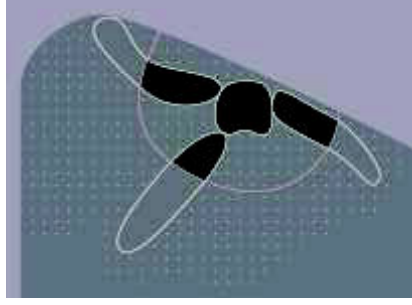
4.12 Scotland's Zero Waste Plan has set a target of recycling or composting 70% of all waste, and limiting waste sent to landfill sites to 5%, by 2025. Meeting these targets will represent a significant change from current practice and move the emphasis from providing enough landfill space to providing sites for other treatment processes (such as composting and energy from waste).

4.13 Our objective is to meet our waste-management needs locally as far as possible. This will make sure that the economic, social and environmental value from managing this important resource is kept in the local area and the environmental costs of transport are kept to a minimum.



4.14 While there is no overall need for new landfill space in the plan area, we are likely to need time extensions at some existing sites to make sure we maintain capacity, make the best use of sites and restore the sites to the highest standards. We should do this while taking into account the effect on communities and the legitimate concerns of residents close to such facilities.

4.15 Managing our waste responsibly and in line with the waste hierarchy, proximity principle and Zero Waste Plan will mean we will need a variety of significant extra waste-treatment facilities (with a capacity of over 300,000 tonnes) in a variety of locations. We



should develop these new facilities as close as possible to the source of the waste, with around 75% of them being in the strategic growth areas within or close to Aberdeen. Outwith this area, facilities should generally meet a more local need or need to be in a rural location. Possible locations include sites next to existing waste-management facilities or land which local development plans have identified for employment or mixed-use purposes where the new site would be compatible with nearby existing or proposed uses.

4.16 When deciding whether a site is suitable, a range of factors will be important, including good access to the transport network and whether there is a realistic prospect of using any heat or electricity generated by the process. We must integrate how we manage waste into all development proposals and consider construction and demolition waste as well as waste generated during the lifetime of the development. Energy from waste facilities within wider development proposals which use surplus heat for homes, businesses and community facilities are likely to be very efficient and will require potential developers to speak to each other at an early stage.

4.17 The River Dee (see figure 6) is an important economic asset as well as being the region's main source of drinking water and a Special Area of Conservation. This resource is under pressure and as a result, managing the use of water and increasing water efficiency is vital. During and beyond the period of this plan the effects of climate change may affect the river and we may have to take appropriate adaptation measures.

Targets

- For all new buildings to be carbon neutral by 2020.
- For the city region's electricity needs to be met from renewable sources by 2020.
- To avoid having to increase the amount of water Scottish Water are licensed to take from the River Dee, as a result of the new developments proposed in the plan.
- For all new developments to use water-saving technology.
- To avoid developments on land which is at an unacceptable risk from coastal or river flooding (as defined by the 'Indicative River and Coastal Flood Map for Scotland' or through a detailed flood risk assessment), except in exceptional circumstances.
- To work towards an extra 300,000 tonnes of new waste-management infrastructure.

How to meet the targets

- Local development plans (and supplementary guidance) will promote a gradual move towards buildings which are carbon neutral.
- Local development plans (and supplementary guidance) will identify areas or technology which can contribute to the supply of renewable energy.
- Work with Scottish Natural Heritage, the Scottish Environment Protection Agency, Scottish Water and other stakeholders to better understand the likely medium to long-term effects of climate change

on the River Dee and any adaptation measures that may be needed.

- The two councils should work together to identify and put in place appropriate residual waste management solutions.
- Local development plans should not identify sites for new development which are at an unacceptable risk from flooding, except for brownfield sites which have appropriate flood prevention measures in place. Unacceptable risk will normally be more than a 1 in 200 (0.5%) chance of a flood happening in any year, although this will vary with the type of development being proposed.
- Use master planning (and supplementary guidance) to consider the possible scope of combined heat and power schemes to contribute towards using energy more efficiently and in reducing the amount of energy used overall.

Monitoring

Monitoring will focus on the quantity of water extracted from the River Dee, the amount of renewable energy produced in the area and the percentage of waste which is taken to landfill, recycled or composted.

Population growth

Objective

To increase the population of the city region and achieve a balanced age range to help maintain and improve people's quality of life.

Issues

4.18 Expectations of housing need and demand over the period to 2035 have been assessed in a Housing Need and Demand Assessment (2011).

4.19 The population of the city region has been growing for a number of years and was 465,000 in 2011. This is expected to grow to around 500,000 by 2035. However, as well as the total size of the population, the age range of the population also presents challenges in the years ahead. The over-65s age group is predicted to grow at the fastest rate – this will have a significant effect on the economy of the area and the way in which services are provided for an increasingly elderly population.

4.20 Planning for an increase in the population of 35,000 over 24 years will be a significant challenge for the North East. However, most of the need for almost 54,000 new homes (see figure 8) comes from the existing population of the area as the average number of people in each household falls. We will only achieve these challenging levels of growth if we work towards our aims and objectives with our partners.

4.21 If the population grows to the levels we are aiming for, we will need an increase in the number of homes which are built each year. This presents a significant challenge given the current access to finance for both developers and households.

4.22 The housing requirement is set out in figure 8 below.

Figure 8: Housing requirement

	2011-2016	2017-2026	2027-2035	Total
Aberdeen HMA	9,965	15,017	13,506	38,488
Rural HMA	4,237	6,411	4,836	15,484
Total	14,202	21,428	18,342	53,972

4.23 Schedule 1 (page 27) shows the housing allowances needed to help deliver this increase in new homes and allow development at the rates targeted in this plan.

4.24 In line with Scottish Planning Policy, these allowances provide a generous supply of land for new housing (Schedule 1) on top of the housing requirement (figure 8). As a result, we cannot expect all the new homes to be built within the relevant plan period. This makes sure the plan can cope with higher levels of demand than we currently expect.

Targets

- To increase the population of the city region to 500,000 by 2035.
- To move towards building at least 2,500 homes a year by 2016 through the development plan.
- To move towards building at least 3,000 homes a year by 2020 through the development plan.

How to meet the targets

4.25 Both planning authorities will need to make land available for homes through local development plans in line with the spatial strategy and schedule 1 of this plan. Phasing of land allocations for the period 2017-2026 may be put in place where appropriate. We will use action programmes, planning agreements and possibly

compulsory purchase powers to focus attention on delivering new housing, employment and commercial uses and providing the necessary infrastructure.

4.26 As well as these, there are also other actions we need to take to meet this objective. These actions are covered under other objectives which focus on the quality of development and will help to make the North East an even more attractive place to live, visit and do business. In this way, providing land for housing and increasing the population is one part of a strategy to increase the quality of life in the area. Providing new housing which does not meet the requirements of the spatial strategy or meet the standards set out under other objectives will not be supported by this plan.

4.27 Meeting these targets will also depend on factors which are not related to the development plan or under the control of the planning authorities. These factors include the health of the global and local economy, the price of oil, access to and the cost of development and mortgage finance, the response of the house-building industry and how the area is marketed to potential residents from the rest of Scotland or further afield.

Monitoring

Monitoring will assess trends in population growth and house-building in the area.

Quality of the environment

Objective

To make sure new development maintains and improves the region's important built, natural and cultural assets.

Issues

4.28 The built, natural and cultural environment is a valuable resource, but one which needs to be protected. The way in which sites are chosen, and high quality design, will make sure that development does not lead to the decline of the North East's built, natural and cultural assets.

4.29 Biodiversity, wildlife habitats and landscape can be vulnerable to the effects of new development. As a result, we need to focus on maintaining and improving them, especially where there are plans for development. In particular, we need to take care over the possible effects of development on Natura 2000 sites and the Cairngorms National Park.

4.30 Not all water bodies in the area meet the quality standards under the Water Framework Directive. River Basin Management Plans have been prepared in partnership to deal with these issues.

4.31 The green belt around Aberdeen will continue to play a vital role in protecting the character and landscape setting of the City. However, it will need to change to meet the growth this plan seeks to achieve. It must guide development to appropriate places while protecting the most important areas. Accessible green networks within and around Aberdeen and other major settlements will also be important to achieving a high quality environment.

4.32 Sites and areas valued for their contribution to the built and historic environment are just as sensitive and should be protected from the negative effects of development.

Targets

- To make sure that development improves and does not lead to the loss of, or damage to, built, natural or cultural heritage assets.
- To avoid new development preventing water bodies achieving 'good ecological status' under the Water Framework Directive.

How to meet the targets

- Local development plans (and supplementary guidance) will make sure the North East's natural environment and historic sites and buildings continue to be protected and improved.
- Both councils will take part in the River Basin Management Planning process, particularly as it relates to new development.
- Both councils will take account of biodiversity, wildlife habitats, landscape, green networks and other sensitive areas in identifying land for development, preparing masterplans and assessing development proposals.

Monitoring

- Measuring the percentage of water bodies that achieve 'good ecological status', carried out by the Scottish Environment Protection Agency.
- Assessing how development has directly or indirectly led to the loss of the built, natural and cultural assets, by carrying out reviews through local development plans.
- Assessing how much development has been granted planning permission against green belt policies, using appeal decisions and Scottish Minister notifications.

Sustainable mixed communities

Objective

To make sure that new development meets the needs of the whole community, both now and in the future, and makes the area a more attractive place for residents and businesses to move to.

Issues

4.33 Too often in the past, new development has not been sustainable, mixed or focused on meeting the needs of the community. We must not allow this to continue. If we are to achieve our vision, we must set the highest standards for urban and rural design, the mix of land uses, using land more efficiently and re-using previously developed land.

4.34 A focus on sustainable mixed communities means not dealing with different land uses in isolation. Retail, employment, education, health, leisure, open space and transport as well as housing are all vital for successful development. Such development creates a strong sense of place and a high-quality environment in which to live and work.

4.35 Our future communities must be mixed in terms of the type and size of homes, as well as their tenure and cost. We also need to make sure that new homes are designed to a high standard and that they respect and improve the existing qualities of the area. If we want to allow people to have a choice of where they want to live, developments must provide a wide range of options, from starter homes to large detached houses. Providing affordable housing is also critical to delivering sustainable mixed communities. However, currently there is more of a need for affordable housing (identified in the Housing Need and Demand Assessment as 38% of new homes) than there is funding available. Meeting the targets in this plan will need considerable effort and creativity on the part of both

public and private sectors working together. The two councils will prepare local housing strategies which will consider in more detail how to provide affordable housing. Having a variety of homes and residential environments will help to improve urban design in the city, towns and rural areas. New housing which does not meet the targets set out below will not be supported by this plan.

4.36 It is important that new development meets the needs of the whole community, including the specific needs of Gypsies / Travellers. With an ageing population and smaller households, new development will need to meet the changing needs of society over its whole life.

4.37 Land brought forward for development must be used efficiently and brownfield sites and regeneration areas should be given priority. Through this process, we should create opportunities for new development that will close the gap between the wealthy and most disadvantaged areas of the region.

4.38 New development needs to focus on communities. In the strategic growth areas, services and facilities for the community must be a part of the development. This plan will not support house-building that fails to provide these services and facilities which help make successful and sustainable communities.

4.39 In the same way, a focus on communities means that new housing and developments for employment and commercial use should be integrated (well related to each other and community facilities) to improve people's quality of life and opportunity. This should be a particular focus in masterplans prepared for larger development areas.

4.40 To create sustainable mixed communities, retail services must be one of the main considerations in preparing masterplans and development briefs for new development. We will take a sequential approach to new retail development. However, we will also support

existing retail centres to make sure that the needs of the community are met now as well as in the future.

4.41 This plan has a clear objective for population growth in the North East and increasing the level of house-building. However, this will only be acceptable if the development is of the highest quality.

Targets

- To increase the range and quality of housing and the residential environment in the 'regeneration priority areas'.
- For 40% of all new housing in Aberdeen City to be on brownfield sites.
- For all housing development of over one hectare in strategic growth areas to be in line with approved supplementary guidance and generally have no less than 30 dwellings per hectare.
- For the quality and design of new developments in the city region to be nationally recognised.
- For new housing to meet the needs of the whole community by providing appropriate levels of affordable housing (around 25%) and an appropriate mix of types and sizes of homes, in line with local development plans and approved supplementary guidance.
- For local development plans and all masterplans to fully consider the scope for a mix of uses on a site.

How to meet the targets

- Local development plans will take account of these targets when deciding how and where to identify land for development.
- Everyone involved in the process needs to show a commitment to quality and set the highest standards throughout the development process, settling for good practice, but aiming for excellence.
- Using supplementary guidance to provide more detail on the appropriate mix of properties, urban-design principles and community facilities.

- Maintain a design review process for masterplans and the most significant planning applications.

Monitoring

- Measuring how many properties there are in each hectare for sites of over one hectare which have been granted planning permission in strategic growth areas, carried out each year using the housing land audit.
- Measuring the percentage of new housing on brownfield sites in Aberdeen, carried out each year using the housing land audit.
- Report on regeneration activity and outcomes.
- Building up a detailed picture of new housing development, including the sizes, types and tenure of properties, carried out each year using the housing land audit.

Accessibility

Objective

To make sure that all new developments contribute towards reducing the need to travel and encourage people to walk, cycle or use public transport by making these attractive choices.

Issues

4.42 It is widely accepted that there are negative environmental, economic and social effects associated with road transport, including air and noise pollution and congestion.

4.43 This plan will help to promote the link between land use and transport to make sure that all new development is conveniently located and designed in such a way as to encourage walking, cycling and the use of public transport. Improving bus and train services will make sure that people will be encouraged to use more environmentally friendly forms of transport.

4.44 Walking and cycling are the most environmentally friendly forms of transport and should also be encouraged as this will help to achieve important health, social and environmental benefits.

4.45 Improving access to the North East is also essential to developing a strong economy and providing better links. The spatial strategy promotes opportunities at harbours and the airport as well as helping to promote accessibility by making sure that future development also allows for improvements in the rail and road networks. This will help to improve bus and train services and encourage people to use them.

Targets

- For all developments to meet the accessibility standards set out in the regional transport strategy.

- For major employment and service developments in strategic growth areas to show that they are easy to access by walking, cycling or using public transport. The travel plans produced for these developments should reduce the need for people to use cars.
- To reduce the percentage of journeys made by car.

How to meet the targets

- Local development plans will identify and protect land from development to make sure that transport proposals identified in the local and regional transport strategies can be developed. Transport proposals needed due to development promoted in the plan will be identified in supplementary guidance.
- Work with Neustrans to make sure that putting the strategic development plan into practice closely links with the regional transport strategy.
- The spatial strategy will direct development to areas that can be accessed by a greater choice of more environmentally friendly forms of transport.
- Both councils will prepare and keep under review core path plans taking account of where new developments will take place and issues which affect both council areas.
- The action programme will, at an early stage, identify the necessary requirements to make sure that developments can be accessed by walking, cycling and public transport.
- Approved supplementary guidance will include layouts and design for new developments which can be accessed easily by walking, cycling and public transport.

Monitoring

Monitoring will focus on the accessibility of developments and the percentage of journeys which are made by different methods.

5 - Putting this plan into practice

5.1 This plan provides a vision that aims to develop the economy, maintain and improve the quality of our environment and provide a high quality of life. The scale of the changes needed and the challenges set by the vision, especially in relation to sustainable development and climate change, should not be underestimated.

5.2 This plan sets the framework for other plans and strategies, and promotes targets to achieve our aims. Partnership working between the public and private sectors will be essential to put the plan into practice. One of the main ways we will put this plan into practice is through local development plans and supplementary guidance. These are prepared separately by Aberdeen City Council and Aberdeenshire Council and must be consistent with this plan.

5.3 However, local development plans will not have the long-term vision of the strategic development plan. There may be action that needs to be taken now to allow proposals to be included in future local development plans. We have developed an action programme to support this plan.

5.4 The programme identifies who is to take action, when this will need to happen and how it will be funded and delivered. This will give the people concerned the confidence to make decisions on investment. The action programme will be monitored regularly and reviewed at least every two years.

5.5 In truly exceptional circumstances, for example if it is essential to put the plan's strategy into practice, planning permission may need to be granted on sites identified in local development plans for the period 2027-2035 (shown in schedule 1). Any permission may have conditions which control the rate of development. We would need to highlight the need for this action through the Action Programme and both the strategic development planning authority and the relevant council would have to agree to it.

5.6 We have carried out a 'Habitats Regulations Appraisal' for this plan. However, we will have to carry out a more detailed assessment at a later stage for local development plans and certain developments to make sure they will not have a negative effect on any Natura 2000 site.

Providing infrastructure

5.7 To make changes on the scale proposed in this plan, we must invest in existing and new infrastructure in the strategic development plan area. This is important to maintain quality of life in communities. The existing roads, sewers and schools will not be able to cope with the demands extra development will bring. We will also need to invest in facilities and services for communities. These two needs have to be carefully considered very early in the land-development process, even before land is identified in local development plans.

5.8 Developers will have to accept the need for contributions towards necessary infrastructure, services and facilities within their own site. However, in cases where development has wider effects, we will have to secure contributions to deal with these as well.

5.9 We will prepare supplementary guidance in support of this plan. This will allow (through a 'Strategic Transport Fund') transport projects which are needed as a result of the combined effect of new development in the strategic growth areas within the Aberdeen Housing Market Area to be funded and delivered.

5.10 We expect that the increase in land value, as a result of granting planning permission, will fund a large percentage of the new infrastructure needed, although the public sector will also need to make an important contribution. We will need to secure a higher percentage of the increase in land values than we have in the past, to be able to create sustainable mixed communities.

5.16 We will review this plan on the timescale set out in the legislation, with a revised plan going to Scottish Ministers within four years of this plan's approval.

5.11 The Aberdeen Western Peripheral Route, dualling of the A90 between Balmedie and Tipperly and of the A90 between Inverurie and the border with Moray are already Transport Scotland projects, along with a new station at Kintore and other service improvements on the Aberdeen-Inverness rail line. All other work affecting the trunk road, or rail network, will need Transport Scotland's permission.

Proposals

5.12 A range of projects have been proposed that will help us achieve our vision for the North-East. While some are well under way, others are at an early stage. These are set out at the front of the plan in the spatial strategy as well as being listed in Schedule 2. You can find more detail on each proposal in the [Proposed] Action Programme.

Monitoring and reviewing this plan

5.13 We recognise that things will change over time, both in response to the direction this plan promotes and events we were not able to predict. Monitoring must take place to assess our progress in meeting the plan's objectives and ultimately, the vision.

5.14 Each objective of this plan identifies ways of monitoring and we will produce a monitoring report to assess our progress in meeting the targets.

5.15 Significant changes in society, the economy, environment and population will only become clear over the longer term, but will also need to be monitored to make sure the plan's objectives are appropriate. Monitoring these will, in practice, be a continuous process. However, we will only report on them as part of the monitoring statement prepared for the next plan.

Schedule 1: Housing allowances

	Effective Land Supply 2011	Constrained Land supply 2011	Existing LDP allowances to 2016	2017 to 2026	2027 to 2035	Total
Aberdeen City – Brownfield*	1,188	1,244	4,500	3,000	3,000	10,500
Aberdeen City – Greenfield*	1,207	506	12,000	5,000	4,000	21,000
Aberdeen City	2,530	1,750	16,500	8,000	7,000	31,500
Huntly – Pitcaple	191	207	300	500	200	1,000
Inverurie – Blackburn*	1,191	279	1,000	1,500	4,000	6,500
Portlethen – Stonehaven*	714	16	2,200	2,400	1,000	5,600
South of Drumlithie – Laurencekirk	253	10	500	400	100	1,000
Peterhead – Hatton	724	311	800	800	600	2,200
Ellon – Blackdog*	280	545	800	1,500	1,500	3,800
Local Growth (AHMA)*	1,283	185	1,000	1,350	1,350	3,700
Local Growth (RHMA)	1,584	1,434	4,200	4,000	4,000	12,200
Small Sites (AHMA)*	455					
Small Sites (RHMA)	745					
Aberdeenshire	7,285	2,987	10,800	12,450	12,750	36,000
AHMA (*)	6,318	2,775	21,500	14,750	14,850	51,100
RHMA	3,497	1,962	5,800	5,700	4,900	16,400
Aberdeen City and Shire	9,815	4,737	27,300	20,450	19,750	67,500

*Aberdeen Housing Market Area (AHMA)

Note: The 'Effective Land Supply 2011' includes the 5-year effective housing land supply as well as the effective supply programmed beyond year five. To avoid double-counting, we have not included any local development plan allocations in the effective or constrained land supply.

The Plain English Campaign's Crystal Mark does not apply to any table.

Schedule 2: Proposal

- The Aberdeen Western Peripheral Route and other associated measures to provide better access and connections to other places and to allow environmentally friendly methods of transport to be provided.
- Dualling between Balmedie and Tippierty and further improvements to the A90 (north) to provide better access and connections.
- Replace the Inveramsay Bridge and longer-term dualling of the A96 between Inverurie and the border with Moray.
- Improvements to the Haudagain roundabout, a third crossing of the River Don and other associated measures to deal with congestion and allow growth in and to the north of the city.
- Two new park-and-ride services around Aberdeen to improve accessibility, air quality and provide wider transport benefits.
- Road and rail improvements, including a new station at Kintore and improved local rail services, to make environmentally friendly methods of transport more attractive – including reducing journey times to Inverness and Edinburgh / Glasgow and beyond.
- Putting the Aberdeen airport masterplan into practice.
- Better links between Aberdeen and the communities on the north coast of Aberdeenshire.
- Initiatives by the universities, colleges and research organisations to strengthen our knowledge economy.
- The Energetica initiative being promoted by Aberdeen City and Shire Economic Future for the Aberdeen to Peterhead strategic growth area.
- Improving the port and associated facilities at Aberdeen and Peterhead to make the most of their opportunities and potential, particularly for the energy (including offshore wind) and fishing sectors.
- Proposals to regenerate Aberdeen city centre and some of the city region's most disadvantaged areas.
- Put in place the City Centre Development Framework, which includes proposals to make part of Union Street an area for pedestrians only.
- A new community stadium – a regionally important facility which will bring economic, social and cultural benefits (two possible locations are shown on map x).
- Upgrades to the onshore electricity grid and the development of offshore transmission from Peterhead.
- Upgrading of Peterhead power station and putting in place carbon capture and storage technology and infrastructure.
- A range of transport measures to deal with the combined effect of new development on the transport network in a number of locations including:
 - new bus services and action to give them priority and increase their frequency;
 - junction and capacity improvements on the A96, A944, A956, A90, Kingswells North and Parkhill;
 - safety improvements on the access road to the A93;
 - improvements to the Parkway and Persley Bridge; and
 - a link over the River Dee.

Glossary

Action programmes

Documents that set out what needs to be done to put the development plan into practice. They show important dates and who is responsible for each action.

Adaptation

Action to limit how exposed the environment and people are to the effects of climate change.

Affordable housing

Housing which is made available at a cost below full market value to meet an identified need. It includes homes rented from the council and housing associations and, in some cases, homes sold or rented from the private sector.

Allocations

Areas of land identified in a local development plan for development.

Biodiversity

The variety of life and richness of all living things in the natural world.

Brownfield sites

Land, normally in towns and villages, which has been built on or used in the past for some purpose. Brownfield land does not include private or public gardens, sports pitches, woodlands or open spaces used for leisure and recreation purposes. The grounds of institutions (such as schools or hospitals) that are no longer used are not considered as brownfield sites.

Carbon neutral

Development which limits the amount of energy used and creates as much renewable energy as it uses each year for heating and electrical appliances.

Compulsory purchase

This gives local authorities the power, by law, to buy land and buildings which are needed to deliver important projects.

Diversify

Increase the range of sectors of the economy to reduce the risk of relying too much on any one sector.

Dwellings per hectare

The number of homes that are built on a single hectare of land.

Effective housing land supply

The housing land available on which to build new homes.

Energetica

A development initiative between Aberdeen and Peterhead to create a world-class business and residential location within a high-quality environmental setting.

Energy from waste

The process of creating energy, in the form of electricity or heat (or both), from waste.

Flood prevention measures

Examples include walls, embankments, new channels and flood storage areas to try and prevent a site from flooding.

Green belt

An area of countryside where strict planning controls are in place to protect the landscape setting of the city, maintain the identity of places and provide land for recreation.

Greenfield sites

Land on which no building has taken place.

Grid reinforcement

Upgrading the electricity supply network, which includes overhead power lines and substations.

Habitats Regulations Appraisal

An assessment we must carry out under the European Habitats Directive to make sure the plan will have no negative effects on internationally designated environmental sites.

Hectare

An area of land equal to 10,000 square metres (100 x 100 metres) or 2.471 acres.

High-value markets

The goods and services across many different market sectors that are considered to be of higher value, for example, luxury ice cream, high-quality clothing, oil support or computing technology services.

Housing requirement

The total amount of new housing needed over the plan period.

Hub

An important point on a transport (or other) network which has a number of connections.

Infrastructure

The parts of cities, towns or villages that make them work. This includes water, drainage, electricity and phones, as well as roads, schools, community halls, healthcare facilities, libraries, open space and bus stops.

Knowledge economy

Sections of the economy that use knowledge rather than physical resources to create wealth. Examples include education, research, and product development.

Local development plan

These are prepared by every local authority. They contain detailed policies and sites that have been chosen for development in line with the approved strategic development plan and after a lot of consultation with local communities. The local development plan will be the main way for planning applications to be assessed.

Mitigation

Action to reduce carbon dioxide and other harmful substances released into the air as a way of reducing the level of climate change.

National development

A project in the National Planning Framework which the Scottish Government agrees is needed and wants to see built.

National Planning Framework

A document that sets out the Scottish Government's development priorities and identifies national developments.

Natura 2000

A European network of protected sites which represent areas of the highest value for natural habitats and species of plants and animals which are rare, endangered or vulnerable in the European Community. The areas are known as either 'Special Areas of Conservation (SAC)' or 'Special Protections Areas (SPA)'.

Non-renewable resources

Resources that will run out and cannot be replaced. Non-renewable energy sources include coal, gas and oil.

Planning gain

Money or infrastructure (see above) provided by developers and landowners to allow essential projects to be built that are directly linked to development proposals. Planning gain will normally be part of a legal agreement.

Renewable resources

Resources that do not run out. Renewable energy sources include energy from the sun, the ground, wind, waves, tides and wood.

Residual waste

The waste left over after items have been removed to be re-used or recycled.

Scottish Planning Policy

Statements of the Scottish Government's policies on national land use and other planning matters, such as housing, transport and green belts.

Sequential approach

A process for choosing sites for retail development. The approach first looks for sites in city and town centres, then sites on the edge of these centres, then other commercial centres identified in the development plan, and finally out-of-town sites which can be, or are, accessed by different forms of transport.

Strategic

Important issues which need both councils to work together.

Strategic development plan

These have replaced structure plans and are prepared by strategic development planning authorities. They set out a plan for their area and must concentrate on the main land use and development matters in that area. These, along with the local development plan (see above), will be the main way for planning applications to be assessed.

Strategic infrastructure

This is major investment in large-scale infrastructure (see above). This can include main roads, major junctions, water reservoirs, waste-water treatment works, pumping stations, secondary schools and hospitals.

Strategic reserve

Areas of land identified in a local development plan for possible future development.

Supplementary guidance or supplementary planning guidance

These documents support a plan by setting out details of how the policies or proposals should be put into practice. These can include masterplans, development briefs and design briefs. They may also include other policy statements on a wide range of issues.

Sustainability labelling

A system which recognises the most sustainable new development through the building standards system.

Sustainable

Something that will last because it has tackled its current and longer-term environmental, social and economic effects and does not rely on non-renewable resources.

Sustainable development

A widely used definition of this is 'development that meets the needs of the present without compromising the ability of future generations to meet their own needs'. It means that development should take full account of its implications on the local community and economy and on the environment both locally and worldwide.

Tenure

The legal right under which property is held. Housing tenures include owner-occupied homes, homes rented from private landlords, and homes rented from a council or housing association. Shared ownership is a tenure where a percentage of a property is owned and the rest is rented.

Vitality and viability

A measure of how lively and busy city and town centres are and their ability to attract ongoing investment for maintenance, improvement and to meet changing needs.

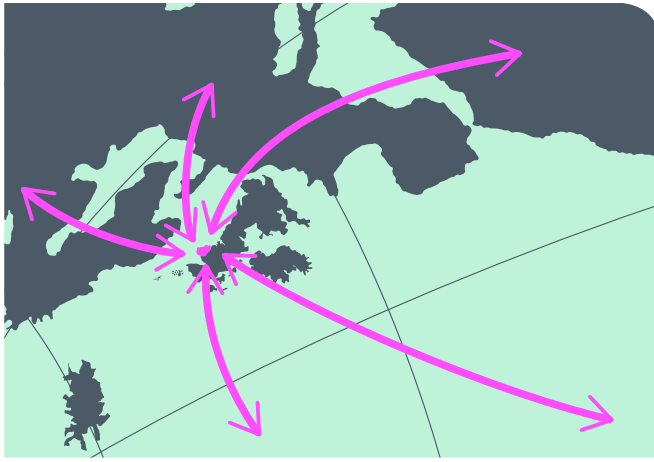
Waste hierarchy

A way of thinking about waste which focuses on reducing waste in the first place through re-using, recycling, and recovering energy. Waste disposal in landfill sites is at the bottom of the hierarchy.

Water bodies

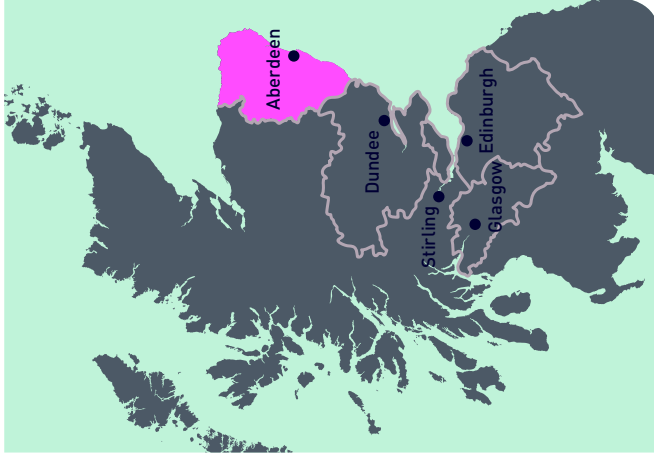
Places where water is found such as rivers, burns, lochs, ponds, boggy wet land, water held under the ground and coastal waters.

01 Context map (location) | International, national, local and B Plan local

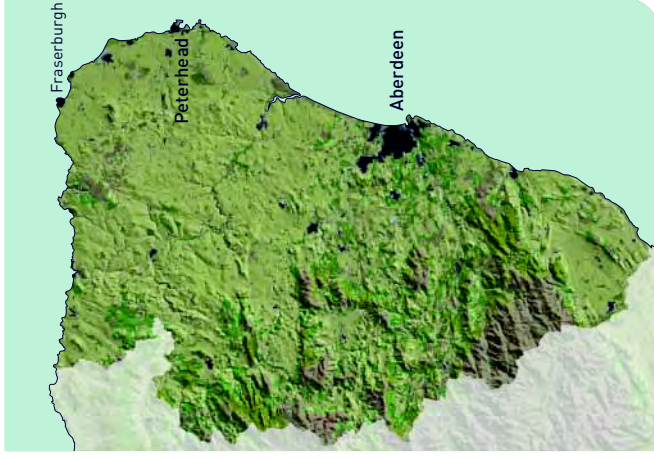


Key

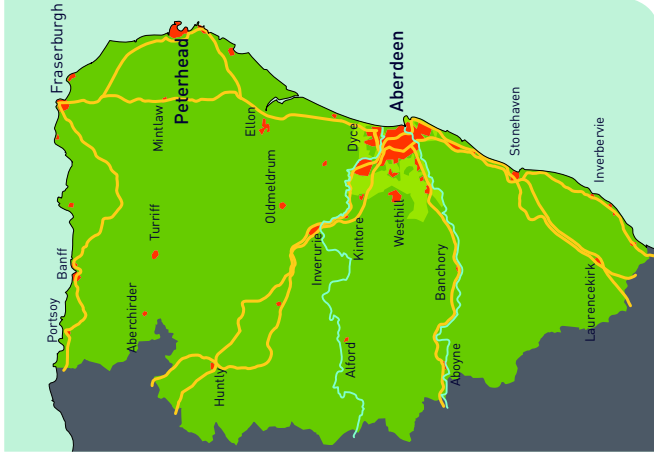
- International trade**
- Africa
 - Asia Pacific
 - Australasia
 - European Union
 - Former Soviet Union and Eastern Europe
 - Latin America
 - Middle East
 - North America
 - Western Europe (excluding EU)



- Strategic Development Plan Areas**
- Aberdeen City and Shire
 - Dundee, Perth, Angus and North Fife
 - Edinburgh and South East Scotland
 - Glasgow and the Clyde Valley

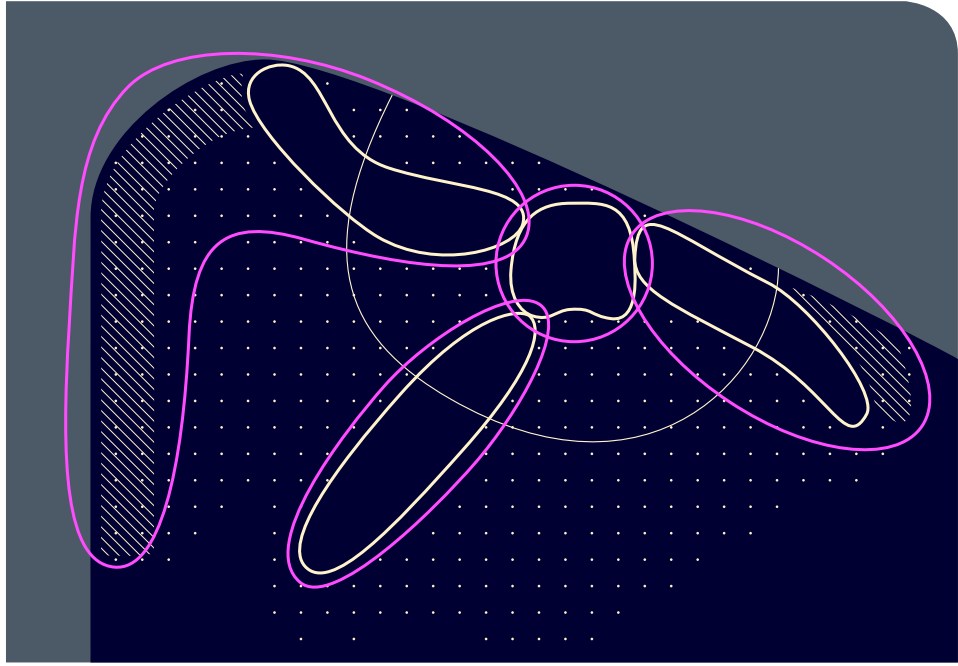
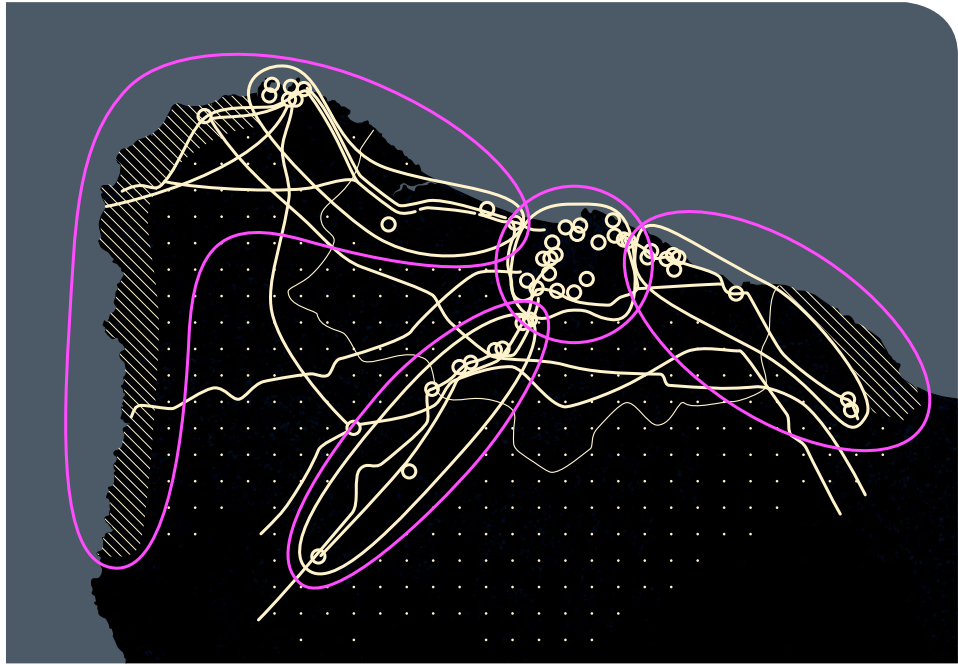
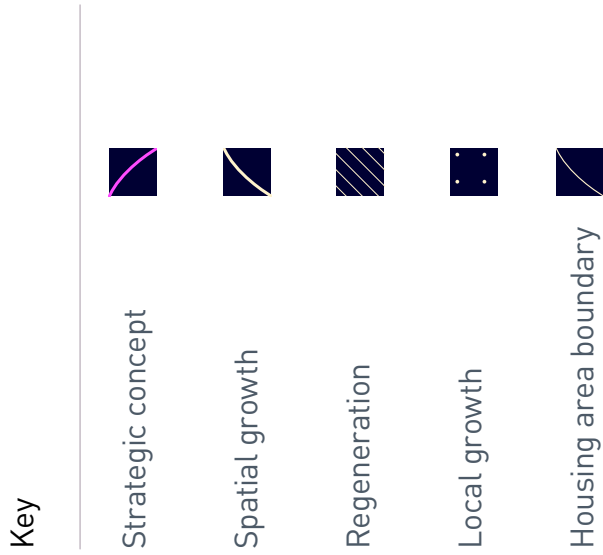


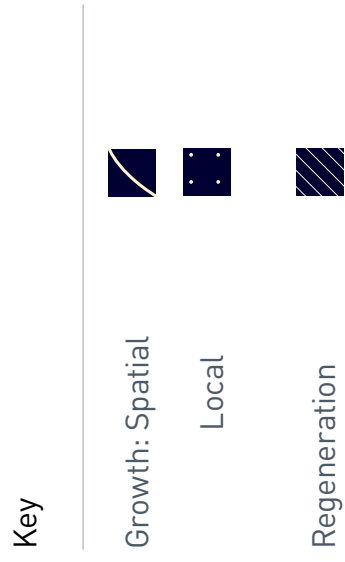
Satellite land use image



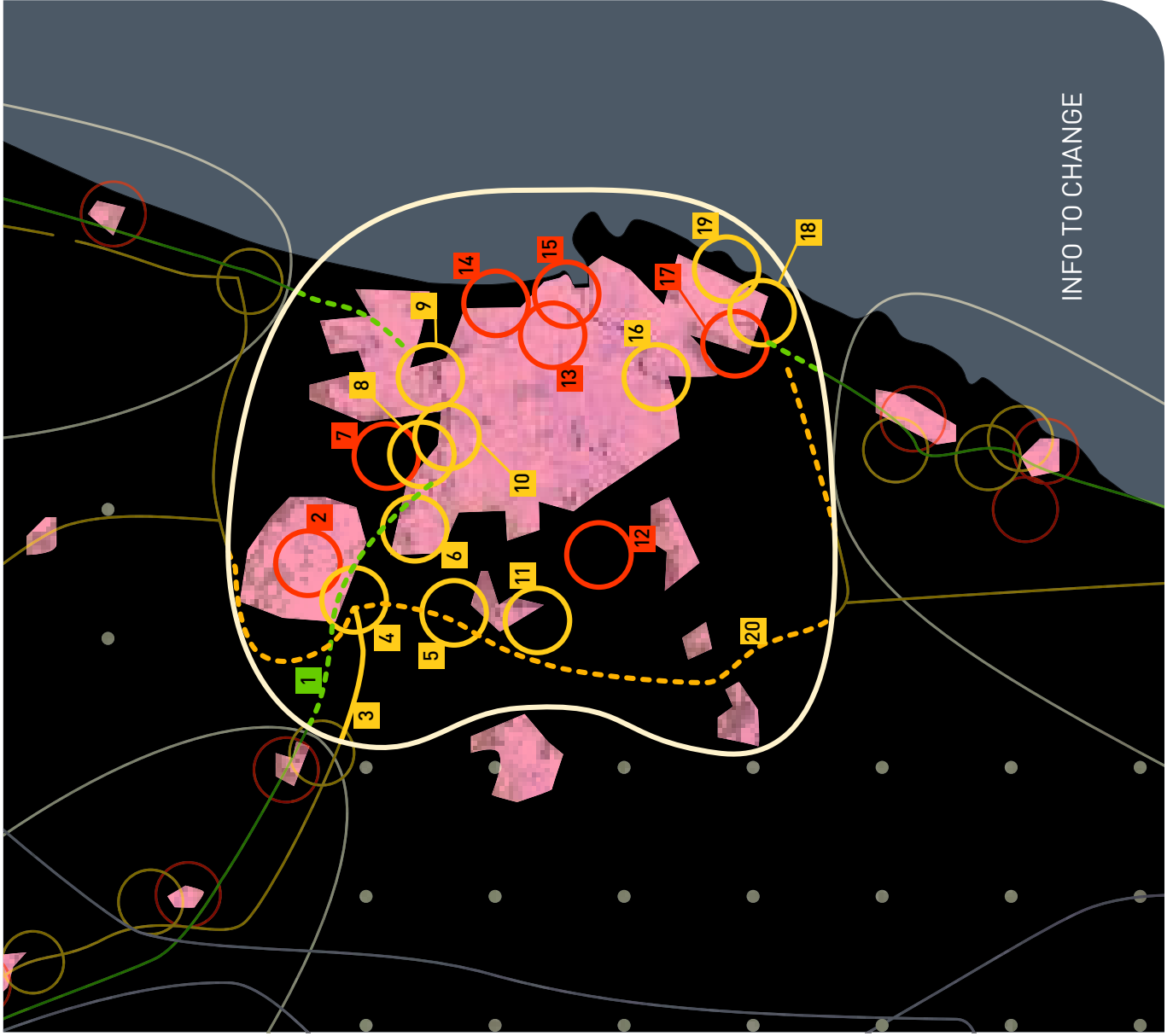
Ordnance Survey B Plan

- Movement
- Green/Greenbelt
- Place
- Water

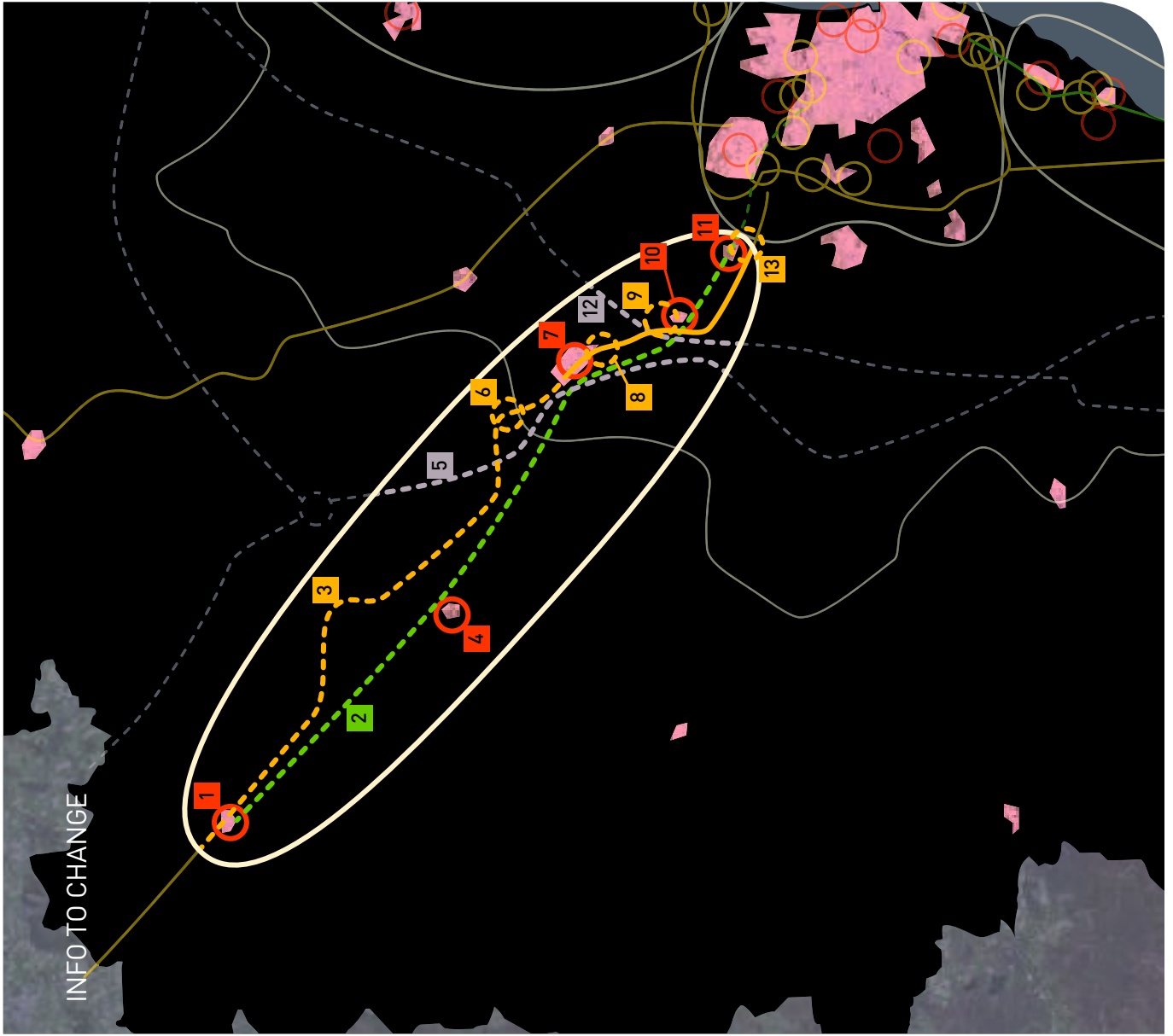




Key	Existing	Proposed
Spatial growth areas		
Urban areas		
Movement		
Place		



Key	Existing	Proposed
Spatial growth areas		
Urban areas		
Housing area boundary		
Movement		
Green		
Place		
Infrastructure		



Key Existing Proposed

Spatial growth areas



Urban areas



Housing area boundary



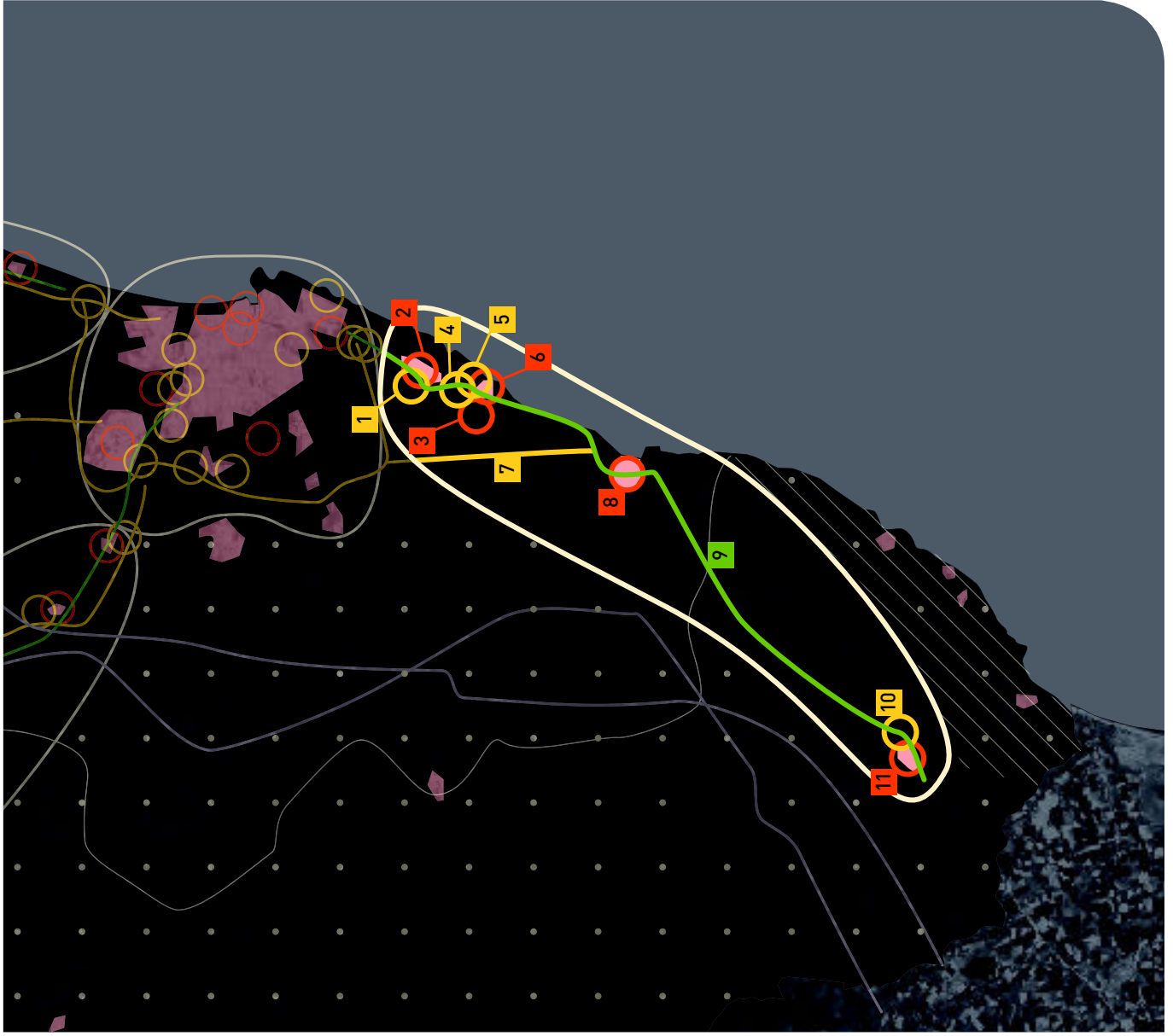
Movement



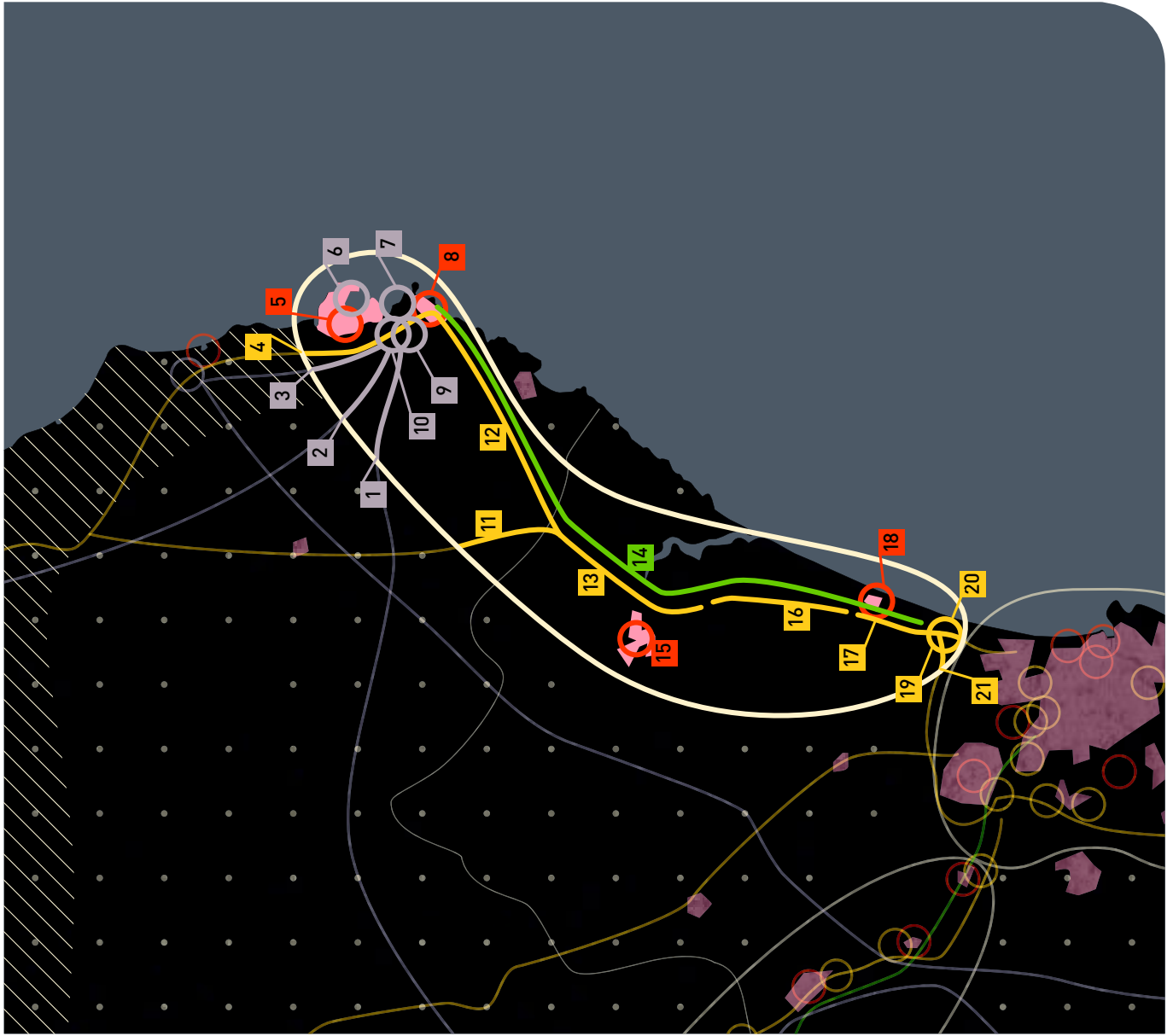
Green

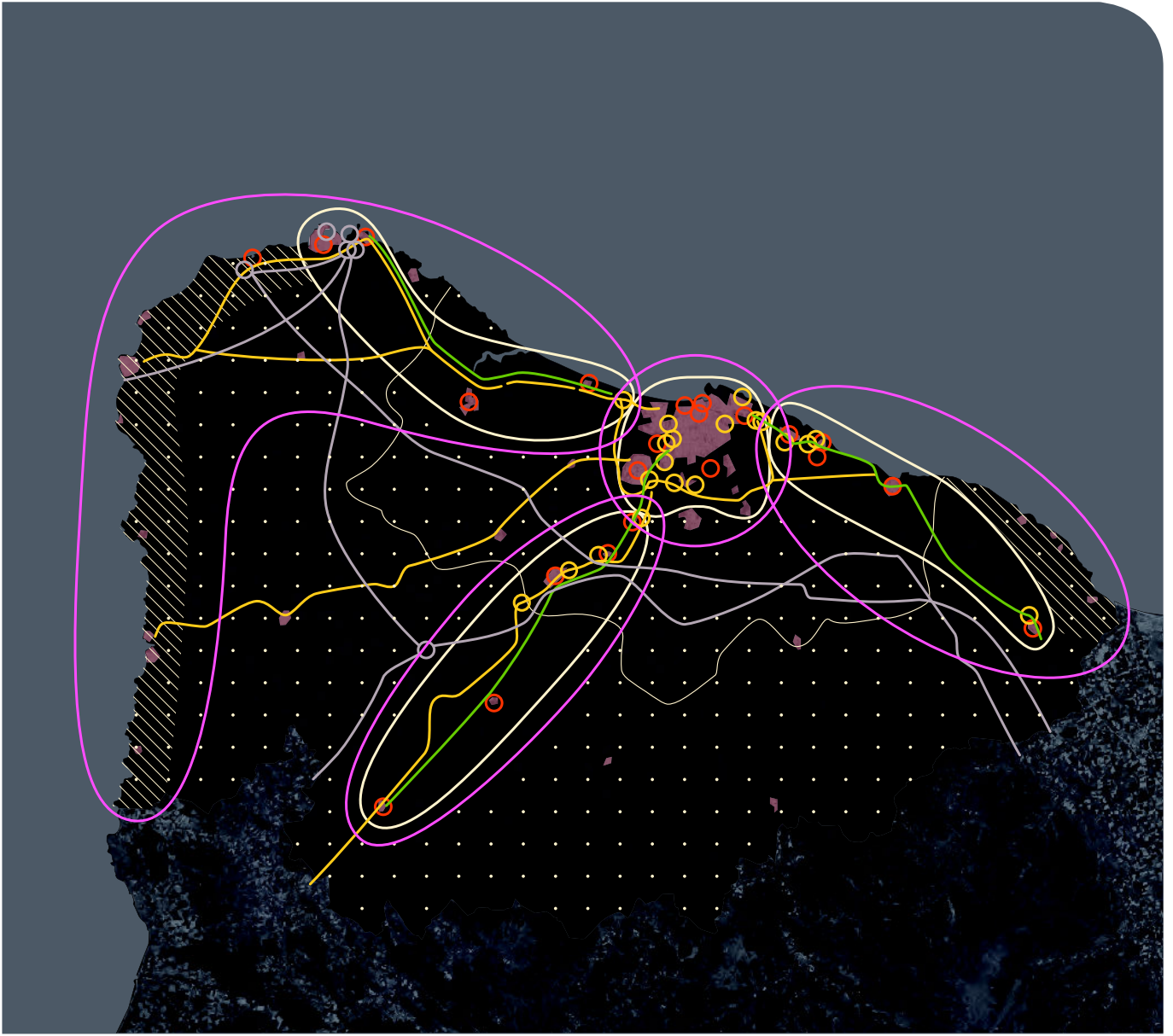
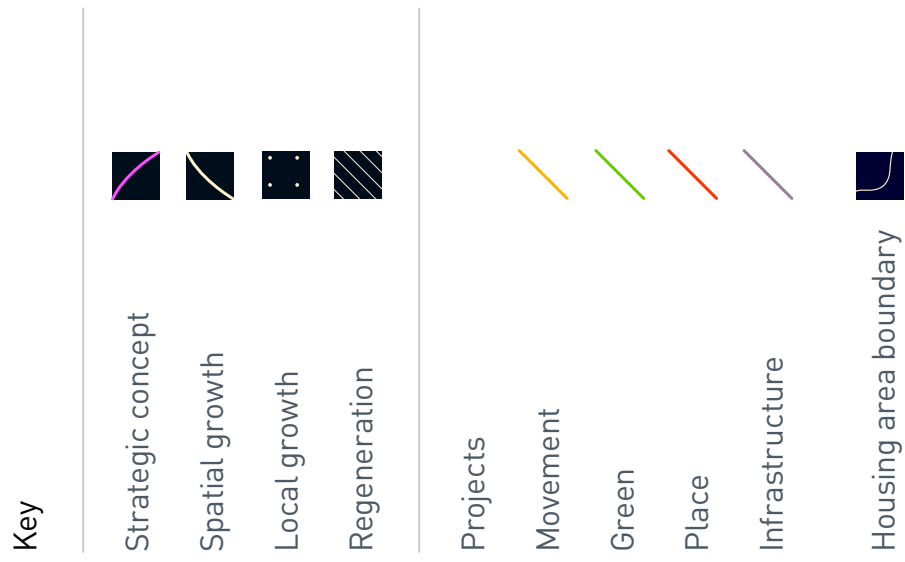


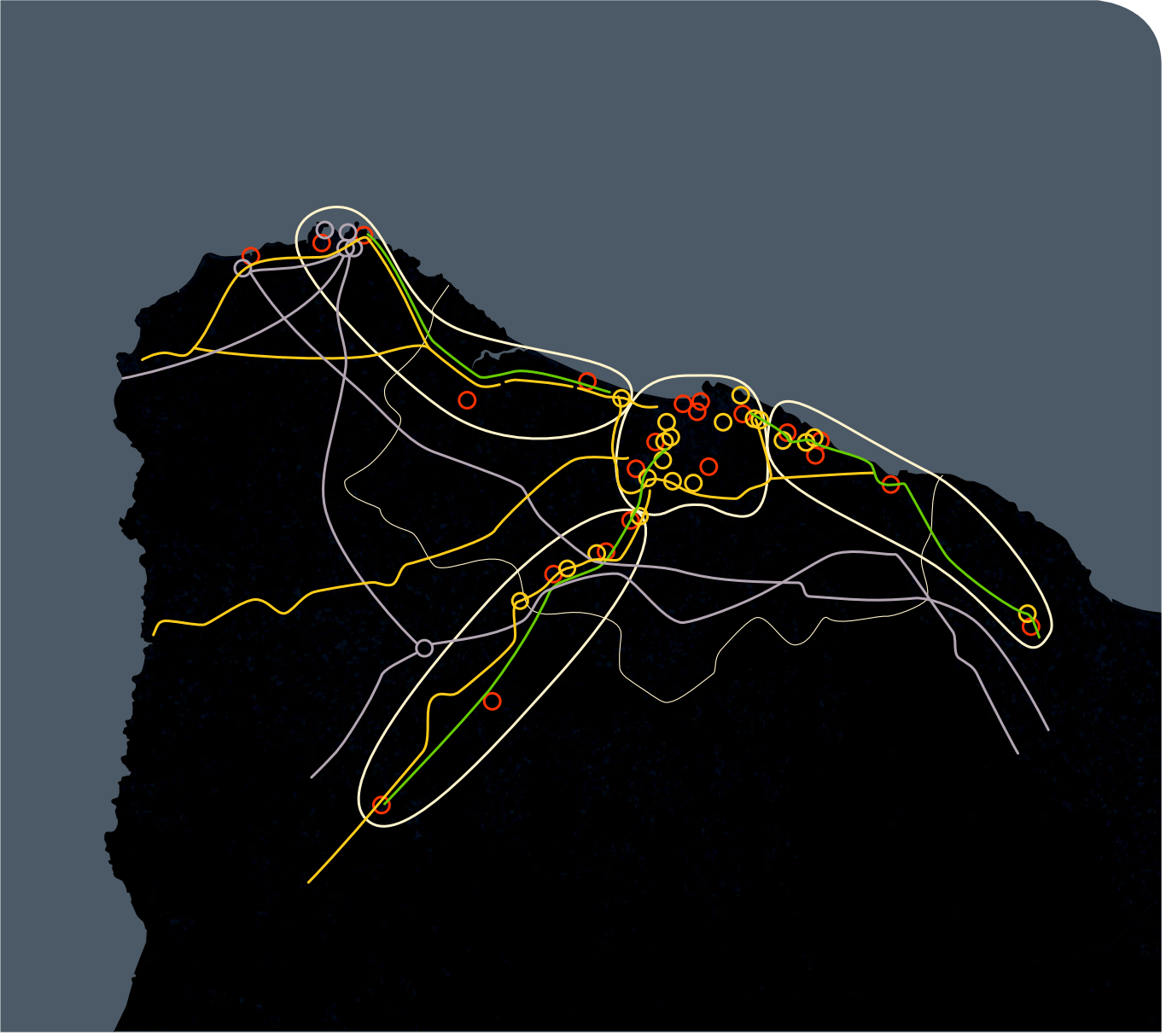
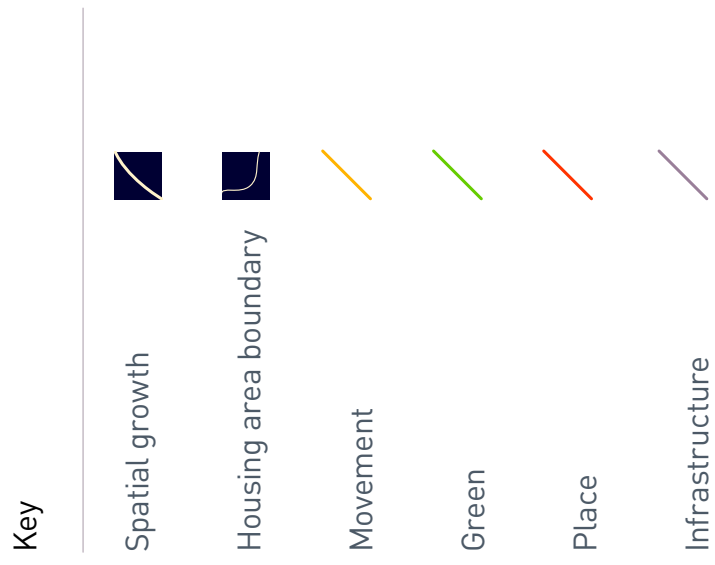
Place



Key	Existing	Proposed
Spatial growth areas		
Urban areas		
Housing area boundary		
Movement		
Green		
Place		
Infrastructure		







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Aberdeen City and Shire Strategic Development Plan

Equalities and Human Rights Impact Assessment

Introduction

Section 270B of the Town and Country Planning (Scotland) Act 1997, as amended, requires planning authorities to “*perform their functions...in a manner which encourages equal opportunities*”. This complements a range of legislation which applies to public and private sector organisations to underpin equal opportunities and human rights.

This Equalities and Human Rights Impact Assessment assesses the Proposed Aberdeen City and Shire Strategic Development Plan, with an overall purpose to help ensure that the plan does not discriminate and that where possible, opportunities are taken to promote equality, as well as all other human rights and good relations between groups.

The Aberdeen City and Shire Strategic Development Plan is an evolution of the Aberdeen City and Shire Structure Plan (2009). That plan went through an Equalities and Human Rights Assessments as well, so this assessment builds on that experience.

All Equalities and Human Rights Assessments involve three stages:

1. Assessing the impact of proposed or existing policies or functions on different equalities groups including collecting and analysing relevant data;
2. Consulting people who are likely to be affected by the policies; and
3. Reviewing and revising the policies in the light of the consultation and assessment.

An Equalities and Human Rights Assessment involves thinking through the potential consequences of policies and functions on both the identified equality target groups and society at large, making sure that as far as possible, any negative impacts are minimised or eliminated and that opportunities for promoting equality and respect for all other human rights are maximised. The assessment follows the process adopted by Aberdeen City Council.

Process

The initial (Stage I) assessment was conducted in September 2011 on basis of the changes envisaged to the Structure Plan by the Main Issues Report. The Main Issues Report and its Equalities and Human Rights Assessments were published for consultation on 7 October 2011.

Stage II (consultation) was undertaken between October and December 2011. No responses were received to the Equalities and Human Rights Assessments.

Stage III of the process was carried out in 2012 as the proposed strategic development plan was prepared. In late 2012 there will be further opportunity to respond to the proposed strategic development plan which will be accompanied by this revised Equalities and Human Rights Assessment.

The proposed strategic development plan will be submitted to Scottish Ministers in mid-2013 who will approve it with or without modifications on the basis of an ‘examination’ into unresolved objections. The impact of any modifications to the plan prior to approval would be for Scottish Ministers to determine. However, if modifications are proposed, the SDPA will request that this assessment is updated prior to approval.

STEP 1: Identify essential information

1. Name of proposal. Aberdeen City and Shire Strategic Development Plan

2. Officers completing this form.

Name	Designation	Service
David Jennings	Strategic Development Plan Manager	Aberdeen City and Shire SDPA

3. Date of Impact Assessment. 14 September 2012

4. When is the proposal next due for review? 2017

5. Committee Name. Aberdeen City and Shire Strategic Development Planning Authority

6. Date the Committee is due to meet. 21 September 2012

7. Identify the Lead Council Service and who else is involved in the delivery of this proposal. (for example other Council services or partner agencies)

Aberdeen City and Shire Strategic Development Planning Authority. Planning and Sustainable Development (Aberdeen City Council) as well as Planning and Building Standards (Aberdeenshire Council) will have a very important role to play in delivery, along with a range of other services of the two councils including housing, economic development and transportation. A range of partnerships, agencies and the private sector will also have an important role to play.

9. Please summarise this Equality and Human Rights Impact Assessment, (EHRIA). This must include any practical actions you intend to take / have taken to reduce, justify or remove any adverse negative impacts.

The assessment has identified positive impacts on a range of equality target groups, along with neutral impacts on the others.

10. Where will you publish the results of the Equality and Human Rights Impact Assessment?

- ✓ Full EHRIA will be attached to the committee report as an appendix.
- ✓ Full EHRIA will be available on the SDPA website, at all public libraries and on request from the SDPA.

STEP 2: Outline the aims of the proposal

11. What are the main aims of the proposal?

To guide the future development of Aberdeen and Aberdeenshire over the period to 2035 in such a way that it becomes more prosperous, sustainable and inclusive.

12. Who will benefit most from the proposal?

The strategic development plan is intended to be of benefit to the whole of Aberdeen City and Aberdeenshire - residents, businesses and visitors.

13. Tell us if and how the proposal will increase equality of opportunity by permitting positive action to redress disadvantage?

N/A

14. What impact will the proposal have on promoting good relations and wider community cohesion?

Sustainable mixed communities are a key component of the way development is envisaged to take place – meeting the needs of all in the community through the provision of a wide range of housing options with high quality design and community facilities.

STEP 3: Gather and consider evidence

15. What evidence is there to identify any potential positive or negative impacts in terms of consultation, research officer knowledge and experience, equality monitoring data, user feedback and other?

The 'Monitoring Statement' identifies the scale of change in Aberdeen City and Shire over recent years, particularly focusing on the targets in the current structure plan. This identifies the scale and nature of population growth across the area.

The 'Housing Need and Demand Assessment' seeks to identify the housing needs of the various equalities groups (p103 – 143). The assessment identified for example an increasing percentage of the population in older age groups (a 200% increase in the over 85 age group for example between 2008 and 2033) and that ethnic minorities generally require homes with more bedrooms. At the strategic planning level, need is generally generic (with a few exceptions), but it is at the delivery phase where services and individual products need to be tailored to the needs of individual groups. It will be local housing strategies in particular which focus on these individual needs but the role of the strategic development plan is to ensure that the framework is in place to be best able to meet these needs in land use planning terms. The main exception relates to the housing needs of the gypsy/traveller community which are already identified explicitly in the structure plan (and there is no proposal to change this). In a few cases, such as within the LGB community, it was not possible to identify particular housing needs due to a lack of data – but it is thought unlikely that any need would require a distinctive response in the strategic development plan.

Customer feedback has expressed concern over a lack of progress with dealing with the housing needs of the gypsy/traveller community on the ground. However, the structure plan saw local development plans as the main delivery mechanism to help meet their housing needs. Both local development plans have responded positively to this and have recently been adopted by both councils. As a consequence, it is too early to tell whether there is a need to change the framework in the strategic development plan. However, early indications are that sites are being identified through the masterplans prepared for some of the larger housing allocations in line with these policies.

No additional impacts were identified in response to the Equalities and Human Rights Impact Assessment published alongside the Main Issues Report in 2011.

Any unresolved objections to the Proposed Strategic Development Plan will be considered by an independent Reporter who will prepare a report with recommendations for Scottish Ministers outlining if and how the plan should be changed. Scottish Ministers are responsible for the final approval of the plan.

STEP 4: Assess likely impacts on equality strands

16. Which, if any, equality target groups and others could be affected positively or negatively by this proposal? Place the symbol in the relevant box.

(Positive +, neutral 0, - negative)

Equality Target Group					
Race*	+	Disability	+	Gender**	0
LGB***	0	Belief	0	Younger	+
Older	+	Others e.g. poverty	+/0		

* Race includes Gypsies/Travellers

** Gender includes women, men, Transgender

***LGB: Lesbian, Gay and Bisexual

17. Please detail the potential positive and/or negative impacts on the groups you have highlighted above? Detail the impacts and describe the groups affected.

Positive impacts (describe groups affected)	Negative Impacts (describe groups affected)
<p>Race – explicit recognition in the structure plan of the specific housing needs of gypsies / travellers. Improved quality of life and life chances in regeneration areas.</p> <p>Disability – increased accessibility to new developments by non-car modes of transport, including buses.</p> <p>Young People – economic growth will increase employment opportunities. Increased affordability and availability of housing. Increased accessibility of new development by non-car modes of transport. Reduced energy demand from new homes will reduce costs.</p> <p>Older People – Increased accessibility of new development by non-car modes of transport. Reduced energy demand from new homes will reduce costs.</p> <p>Other (poverty) – affordable housing for low income groups / young people / people with families / single people. Economic growth will increase employment opportunities. Regeneration will increase the quality of life and life chances.</p>	

STEP 5: Apply the three key assessment tests for compliance assurance

18. Does this policy/procedure have the potential to interfere with an individual’s rights as set out in the Human Rights Act 1998? State which rights might be affected by ticking the appropriate box(es) and how. **If you answer “no”, go to question 22.**

<input type="checkbox"/> Article 3 – Right not to be subjected to torture, inhumane or degrading treatment or punishment <input type="checkbox"/> Article 6 – Right to a fair and public hearing <input type="checkbox"/> Article 8 – Right to respect for private and family life, home and correspondence <input type="checkbox"/> Article 10 – freedom of expression <input type="checkbox"/> Other article not listed above
How?
No

Legality

19. Where there is a potential negative impact is there a legal basis in the relevant domestic law?

N/A

Legitimate aim

20. Is the aim of the policy a legitimate aim being served in terms of the relevant equality legislation or the Human Rights Act?

N/A

Proportionality

21. Is the impact of the policy proportionate to the legitimate aim being pursued? Is it the minimum necessary interference to achieve the legitimate aim?

N/A

STEP 6: Monitor and review

22. How will you monitor the implementation of the proposal? (For example, customer satisfaction questionnaires)

Monitoring is an essential part of the implementation of the strategic development plan. The plan will contain a monitoring framework directly related to the targets in the plan (as well as changes to the context) which will include a monitoring statement as well as more regular reports to the SDPA on individual subjects.

23. How will the results of this impact assessment and any further monitoring be used to develop the proposal?

This impact assessment has been produced to accompany the proposed strategic development plan, building on the assessment of the main issues report and previous structure plan. Any response to this assessment will be taken into account in the assessment of representations to the proposed plan itself. Any modifications to the proposed plan will be assessed by Scottish Ministers before approval of the strategic development plan (expected in late 2013 or early 2014). Monitoring will inform the review of the plan in 5 years' time.

Local development plans are a key means of implementing the strategic development plan. These are also subject to Equalities and Human Rights Assessments so the detailed implementation of the plan will also be subject to assessment by both Aberdeen City Council and Aberdeenshire Council.

STEP 7: SIGN OFF

The final stage of the EHRIA is formally to sign off the document as being a complete, rigorous and robust assessment. At this stage the assessment is interim in nature and subject to consultation.

Person(s) completing the impact assessment.

Name	Date	Signature
David Jennings	14 September 2012	

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	6 th November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	MUSIC (Mitigation in Urban Areas: Solutions for Innovative Cities) Project Update
REPORT NUMBER	EPI/12/219

1 PURPOSE OF REPORT

The purpose of this report is to provide Elected Members with a progress update on the MUSIC (Mitigation in Urban Areas: Solutions for Innovative Cities) Project and to solicit the active support of members in the Project.

2 RECOMMENDATION(S)

It is recommended that the Committee:

1. Notes the positive progress of the MUSIC project to date;
2. Approves the attendance of a Councillor to represent the Council at the MUSIC Partnership meeting and Midterm Conference in Ghent, 19th – 21st November 2012;
3. Approve the selected Councillor to continue to participate as a 'Frontrunner' for the duration of the project (to April 2014); and
4. Instruct officers to report back to this Committee with progress on the MUSIC Project twice annually, following the reporting schedule set by the EU fund.

3. FINANCIAL IMPLICATIONS

The MUSIC Project is part funded through the North West Europe Interreg IVB programme European Regional Development Fund (ERDF). The project's 'Lead Beneficiary' (Lead Partner) is the City of Rotterdam (NL) and ACC is a 'Beneficiary' (Partner), alongside the cities of Ludwigsburg (Germany), Montreuil (France) and Ghent (Belgium). In addition, ACC is supported within the project by DRIFT (Dutch Research Institute for Transitions) and the Henri Tudor Institute, Luxembourg.

Responsibility for co-ordinating ACC's MUSIC Project finances lies with the Partnerships, Projects and Funding Team. ACC's Corporate Accounting Manager is authorised and designated by the European Commission to act as ACC's 'First Level Controller' – ACC's accountability rests with that officer.

MUSIC Project – Budget (Cost Centre: C27124)

Aberdeen's total budget: €645,151 (£516,120 – exchange rate dependent)

The Aberdeen element of the MUSIC Project consists of a 50% contribution from ERDF, with a 50% matching contribution from ACC. The ACC match funding has been agreed from within existing budgets, the majority of which consists of officer time allocated to manage and deliver the project.

Finance update (to date of last ERDF claim – 30th June 2012):

Project spend to date: €163,667 (approx. £138,618 - exchange rate variable)

ERDF claimed to date: €81,834 (approx. £69,309 - exchange rate variable)

50% of the Project's overall final spend will be reclaimed from the ERDF.

Partner meeting and Mid term Conference costs

As Beneficiary, ACC is committed to the MUSIC partner meeting and midterm conference taking place in Ghent in late November 2012. Costs associated with this, including officer and Elected Member attendance, will be covered by the existing project budget.

Travel and accommodation arrangements are yet to be put in place for these events. They will include return flights and accommodation for 4 persons for 3 nights, estimated as: £3,500.

4. OTHER IMPLICATIONS

ACC will continue to benefit from the MUSIC Project through improved city wide engagement on the subject of Sustainability. ACC are being assisted in this 'Transition Management' process by a member of the DRIFT partnership, to trial a methodology which is increasingly recognised as a means of developing citywide low carbon strategies. In Sept 2012, EP&I Committee agreed to use the transition management process to develop a Smarter City strategy and action plan for Aberdeen.

The Transition Management approach engages key stakeholders (Frontrunners) to assist with Aberdeen's transition towards a low carbon economy, thereby helping to meet ACC's '*Public Bodies Climate Change Duties*' under the *Climate Change (Scotland) Act 2009* and reduce the risks to Aberdeen associated with future climate change.

Our Geographical Information Systems data is directly benefiting from increased investment, as well as working in collaboration with the Henri Tudor Institute, a disciplinary expert body based in Luxembourg. It is the project's aim to make GIS technology and applications more user-friendly and a tool for public consultation on city issues, i.e. addressing fuel poverty.

Furthermore, photo voltaic (solar) panels have been installed on Loriston Primary School in Cove as a demonstration project to identify the benefits of making energy efficiency measures at the heart of a community. Installation of

these panels also help to reduce our energy use and our reliance on fossil fuels – thereby reducing annual energy costs and carbon emissions.

5. BACKGROUND/MAIN ISSUES

5.1 MUSIC Project Background

The MUSIC Project is a four year European project, running from April 2010 to June 2014.

The MUSIC-partnership is composed of five frontrunner-cities; Lead Partner Rotterdam, and Ghent, Ludwigsburg, Montreuil and Aberdeen, along with two renowned knowledge institutions, DRIFT on governance methodology and CRP Henri Tudor on Geographical Information Systems (GIS).

The aim of the MUSIC Project is to stimulate actions to reduce CO₂ emissions in urban areas by 50% by 2030 and thereby create solutions for innovative cities.

New techniques and measures to reach the EU-climate targets of 20% CO₂ reduction in 2020 are increasingly available and implemented. However, to mainstream these techniques in urban policy and consider them as “business as usual”, techniques is not enough. A transition towards innovative thinking about urban planning is needed in which all stakeholders are involved.

Aberdeen’s contribution to the MUSIC Project is co-ordinated by a dedicated Project Manager based within the Environmental Policy team. The project is split into three distinct work packages, which are supported by officers with expertise in the relevant fields. Details of the Work Packages (WP) and supporting teams are outlined below:

5.2 WP1 – Transition Management

5.2.1 WP1 Outline

The project’s principle objective is to mobilise stakeholders to take action towards CO₂ reduction through the development of a transnationally applicable Transition Management (TM) Strategy. TM is an innovative approach to changes in society, based on a multi-stakeholder learning process – searching for tangible small scale solutions to big problems. In MUSIC, these stakeholders will include local governments, hospitals, school boards, businesses, building owners and energy agencies.

Each partner in the MUSIC project is conducting a local consultation process using the TM methodology. This consists of hosting a series of ‘Arena’ meetings where the role of the project team is to facilitate discussions and observe TM is based on the assumption that transitions can never be completely controlled but can be steered - by influencing, adapting and monitoring – the frontrunner workshops will lead to a shared vision, action agenda and concrete actions on CO₂ reduction.

Summary of the TM process

1. Preparation & Exploration – form the TM team by identifying and approaching frontrunners
2. Problem structuring & Envisioning – the first arena meetings take place where the city's issues are considered and priorities for change identified. Develop a set of guiding principles and form a shared vision for the city's sustainable future
3. Backcasting, Pathways and Agenda Building – Identify the transitions that will need to be experienced via specific targeted actions
4. Experimenting and Implementing
5. Monitoring & Evaluation

ACC's MUSIC Project Manager delivers WP1, including Chairing the TM Arenas. Their work is actively supported by the Dutch Research Institute for Transitions (DRIFT), Rotterdam.

5.2.2 WP1 progress to date

Five TM Arenas have been successfully completed with our Front Runners, including one introductory session, preparation and exploration of city sustainability issues and most recently, envisioning what kind of city our participants want to live in by their target year of 2030. The group have established a list of 'Guiding Principles'; themes that identify the main challenges that face the city, these are:

By 2030, we are living in a(n):

- Opportunity City
- Attractive City
- Learning City
- Energy Efficient & Resilient City
- Accessible City

5.2.3 WP1 next steps

The next meetings of the group will consider 'Backcasting' – that is identifying the various stages of progress that will need to take place between now and 2030 before the sustainable future can be realised. This process is complimented by the building of 'Pathways' that identify who the responsibility for these tasks should lie with. This is then followed by a package of wider dissemination of the group's findings, a broad publicity campaign and expansion of the network.

5.3 WP2 – Geospatial Urban Energy and Support System

5.3.1 WP2 Outline

Integrating energy issues in urban planning will be progressed through the development of a Geospatial Urban Energy Information and Support System. This Geographical Information System (GIS) based demonstration platform will allow cities to form energy maps and create scenarios that will observe opportunities for CO₂ reduction in urban planning. The system will facilitate transnational comparisons and strengthen joint strategies for the future of this technology.

ACC's MUSIC Project Manager co-ordinates delivery of WP2, with active support from our GIS Team and the Henri Tudor Public Research Centre, Luxembourg.

5.3.2 WP2 progress to date

Working with the Henri Tudor Institute in Luxembourg, the GIS team has been using a system called iGuess to create energy maps of the project cities. In essence, these maps will show not only where energy projects are taking place, such as Combined Heat and Power (CHP) facilities, but it will identify the energy potential of the city based on solar exposures, thermal energy and wind power. This can be used as an engagement tool throughout the community to inform businesses and householders how to best harness the energy potential of their property.

5.3.3 WP2 next steps

The three WPs of the project will start being combined in the upcoming TM Arenas. This will involve inviting the GIS coordinator to the arenas to demonstrate the technology and how it can be used to create visualisations of different scenarios and working with the school that has had PVs installed to undertake a similar process with their pupils about energy/sustainability issues.

In Aberdeen we aim to create a map showing spots of 'Energy Poverty' – where the housing stock is of insufficient quality and the heating demands outweigh the income of the household.

5.4 WP3 – Innovative Pilots

5.4.1 WP3 Outline

The MUSIC Project also allows for an innovative pilot test of the tools and strategies developed through the work explained above. In Aberdeen's case, this will consist of an energy efficiency measure being made (installation of photo-voltaic (PV) panels) at the centre of a local community (Loirston Primary) where stakeholder involvement can be witnessed and energy data gathered and stored on the GIS systems.

ACC's MUSIC Project Manager co-ordinates delivery of WP3, with active support from our Energy Management Team.

5.4.2 WP3 progress to date

Loirston Primary School was identified as the community centre where the energy efficiency investment would be made. The site was selected on the basis of its solar exposure and building fabrication. The contracts for installation of the PVs was concluded in March 2012 and work got underway the following month. The PVs have now been installed.

5.4.3 WP3 next steps

There will be an education package associated with the PV installation delivered through the school in order to engage with the pupils, their parents and the school itself – ACC will work with 'Our Generation' to deliver this.

5.5 MUSIC Project management

5.5.1 Next steps

Having submitted four progress reports to the North West Europe Programme's secretariat, the project has been commended for the activities conducted by the partnership.

The MUSIC Project is currently midterm and while much has been achieved, there remains a significant amount to deliver. Details of actions for the forthcoming period are outlined below:

Project meeting and Midterm Conference, Ghent, Belgium (details below);

Continue to recruit Front Runners to Transition Management process;

Coordinate publicity & promotion of project activities and findings;

2013 – Project meeting, Montreuil, France;

2013 – Monitoring and evaluating of MUSIC Project in Aberdeen

5.5.2 MUSIC Project meeting & Midterm Conference, Ghent, Belgium

The event will take place over three days and will consist of the following:

Days 1 & 2: 19th & 20th November – Project partners meeting

Project partners will present on the progress made within their projects and the plans they have for the ensuing six months. There will be focus groups to allow the GIS representatives to have technical discussions, whilst the remaining project members will consider common issues pertaining to the Transition Management process. There will also be time attributed to discussing project management and finance issues.

Day 3: 21st November – Midterm Conference

A free and open invite Conference where the cities will share their results, discuss their findings and consider the process that got them where they are today.

The Conference will focus on the following questions:

- How to create a common understanding of problems and potential for change?
- How to mobilise different types of actors in the city to work towards a sustainable future?
- How to get support from within your organisation for an innovative approach to reduce CO₂ emissions?
- How to use “hard” assessment tools to support “soft” participation processes?

6. IMPACT

6.1 Corporate Impact:

The MUSIC Project and associated recommendations in this report aim to assist with delivery of the following corporate commitments:

Aberdeen City Council Vision Statement - Aberdeen: A Smarter City...

- We will define the image of an international 21st century energy city, leading a new leaner, cleaner industrial revolution using the intensity of our social, business and community connections.
- At the centre of the vision lies the concept of 21st century sustainable living. This will require smarter connections both within Aberdeen and to the world beyond. We will take a European lead in adapting new transport and communication technologies to make this possible.

High-level priorities:

SMARTER GOVERNANCE (Participation)

- We will encourage and support citizens to participate in the development, design and decision making of services to promote civic pride, active citizenship and resilience.

SMARTER ENVIRONMENT (Natural Resources)

- We will design and construct all new infrastructure to be energy efficient by maximising the use of low carbon technology and materials. We will use recycled materials where appropriate.
- We will increase energy efficiency and introduce carbon reduction measures in our processes and our housing and non housing assets to reduce our carbon footprint, save money and to bring people out of fuel poverty.

SMARTER MOBILITY (Transport and ICT)

- We will provide and promote a sustainable transport system, including cycling, which reduces our carbon emissions.

Community Plan & Single Outcome Agreement (SOA) Local Outcomes:

- The impact of council activities on the environment is minimised, including a target of being carbon neutral overall by 2020 and a commitment that all new council developments will be carbon neutral.
- Minimise the environmental impact of transport on our community and the wider world.

6.2 Public Impact:

The MUSIC Project and the recommendations in this report relate to strategic research and discussions work at this stage, as opposed to implementing direct change. Therefore, an Equalities and Human Rights Impact Assessment (EHRIA) has not been carried out at this stage. Any subsequent implementation work may be subject to an EHRIA, where appropriate.

7. BACKGROUND PAPERS

Website: www.themusicproject.eu/

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE:	Environment, Planning and Infrastructure
DATE:	6th November 2012
DIRECTOR:	Gordon McIntosh
TITLE OF REPORT:	Various small scale traffic management and development associated proposals (New Works)
REPORT NUMBER:	EPI/12/230

1. PURPOSE OF REPORT

This report is to advise Committee of the need for various small scale traffic management measures identified by officers, residents, local members, emergency services, etc and verified as necessary through surveys by officers. It also brings forward proposals associated with new developments as part of the development management process. In addition to these measures, this report also includes proposals for individual parking spaces for Blue Badge holders which now require to be progressed through the normal legal process for the required Traffic Regulation Order.

2. RECOMMENDATION(S)

That the Committee:

1. Approve the proposals in principle.
2. Instruct the appropriate officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required as described in this report. If no significant objections are received, then progress with the public advertisement and report the results to a future meeting of this Committee.
3. Instruct the appropriate officers to commence the combined statutory consultation for the traffic regulation order for the list of Blue Badge parking spaces and report back to a future meeting of this Committee.

3. FINANCIAL IMPLICATIONS

The current Five Year Business Plan has identified savings from the Road Safety and Traffic Management budget. There has also been a comprehensive review of the Capital Plan which will result in proposals having to await funding for implementation.

Budget	Implementation costs (£)	Additional maintenance costs (£) after 5 years	Comments
(●) Cycling, Walking, Safer, Streets (Scot Gov grant-funded)	2650	1875	If budgets are not currently available locations will be placed on a priority list for when future funding becomes available
(❖) Developer financed	-	8950	Maintenance of these works generally falls to the council maintenance budget when they are on-street restrictions
(➤) Disabled Parking	3300	4000	Some of these spaces will require to be relined approximately every 10 years at a cost of £100 per space and some will require removal before this time at a cost of £108 per space.

4. OTHER IMPLICATIONS

There is a risk that any approved traffic regulation orders may have to re-enter the legislative process if they are unable to be implemented within the statutory implementation time of 2 years from the start of public consultation if funding is insufficient.

5. BACKGROUND/MAIN ISSUES

There are 7 traffic management proposals for locations brought forward during the course of routine examination of road safety and traffic flows and 3 proposals resulting from requests from developers.

Key:
<ul style="list-style-type: none"> ○ Funded from the Cycling, Walking, Safer Streets grant funded budget ❖ Funded by the developer ➤ Funded from the current Disabled Parking revenue budget ○ No funding required

The following proposals will be funded from Cycling, Walking, Safer Streets budget

Braeside Place – Proposed “One-way” restriction from north to south

Braeside Primary school has, over an extended period of time, been used to house primary schools temporarily whilst new accommodation was being built. The one-way was required as pupils were being driven from their old school to Braeside primary (busses/private cars) and Braeside Place was considered too narrow to accommodate the increased traffic around the school.

A temporary One-way was arranged and has been in place for some time now. However during this period it was indicated to officers that some residents would prefer the one-way restriction to become permanent although others were keen to see the temporary restriction returned to the original two-way system.

A questionnaire was delivered to all the residents of Braeside Place asking them to indicate their preference. The returns were conclusive with the majority (almost 2-1) in favour of maintaining the one-way system.

This proposal is submitted for approval to start the legal process to make the One-way a permanent restriction.

The proposals are indicated on the plan below.

Implementation cost - £nil

Estimated additional maintenance cost – no additional costs

Ward (11) – Airyhall/Broomhill/Garthdee

Elected members – Taylor, Townson, Yuill

Garvock Wynd – Proposed “At any time” waiting restrictions

Follow reports from the City Wardens covering that area detailing the parking patterns now regularly in place, it is clear that waiting restrictions are now required to allow access, both pedestrian and vehicular, along this narrow street. Previously vehicles would only park on one side however they are now parking on both sides and totally blocking the footway on the north side.

It is now proposer to put “At any time” waiting restrictions along the full length of the north side to maintain pedestrian access to the footway.

The proposals are indicated on the plan below.

Implementation cost - £750

Estimated additional maintenance cost – £750 every 5 years

Ward (8) – George Street/Harbour

Elected members – May, Jean Morrison, Nathan Morrison

Girdleness Road – Proposed relocation of “School Keep Clear” zig-zags

Currently there are two sets of “School Keep Clear” zig-zags outside Tullos Primary school with a small unrestricted length of kerbside between them, and a set of zig-zags associated with the Zebra crossing adjacent to the school.

There is a short section of carriageway where two sets of zig-zags are overlapping that requires to be tidied up and the short section of unrestricted kerbside between the School Keep Clear zig-zags is being used by parents/carers to park on whilst the pupils are being taken into the school. This parking is not welcome at this location and it is proposed to relocate one set of “School Keep Clear” zigzags to cover this gap and also tidy up the restrictions adjacent to the zebra crossing.

The proposals are indicated on the plan below.

Implementation cost - £600

Estimated additional maintenance cost – £500 every 10 years

Ward (12) – Torry/Ferryhill

Elected members – Allan, Dickson, Donnelly, Kiddie

Maberly Street / George Street – Proposed loading restrictions; Mon – Fri, 7:00am to 9:00am and 4:00pm to 6:00pm

Maberly Street is a relatively narrow street which takes a large volume of traffic at peak times, both vehicular and pedestrian, and is a street regularly used by the emergency services.

On the corner of Maberly Street there is a Public House which takes regular weekly deliveries of beer/drinks that takes some considerable time to complete. The lorry making these deliveries consistently parked adjacent to the stop line associated with the traffic signals. They also carried this activity out during the peak traffic flow times, generally 7:00 – 7:30am, creating a dangerous situation.

The delivery company was advised by Grampian Police to deliver outwith the peak vehicle flow times, which are generally 7:00am to 9:00am and 4:00pm to 6:00pm at this location, or to off-load their goods from George Street and trolley them to the cellar entrance.

The company adhered to this advice for some time however, they have recently reverted to their previous practice which is not acceptable in any way.

Currently there are “At any time” waiting restrictions on all roads leading from this junction, however it is now proposed to implement a loading ban, mainly on Maberly Street but also into George Street a short distance, to allow the City Wardens to keep these delivery vehicles away from the signaled junction.

The extent of the proposals is indicated on the plan below.

Implementation cost - £800

Estimated additional maintenance cost – £300 every 10 years

Ward (8) – George Street/Harbour

Elected members – May, Jean Morrison, Nathan Morrison

Maryville Place – Proposed “At any time” waiting restrictions

Some time ago drivers of the waste collection vehicles reported that residents’ vehicles were being parked in the turning head of this small cul-de-sac and causing the refuse vehicles to reverse into the cul-d-sac from North Anderson Drive and along the length of the road. This is not an ideal situation from a road safety point of view.

Letters were delivered to these properties asking that they cease from this practice especially on bin collection days and for a period of time the situation improved. However, the parking has now recommenced and we now proposed to put “At any time” waiting restrictions around the turning head to resolve this issue.

The proposals are indicated on the plan below.

Implementation cost - £350

Estimated additional maintenance cost – £350 every 10 years

Ward (10) – Hazlehead/Ashley/Queens Cross

Elected members – Corall, Greig, Stewart, Thomson

Union Street / Crown Street junction – Proposed alteration to Traffic Regulation Order to exempt winter maintenance vehicles from the right turn restriction

It has recently been highlighted that the Traffic Regulation Order, restricting the right-turn movements for all vehicles except buses from Union Street into Crown Street, does not have within it an exemption for winter maintenance vehicles to allow access to clear snow/grit etc. during periods of adverse weather.

These snow clearing and gritting operations are mainly carried out overnight or in the early hours of the day when traffic is very light, especially in these conditions.

The alternative route for these operations is not reasonably accessible and could, in practical terms, result in a reduced frequency of visits over that section of carriageway.

It is therefore proposed that winter maintenance vehicles be exempt from the right-turn restriction (as the buses currently are) in order to carry out these road safety operations.

Implementation cost - £nil
Estimated additional maintenance cost – £nil

Ward (12) – Torry/Ferryhill
Elected members – Allan, Dickson, Donnelly, Kiddie

Westburn Road – Proposed extension of “At any time” waiting restrictions

It has been reported, and subsequently confirmed, that the visibility to the right when exiting the eastern-most access to Aberdeen Royal Infirmary, could be improved.

It is therefore proposed to remove two Pay and Display parking spaces, approx 10m) from the north side of Westburn Road and replace them with “At any time” waiting restrictions. Visibility should be noticeably improved with this modification.

The proposals are indicated on the plan below.

Implementation cost - £150
Estimated additional maintenance cost – £50 every 5 years

Ward (7) – Midsocket/Rosemount
Elected members – Cormie, Forsyth, Laing

The following proposals will be funded by the developer

❖ **Earlspark Avenue** – Proposed 20mph with speed tables.

As a result of a further residential development proposal to the north of Earlspark Avenue, a condition was set within its planning consent that residents were to be consulted on a proposal to install traffic calming speed tables on Earlspark Avenue.

This survey was carried out and the result revealed that the overwhelming majority of residents who replied were in favour of the traffic calming measures being promoted.

It is therefore proposed that a 20mph speed restriction be promoted with traffic calming speed tables incorporated.

The proposals are indicated on the plan below.

Implementation cost - £nil
Estimated additional maintenance cost – £150 every 5 years

Ward (9) – Lower Deeside
Elected members – Boulton, Malik, Malone

- ❖ **Mugiemoss, Davidson Mills** – Proposed “At any time” waiting restrictions, “One-way” restrictions, “No right turn”, “No left turn” and “No U-turn” restrictions, “disabled parking bays” and “Stopping up” orders.

As a part of the planning application for this industrial/retail/residential development, several traffic movement and parking/waiting restrictions are required to be promoted for both safety and traffic management reasons.

The proposals are indicated on the plans below.

Plan 1 – Proposed “One-way”, “No right/left turn” and “No U-turn” restrictions

Plan 2 – Proposed “Stopping up” (1)

Plan 3 – Proposed “At any time” waiting restrictions

Plan 4 – Proposed “Disabled parking bays”

Implementation cost - £nil

Estimated additional maintenance cost – £5000 every 5 years

Ward (1) – Dyce/Bucksburn/Danestone

Elected members – Crockett, Lawrence, MacGregor, Samarai

- ❖ **Wellington Road, Cove** – Proposed “At any time” waiting restriction, “One-way” restriction, “No right/left turn” restriction, and “Prohibition of Driving” order

As part of the Planning application for this residential development, traffic movements and waiting restrictions are required to be promoted for both road safety and traffic management reasons

The proposals are indicated on the plans below.

Plan 1 – Proposed “At any time” waiting restriction, “One –way” restriction and “No right/left turn” restriction

Plan 2 – Proposed “Prohibition of driving” restriction

Implementation cost - £nil

Estimated additional maintenance cost – £3800 every 5 years

Ward (13) – Kincorth/Loirston

Elected members – Cooney, Finlayson, McCaig

The following proposals will be funded from the Disabled Parking Revenue budget

➤ **Disabled parking bays to be provided through the Disabled Persons Parking Places (Scotland) Act 2009**

There is one application to be considered at this meeting.

(Plans are not included as, under normal circumstance, are located close to the properties.)

On-street parking – 28 spaces

27 Berrywell Gardens	26 School Drive
62 Gladstone Place, Harlaw	121 School Drive
11 Osborne Place	89 Bankhead Avenue
29 Hilton Terrace	95 Bankhead Avenue
105 Western Road, Woodside	14 Sunnyside Gardens
8 Kepplehills Drive	85 Crown Crescent, Culter
9 Swannay Road	448 North Anderson Drive
5 Seaton Road	14 Faulds Wynd
240 Victoria Road	49 Tollohill Gardens
226 Victoria Road	16 Seaforth Road
11 Manor Walk	38 Dulnain Road
175 Hilton Avenue	442 Clifton Road
14 Bedford Avenue	21 School Terrace
33 Aboyne Road	Duff Street

Off-street parking – 12 spaces

Mugiemoss, Davidson Mills – 7 spaces	3a Kings Crescent
1 Overhills Walk	3 Craigielea Avenue
34 Garnitehill House, Marchburn Drive	13 Findon Ness (o/s No 17)

6. IMPACT

This report meets with the local Community Plan objectives to continually improve road safety and maximise accessibility for pedestrians and all modes of transport.

The proposals are in line with the Councils Transportation Strategy to improve safety for all road users by continuing to reduce the number of casualties in traffic collisions.

This report is likely to be of interest to the public in the streets affected by the proposals.

There is no Equality and Human Rights Impact Assessment required as this report only recommends that these proposals progress to the Statutory Consultation process therefore there will be no changes effected as a result of the recommendations being approved by the Committee

7. BACKGROUND PAPERS

N/A

8. REPORT AUTHOR DETAILS

Ruth Milne
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(01224) 538052

Consultees comments

Enterprise, Planning and Infrastructure Committee

Convener: Councillor Barney Crockett – has been consulted

Vice Convener: Councillor Ramsay Milne – has been consulted

Councillor George Adam – has been consulted

Councillor Yvonne Allan – has been consulted

Councillor Kirsty Blackman – has been consulted

Councillor Marie Boulton – has been consulted

Councillor David Cameron – has been consulted

Councillor Scott Carle – has been consulted

Councillor Neil Cooney – has been consulted

Councillor John Corall – has been consulted

Councillor Bill Cormie – has been consulted

Councillor Steve Delaney – has been consulted

Councillor Graham Dickson – has been consulted

Councillor Alan Donnelly – has been consulted

Councillor Jackie Dunbar – has been consulted

Councillor Lesley Dunbar – has been consulted

Councillor Andrew Findlayson – has been consulted

Councillor Fraser Forsythe – has been consulted

Councillor Gordon Graham – has been consulted

Councillor Ross Grant – has been consulted

Councillor Martin Greig – has been consulted

Councillor Len Ironside – has been consulted

Councillor Muriel Jaffrey – has been consulted

Councillor James Kiddie – has been consulted

Councillor Jenny Laing – has been consulted

Councillor Graeme Lawrence – has been consulted

Councillor Neil MacGregor – has been consulted

Councillor M Tauqeer Malik – has been consulted

Councillor Aileen Malone – has been consulted

Councillor Andrew May – has been consulted

Councillor Callum McCaig – has been consulted

Councillor Jean Morrison – has been consulted and has no comments

Councillor Nathan Morrison – has been consulted

Councillor Jim Noble – has been consulted

Councillor John Reynolds – has been consulted

Councillor Gill Samarai – has been consulted

Councillor Jennifer Stewart – has been consulted

Councillor Sandy Stuart – has been consulted

Councillor Angela Taylor – has been consulted

Councillor Ross Thomson – has been consulted

Councillor Gordon Townson – has been consulted

Councillor Willie Young – has been consulted

Councillor Ian Yuill – has been consulted

Council Officers

Barry Jenkins, Head of Finance, Resources – *has been consulted and has no comments on this report*

Jane MacEachran, City Solicitor, Continuous Improvement - *has been consulted*

Ciaran Monaghan, Head of Service, Office of Chief Executive - *has been consulted*

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure - *has been consulted*

Hugh Murdoch, Head of Service, Shelter and Environment – *has been consulted*

Margaret Bochel, Head of Planning & Infrastructure, Strategic Leadership – *has been consulted and has no comments on this report*

Mike Cheyne, Roads Manager - *has been consulted*

Neil Carnegie, Community Safety Manager - *has been consulted*

Dave Young, Account Manager, Service, Design and Development - *has been consulted*

Laura Watson, Service Co-ordinator

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	6th November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Justice Mill Lane – Traffic Management Proposals Initial Statutory Consultation
REPORT NUMBER	EPI/12/221

1. PURPOSE OF REPORT

To advise the Committee of the results of the initial statutory consultation on the proposed traffic management scheme on Justice Mill Lane and the request to include a cycling provision.

2. RECOMMENDATION(S)

That the committee acknowledges the responses received as a result of the statutory consultation and hence instructs the appropriate officials to progress to public advertisement and report the results to a future Committee.

3. FINANCIAL IMPLICATIONS

Developer contributions are available for the implementation of the proposed traffic management scheme and subject to the preparation of an appropriate detailed design would fund the works. The proposed improvements whilst enhancing the environment will potentially reduce future road revenue maintenance costs.

A preliminary design and estimation for the traffic management proposals on Justice Mill Lane has now been completed, with the estimated costs being £210,000. This cost does not include the signalised crossing on Holburn Street which is estimated at £35,000. Therefore the total cost of the works is £245,000.

4. OTHER IMPLICATIONS

None

5. BACKGROUND/MAIN ISSUES

5.1 Report

At its meeting on 13th September 2011 the Enterprise, Planning and Infrastructure Committee considered a previous traffic management proposal that included a one-way system on Justice Mill Lane and at its meeting it was resolved to revise the design to retain two-way traffic.

A preliminary design was further submitted to Committee at its meeting on 31st May 2012 at which the Committee resolved to approve the preliminary scheme on Justice Mill Lane and to commence the required legal process for the necessary Traffic Regulation Order.

5.2 Proposals

The proposed scheme for Justice Mill Lane includes a revision of parking restrictions, traffic calming measures, realignment of kerb lines, a right-turn ban when exiting Justice Mill Lane westbound into Holburn Street and the upgrading of the footway to provide a 2m minimum width. In order to maintain two-way traffic and a standard 2m wide footway it has been necessary to include a give and take priority section where there is limited road space for two vehicles to pass and this effectively enhances the traffic calming element of the design. A controlled pedestrian crossing is also proposed on Holburn Street as part of the scheme to the north of Justice Mill Lane and would be seen to improve pedestrian access and safety. A plan of the proposed traffic management proposal is attached in Appendix A.

5.3 It is recommended this Committee acknowledges the responses received as a result of the Statutory Consultation and thereafter instructs officers to progress to the public advertisement stage and report the results to a future committee.

5.4 Provision for Cyclists

At its meeting on 31st May 2012 Committee there was a request raised by Councillor Yuill to include the provision for the separation of cyclists in both directions within the detailed design. A subsequent meeting was held with officers and Councillor Yuill explaining that there is insufficient road space to accommodate a segregated cycle provision and cyclists will require to operate within the controlled traffic management scheme with all other road users. Following discussion Councillor Yuill accepted that adequate provision had been made for cyclists.

6. IMPACT

Within the Community Plan, City Centre redevelopment is identified as a strategic priority, and within the Single Outcome Agreement, two national outcome objectives identified are that “We live in well designed, sustainable places” and that “We value our natural and build environment and protect it and enhance it for future generations.”

7. BACKGROUND PAPERS

Minute of Enterprise Planning and Infrastructure Committee meeting
13th September 2011

Minute of Enterprise Planning and Infrastructure Committee meeting
31st May 2012.

8. REPORT AUTHOR DETAILS

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Table 1
Initial Statutory Consultation

<u>Consultee</u>	<u>Response</u>
Grampian Police	No issues from a Police perspective regarding this proposal.
Freight Transport Association	Has been consulted 21/09/12
Scottish Ambulance Service	Has been consulted 21/09/12
Stagecoach Bluebird	Has been consulted 21/09/12
Public Transport Unit	Has been consulted 21/09/12
The Access Panel	Has been consulted 21/09/12
Grampian Fire & Rescue Service	"As the fire service is always keen to support any scheme to enhance road safety, we therefore offer no objection to these proposals."
First Aberdeen Ltd	Has been consulted 21/09/12
RAC Foundation	To be no longer consulted
Federation of Small Businesses	Has been consulted 21/09/12
Road Haulage Association	Has been consulted 21/09/12
Aberdeen Cycle Forum	"Cycling by Design states that in all cases traffic management schemes should permit all cycle movements to ensure full permeability and to maintain direct access. The default position should be cyclist's exemptions from turning right. ACF believes the default position of a cycle exemption from the right turn ban should be adopted here, with monitoring for the first year to assess if any issues arise."
NESTRANS	Nestrans does not have any formal comments on the proposals on Justice Mill Lane as this not considered to be part of the strategic road network, although the potential road safety benefit are welcomed.
Ferryhill Community Council	Has been consulted 21/09/12
Ashley & Broomhill Community Council	Has been consulted 21/09/12
Queens Cross / Harlaw Community Council	Has been consulted 21/09/12
Councillor Jennifer Stewart	Has been consulted 21/09/12
Councillor Martin Greig	Has been consulted 21/09/12
Councillor John Corall	Has been consulted 21/09/12
Councillor Ross Thomson	Has been consulted 21/09/12
Councillor Yvonne Allan	Has been consulted 21/09/12
Councillor Graham Dickson	Has been consulted 21/09/12
Councillor Alan Donnelly	Has been consulted 21/09/12
Councillor James Kiddie	Has been consulted 21/09/12

Consultees comments

Councillor Alan Donnelly – *has been consulted 02/10/12*
Councillor Graham Dickson – *has been consulted 02/10/12*
Councillor James Kiddie – *has been consulted 02/10/12*
Councillor Jennifer Stewart – *has been consulted 02/10/12*
Councillor John Corall – *has been consulted 02/10/12*
Councillor Martin Greig – *has been consulted 02/10/12*
Councillor Ross Thomson – *has been consulted 02/10/12*
Councillor Yvonne Allan – *has been consulted 02/10/12*

Council Officers

Barry Jenkins, Head of Finance, Corporate Governance – *has been consulted and has no comments from Finance*
Jane MacEachran, Head of Legal and Democratic Service – *has been consulted*
Ciaran Monaghan, Head of Service, Office of Chief Executive – *has been consulted*
Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – *has been consulted*
Hugh Murdoch, Head of Asset Management and Operations, E,P and I – *has been consulted*
Margaret Bochel, Head of Planning & Sustainable Development – *has been consulted and is in agreement with the comments of this report.*
Mike Cheyne, General Manager, Operations – *has been consulted*
Neal Carnegie, Community Safety Manager – *has been consulted*
Dave Young, Account Manager, Corporate Governance – *has been consulted*
Laura Watson, Service Co-ordinator E P & I
Mark Masson, Committee Services Officer

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Director of Corporate Governance
TITLE OF REPORT	Traffic Orders at the Final Stage of the Statutory Process
	(i) The (A96 / Old Meldrum Road / Mugiemooss Road, Aberdeen) (Redetermination of Means of Exercise of Public Right of Passage) Order 201()
	(ii) The Aberdeen City Council (North Grampian Circle, South Grampian Circle, Aberdeen) (One-Way) Order 201()
REPORT NUMBER:	CG/12/112

1. PURPOSE OF REPORT

This report deals with two orders at the final statutory stage; that is to say, the main statutory advertisement period is now over in respect of each of these orders and this report presents the objections (where relevant) in each case. The public notices are attached, from which members will be able to see the exact content of the proposals.

2. RECOMMENDATION(S)

It is recommended that the Committee:-

- (a) approve The Aberdeen City Council (A96 / Old Meldrum Road / Mugiemooss Road, Aberdeen)(Redetermination of Means of Exercise of Public Right of Passage) Order 201(), and agree that this order be made as originally envisaged; and
- (b) approve The Aberdeen City Council (North Grampian Circle, South Grampian Circle, Aberdeen) (One-Way) Order 201(), and agree that this order be made as originally envisaged.

3. FINANCIAL IMPLICATIONS

The proposals contained in the first order will be fully funded by CWSS budget, whilst the second order will be fully funded from the Traffic Calming and Road Safety budget for 2012 – 2013.

4. OTHER IMPLICATIONS

There are no other implications worthy of being identified in the abstract here, although, again, both Section 5 and the appendix rehearse concerns raised by objectors.

5. BACKGROUND/MAIN ISSUES

This section has been sub-divided into sub-sections corresponding to the two orders under consideration.

5.1 **(A96 / Old Meldrum Road / Mugiemooss Road, Aberdeen) ((Redetermination of Means of Exercise of Public Right of Passage) Order 201()**

- 5.1.1 Members will recall that at previous meetings it considered proposals to redetermine the means of exercise of the public right of passage over certain lengths of the A96, Old Meldrum Road and Mugiemooss Road so that these lengths of road become cycle track to be used by persons on foot and pedal cycle only. At the time of the first proposal was advertised, no objections were received, however on advertising the second section one objection was received. This objection was reported to the Committee and on consideration it was agreed that in accordance with The Stopping Up of Roads and Private Accesses and the Redetermination of Public Rights of Passage (Procedure) (Scotland) Regulations 1986, that the matter be referred to the Scottish Ministers for determination.

Subsequent to the latter decision, a technical issue with the initial notice was identified and therefore it was deemed necessary for the proposal to redetermine all of the lengths of the A96, Old Meldrum Road and Mugiemooss Road to be re-advertised thus providing a new opportunity for objections to be submitted. During this period no new objections were received. The previous objector was written to and advised of the position, in particular of the new objection period and that she required to confirm whether she wished to object in the same terms or for her original objection to remain. The objection period has now concluded and the previous objector has not confirmed that she wishes her objection to proceed and therefore on that basis no objections have been received.

5.2 **The Aberdeen City Council (North Grampian Circle, South Grampian Circle, Aberdeen) (One-Way) Order 201()**

- 5.2.1 No statutory objections have been received.

6. SERVICE AND IMPACT

Section 5 above – and also the public notices attached – will allow members to consider the possible impact on communities compared with the intended virtue of the original proposals.

7. BACKGROUND PAPERS

No background papers were used as a point of departure for writing this report.

The statutory advertisements are published here for information, allowing members to see the import of each order as advertised.

8. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

ROAD SCOTLAND ACT 1984

**THE ABERDEEN CITY COUNCIL
(A96 / OLD MELDRUM ROAD / MUGIEMOSS ROAD, ABERDEEN)(REDETERMINATION OF MEANS OF
EXERCISE OF PUBLIC RIGHT OF PASSAGE) ORDER 201()**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Scotland Act 1984. The effect of this order will be to redetermine the means of exercise of the public right of passage over certain lengths of the A96, Old Meldrum Road and Mugiemoos Road, detailed in the Schedule below so that these lengths of road become cycle track to be used by persons on foot and pedal cycle only.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 22 September to 23 October 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen. It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 22 September to 23 October 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

20 September 2012

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

SCHEDULE

INTERPRETATION - In this Schedule:

"Plan 01" means the plan numbered ACC/TRO/D5299/03 and "Plan 02" means the plan numbered ACC/TRO/D5299/04 both entitled "The Aberdeen City Council (A96 Great Northern Road / Inverurie Road / Old Meldrum Road, Aberdeen) (Redetermination of Means of Exercise of Public Right of Passage) Order 2012" signed with reference to this order and deposited at the offices of the Aberdeen City Council 74-76 Spring Garden, Aberdeen.

Roads over which means of exercise of public right of passage is to be redetermined - Aberdeen

That length of footway on the north side of the Inverurie Road from a point 143m east of its junction with Kepplehills Road, eastwards for a distance of 8m, as indicated by sawtooth hatching between the points marked 'A' and 'B' on Plan 01.

That length of footway associated with the Inverurie Road Pedestrian Underpass linking Gilbert Road to Inverurie Road, as indicated by sawtooth hatching between the points marked 'C' and 'D' on Plan 01.

That length of footway on the west side of Gilbert Road from a point 4m north-east of its junction with the A96 Trunk Road, north-eastwards for a distance of 8m, as indicated by sawtooth hatching between the points marked 'E' and 'F' on Plan 01.

That length of footway on the east side of Old Meldrum Road between its junction with the A96 Trunk Road and its junction with Station Road, Bucksburn, as indicated by sawtooth hatching between the points marked 'G' and 'H' on Plan 01.

That length of footway on the north side of the A96 Great Northern Road, Aberdeen, between its junction with the Haudagain Roundabout and a point 38m west of its junction with Bank Street, Woodside, as indicated by sawtooth hatching between the points marked 'I' and 'J' on Plan 02.

That length of footway on the south side of the A96 Great Northern Road from its junction with Queen Street, Woodside, eastwards for a distance of 30m, as indicated by sawtooth hatching between the points marked 'K' and 'L' on Plan 02.

ABERDEEN CITY COUNCIL

ROAD TRAFFIC REGULATION ACT 1984

**THE ABERDEEN CITY COUNCIL (NORTH GRAMPIAN CIRCLE, SOUTH GRAMPIAN CIRCLE, ABERDEEN)
(ONE-WAY) ORDER 2012**

Aberdeen City Council proposes to make the above-named order in terms of its powers under the Road Traffic Regulation Act 1984. The effect of the order will be to introduce a one-way requirement on North Grampian Circle, Aberdeen, and South Grampian Circle, Aberdeen. The permitted direction of travel on North Grampian Circle would be eastbound. The permitted direction of travel on the West section of South Grampian Circle would be eastbound. The permitted direction of travel on the East section of South Grampian Circle would be westbound.

Full details of the proposal are to be found in the draft order, which, together with maps showing the intended measures and an accompanying statement of the Council's reasons for promoting them, may be examined during normal office hours on weekdays between 12 September to 3 October 2012, in the offices of the roads officials in the Enterprise, Planning and Infrastructure department, at 74-76 Spring Garden, Aberdeen.

It is recommended that anyone visiting Spring Garden to view any of the documents should make an appointment to do so, in order that a member of staff can be present to offer an explanation if necessary. Anyone unable to visit Spring Garden can telephone (01224 538069) to speak to one of the officials.

Anyone wishing to object to the proposed order should send details of the grounds for objection, including their name and address, in writing to the undersigned or to trafficmanagement@aberdeencity.gov.uk during the statutory objection period which also runs from 12 September to 3 October 2012, inclusively.

Any person who submits an objection to a road traffic order should be aware that any objection made will be available to members of the Committee, available for inspection by members of the public, distributed to the press, and will form part of the agenda pack which is available on the Council's website. To that extent, however, they are redacted, with e-mail addresses, telephone numbers and signatures removed from this correspondence.

Jane MacEachran, Head of Legal and Democratic Services, Aberdeen City Council, Town House, Aberdeen

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning & Infrastructure
DATE	6 th November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	AWPR – Advance Works & Services
REPORT NUMBER:	EPI/12/242

1. PURPOSE OF REPORT

The purpose of this report is to inform Elected Members of the outcome of the appeal process and to seek approval to procure the Works & Services required in advance of the Main Works construction of the Aberdeen Western Peripheral Route (“AWPR”), including approval of the estimated expenditure relating to advance Works & Services .

2. RECOMMENDATION(S)

It is recommended that Committee: -

- (a) Note the disposal of the appeal against the decision of Scottish Ministers to approve the AWPR scheme by the United Kingdom Supreme Court;
- (b) Note the conclusion of the statutory approvals process for AWPR by Scottish Ministers in accordance with the Roads (Scotland) Act 1984; and,
- (c) Per Standing Order 1 (3), Members approve the procurement of the advance Works & Services detailed within the exempt information Annexe A to this report, deemed necessary to expedite the commencement of construction of the AWPR, and also approve the estimated expenditure for each contract (with a +10% allowance to account for variation in tender estimates).

3. FINANCIAL IMPLICATIONS

Funding for the AWPR has been the subject of agreement between Aberdeen City Council, Aberdeenshire Council and the Scottish Government with a signed Memorandum of Understanding. The Works and Services required for the AWPR will be paid for through this ongoing joint funding agreement, as detailed within report CG/12/08 by the Head of Finance to Full Council on 10th October 2012.

4. OTHER IMPLICATIONS

- 4.1 The delivery of the AWPR will achieve a number of the objectives and policies listed in Aberdeen City Council's "The Smarter City" policy statement and also the Single Outcome Agreement. This includes, for example, the provision of a sustainable transport system and promotion of Aberdeen as a city to invest, live, work and export from.
- 4.2 Environmental and sustainability issues associated with the AWPR were considered during the Environmental Impact Assessment of the proposed scheme as published in the 2007 Environmental Statement, and examined during the subsequent Public Local Inquiry (PLI) and with appropriate mitigation measures being confirmed within the Ministerial Decision Letter of 21st December 2009.

5. MAIN ISSUES

- 5.1 With the judgment of the UK Supreme Court on 17th October 2012 (Case Reference: UKSC 2012/0098), the appeal by Mr. William Walton against the decision of Scottish Ministers of 21st December 2009 to approve the AWPR was unanimously dismissed, and the AWPR scheme has now completed its statutory authorisation process.
- 5.2 Aberdeen City Council, in its capacity as the appointed Agent of Scottish Ministers in relation to the AWPR scheme, published a Prior Information Notice ("PIN") on 19th October 2012 in the Official Journal of the European Union ("OJEU") and the Public Contracts Scotland web portal relating to the Main Works construction contract of the AWPR scheme.
- 5.3 In order to, variously, de-risk specific elements of the scheme; meet on-going ecological and environmental commitments; and, to facilitate the earliest possible commencement of construction of this long delayed scheme, it is proposed that a range of advance Works & Services are procured and delivered to ensure that the AWPR is delivered in as short as possible timeframe.
- 5.4 Members should note that, per Standing Order 1(6)(b), Chief Officers of the Council approved the expenditure and acceptance of a Supplementary Ground Investigation tender, following advertisement in OJEU (reference 2012/S 20-032228), notwithstanding the fact that the amount of the tender was marginally in excess of the original estimated expenditure amount to meet the exigencies of the Service.
- 5.5 Aberdeen City Council's Standing Orders in Relation to Contracts and Procurement (approved by Full Council on 30th June 2010 – Article 15 refers) and Financial Regulations shall apply to all Works and Services procured. This includes adherence to the European Treaty principles of transparency, equal treatment and non-discrimination, proportionality

and mutual recognition which apply to all procurement activity regardless of value.

- 5.6 Where the estimated or aggregated value of contracts is within 10% of the relevant European Union (“EU”) threshold, and/or where the contract may potentially be of interest to economic operators elsewhere in the EU, then such contract opportunities will be advertised in the OJEU. Where the estimated or aggregated value is below this threshold, then contracts (and all sub-contracts) will be advertised via the Public Contracts Scotland (www.publiccontractsscotland.gov.uk) web portal.
- 5.7 The utilisation of the Public Contracts Scotland portal for the advertisement of all sub-OJEU threshold contract opportunities has the potential to expose contract opportunities to Scottish and other Small to Medium Enterprises (and, of course, secure better value for money for Aberdeen City Council as a purchaser). This is especially important in the current difficult economic climate and in addition to ensuring consistency with the existing EU procurement law requirement that contracts should be the subject of “adequate publicity”.
- 5.8 The anticipated advance Works & Services to be procured are listed in Annexe A elsewhere within Elected Member’s report pack. It is important to note that Annexe A contains exempt information as described in paragraph 8 of Part 1 of Schedule 7A of the Local Government (Scotland) Act 1973, enacted by the Local Government (Access to Information) Act 1985, and is therefore not included within the main body of this report as were it to do so, there would be disclosure to the public of exempt information as defined in the Schedule.
- 5.9 This is because the report refers to the amount of expenditure proposed to be incurred by the Council for a number of contracts for the provision of works and services (including, amongst others, archeological services, ecological and other advance works) where disclosure to the public of the amount herein referred to would be likely to give an advantage to a person or organisation entering, or seeking to enter, a contract with the Council.
- 5.10 Approval is sought to go out to tender for each contract specified in Annexe A and approval of the estimated expenditure (with a +10% allowance to account for variation in tender estimates) in accordance with Council Standing Order 1 (3).
- 5.11 It should be noted that the above Services and Works are entirely separate from the Main Works contract which is the subject of a separate OJEU procurement process as outlined in report EPI/12/056 to Members of this Committee dated 31st January 2012 (Article 46 refers).



6. IMPACT

- 6.1 Corporate - The AWPR will provide access to the Park & Choose and rail freight transfer sites around the periphery of Aberdeen and improve access to national and European transport networks, reducing the peripherality of the Aberdeen City and Shire. It will remove traffic from unsuitable rural and urban roads in and around Aberdeen and will improve road safety. By cutting congestion it will reduce journey times and improve journey time reliability.
- 6.2 Public - The AWPR will provide a boost to the north-east economy increasing business, employment, leisure and tourism opportunities.

7. BACKGROUND PAPERS

None.

9. REPORT AUTHOR DETAILS

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ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise, Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	Strategic and Local Transportation Projects Update Report
REPORT NUMBER:	EPI/12/188

1. PURPOSE OF REPORT

The purpose of this report is to advise Members of the progress to date of various strategic and local transportation projects within Aberdeen City and the wider area. These projects flow from the development of the Regional Transport Strategy (RTS) produced by Nestrans, and the Council's own Local Transport Strategy (LTS).

2. RECOMMENDATION(S)

It is recommended that Members:

- a) Note the contents of this report;
- b) Endorse development through Nestrans as outlined in section 5 of the report, including Board decisions and progress on the 2012/13 programme of works; and
- c) Instruct officers to initiate a refresh of the LTS and to report back to this Committee with a suggested programme of activity including public and stakeholder engagement.

3. FINANCIAL IMPLICATIONS

The projects described in this report are being funded through various budgets including Nestrans, the Regional Transport Partnership. Details are included in the relevant sections. There are no implications for approved PBB options.

4. OTHER IMPLICATIONS

None

5. BACKGROUND/MAIN ISSUES

A) Issues Requiring a Committee Decision

Nestrans

1 Nestrans Projects and Programmes

1.1 The Nestrans Board met on the 20th June and 29th August 2012 and copies of the minutes of these meetings are available in Appendix A. The Board also met on the 9th October 2012 and the minute of this meeting will be included within a future report.

1.2 Nestrans Capital Programme 2012/13

1.2.1 The capital programme expenditure for 2012/13 was approved at the Nestrans board meeting on 18th April 2012. Details of the programme within Aberdeen City, totaling approximately £976,000, are listed below, along with an update for those projects still outstanding.

1.2.2 Active Travel

Core Paths

A series of improvements have now begun on the following Core Paths, with completion expected by February 2013:

- Core Path 8: Auchmill Community Woodland
- Core Path 61: Hazledene Road – Countesswells Road/Hayfield Road
- Core Path 63: Den of Cults
- Core Path 66: Deeside Line
- Core Path 68: Den of Cults
- Core Path 69: Duthie Park
- Core Path 70: River Dee
- Core Path 78: Coastal Path

Physical works will be accompanied by promotion and signage.

Aberdeen to Blackburn Cycle Route

The statutory consultation process for the Redetermination Order to allow shared cycle and pedestrian use on the footpaths along the A96 trunk road was completed on 11th September 2012 and no formal objections were received. The signing and lining of the route is now underway following the infrastructure improvements which were carried out last year. The Order will be completed by Transport Scotland within approximately 4 weeks of the installation of the signing and lining, therefore it will be necessary to cover the signs after installation and remove the covers as soon as the order comes into force.

A statutory consultation was carried out during March and April 2012 on the proposed redetermination to shared pedestrian and cycle use of those sections of footway under the control of Aberdeen City Council (sections of Great Northern Road, Old Meldrum Road and Muggiemoss Road). Although no statutory objections were received it subsequently came to light that the word “redetermination” was omitted from the consultation documents and therefore it was necessary to re-advertise

this order between 22nd September and 22nd October 2012. The outcomes of this process will be reported to this Committee within a separate report.

Toucan crossings will be provided at either end of the route, one on the A96 west of the Sclattie Park roundabout and the other on Great Northern Road beside its junction with Queen Street. These will be installed this year in time for the formal route opening.

Cycle Demonstration Project

Improvements to two popular paths will be taking place this financial year. One of these, Link 3 in the Greenbrae Cycle Project Action Plan (Greenbrae Drive to Seaview), should be completed by the end of December, while the other, Link 6 (Lochside to Denmore Road), will be completed during January and February 2013. These will deliver improved pedestrian and cycle links to Greenbrae School and to the Denmore Industrial Estate. A series of shared use paths have successfully been through the Traffic Regulation Order (TRO) process and are currently being formalised through signage. These are:

- Link 2 (Greenbrae Crescent to Greenbrae Drive)
- Link 5 (Dubford Road)
- Link 7 (Greenbrae Drive West)

Working in partnership with members of the local community, a refresh of the Action Plan has commenced to reflect some recent developments within the area. Links 15 (Lochside Road to Scotstown Road) and 16 (Lochside Road to Dubford Road) are to be removed from the Plan following discussions with the landowners. Link 15 appears unfeasible from an engineering point of view, while improvements to Link 16 are being taken forward by the landowners to their own specification. Links 13 (Dubford Road to Greenbrae Circle) and 14 (Greenbrae Circle to Greenbrae Drive) are also to be removed from the Action Plan because of the limited benefits they will bring in comparison to the expenditure required and due to the fact that these links are effectively duplicated elsewhere, therefore resources could be better spent elsewhere.

Due to the high costs and potentially limited benefits associated with Links 10 (Seaview Drive to Dubford Road) and 11 (Dubford Road to Dubford Gardens), alternative cycle routes, using quiet roads and existing pathways, have been identified which will be far more cost-effective than constructing brand new paths of considerable length and can be formalised merely by signage and promotion. It is also anticipated that similar paths to those originally outlined in the Action Plan may ultimately be delivered by the private sector as part of the forthcoming Dubford development. Although the revised routes will be predominantly on-road, these roads are lightly-trafficked and benefit from traffic-calming features. These routes will arguably be safer than those originally proposed, especially for child cyclists, as they will benefit from existing street lighting and natural surveillance. It is

anticipated that these routes will also be formalised by the end of the year.

A public event took place on 6th October to discuss these modifications with members of the community and to update them on the progress of the Action Plan. The event was also intended to further encourage and promote cycling to residents and comprised the Getabout Bike Roadshow and, a 'bike doctor' free for the local community. The event was attended by approximately 50-60 local residents and all proposals warmly received.

Hands Up Survey results for 2012 show that the cycle to school rate has doubled at Greenbrae School in the past year, with 6% of pupils now regularly cycling to school, compared to 3% in 2011. Following the installation of a scooter rack at the school earlier this year, 9% of school pupils are now regularly arriving by scooter, compared to 1% last year.

The next stage of the project will involve looking at ways of improving cycle access from the study area to the popular trip generators of Oldmachar Academy and the Middleton Park shopping facilities and preparing a cycle map of the local area in partnership with Greenbrae School pupils.

1.2.3 Public Transport

A96 Park and Choose/Dyce Drive Link Road

Work is continuing on the specimen design for inclusion in the Aberdeen Western Peripheral Route (AWPR) contract.

Aberdeen City and Shire Joint Bus Stop Information Initiatives

The programme of provision/replacement of bus timetable display cases at bus stops throughout Aberdeen City and Shire is continuing.

Upgrade Bus Lane Enforcement Cameras on Strategic Bus Corridors

Tenders have been returned and the tender has been awarded to Vysionics ITS Limited. It is expected that the system will be operational early 2013. The Councils' Finance and Resources Committee, at their meeting in October, agreed a £60 penalty charge for bus lane violations.

A four week public awareness campaign will be undertaken in advance of the system becoming operational to inform the public of the start date for civil enforcement and to explain the objectives of the scheme. Various advertising media will be utilised to promote the new system including local press, bus panels and websites.

Warning letters will be issued to motorists captured driving in bus lanes during the first two weeks of the publicity campaign, which is in line with

best practice and Department for Transport guidance, thus allowing drivers to change their habits prior to enforcement commencing.

Airport Bus Turning Circle

Design is underway and it is intended to lodge a planning application for this project in November 2012.

Night Time Transport Zone

The installation of lit signs at night time bus stops on Union Street should be complete by the end of the year.

1.2.4 Strategic Road Safety Improvements

Road Studs and Lining

A programme of renewing road studs and relining works on Wellington Road and the A944 is continuing.

1.3 Nestrans Revenue Programme 2012/13

1.3.1 The revenue programme expenditure for 2012/13 was approved at the same meeting and details of the programme, totaling £324,000, are listed below along with scheme updates.

1.3.2 Bus Action Plan

Bridge of Don Park & Ride Feasibility

An economic assessment of the retention and improvement of the Park and Ride within the Aberdeen Exhibition and Conference Centre (AECC) site has been undertaken. The report compares the loss of the Park and Ride site with 500 space and 1000 space car parks.

The value of the Park and Ride is considered in terms of user benefits including travel time, vehicle operating costs, public finances and local network benefits such as noise, local air quality, greenhouse gases, journey ambience and accident benefits.

As would be expected, the benefits increase as the volume of traffic using the Park and Ride increases and the report clearly shows that the financial benefit (net present value) of the Park and Ride site substantially outweighs any anticipated expenditure to create the infrastructure. The report results are included in Appendix B.

As reported to this Committee in September a tendering process is being undertaken to appoint a Development Partner for the AECC. There is an initial shortlist of 4 applicants and it is anticipated that the final appointment will be made in Summer 2013.

As developers are coming forward with opportunities for development of the whole AECC site, these may include alternate options for the Park and Ride. Officers will work with the developers throughout the

process to ensure that sustainable transport options are considered and that realistic Park and Ride options remain available.

Bus Link Improvements to Anderson Drive

Modeling of east-west priority along junctions on Anderson Drive and sections of bus priority on the corridor is continuing.

Upgrade of Backroom Office Equipment for Bus Lane Decriminalisation See Section 1.2.3.

1.3.3 Rail Action Plan

Contribution to Dyce Shuttle Bus

Tenders have been returned and awarded to Stagecoach. Patronage data continues to be gathered for this service and is showing a sharp increase in passenger numbers in recent months.

1.3.4 Project Feasibility and Monitoring

Bridge of Dee - Project Feasibility & Development

The pre-appraisal report (Volume 1 of 4), the outcomes of which were agreed by this Committee in May 2012, has been published on the Aberdeen City Council website alongside all other information relating to the study that is already available:

http://www.aberdeencity.gov.uk/transport_streets/roads_pavements/transport_projects/road_access_from_south_home.asp

This gives the public an opportunity to comment and an offer was also extended to meet with adjacent Community Councils. A meeting was therefore arranged for Monday 22 October 2012 to facilitate this discussion to which all the affected community councils were invited. Unfortunately no representatives of the community councils were able to attend. Further efforts will be made to enable the community councils to discuss the pre-appraisal should they wish to and the further stages of the STAG process will also have continued opportunities for stakeholder and public engagement.

The formal Scottish Transport Appraisal Guidance (STAG) process is now ongoing and a programme has been developed for the STAG Part 1 Appraisal phase of the work including identification of elements of the study that will be progressed during 2012/13. The methodology will be in line with the STAG - Part 1 Appraisal Guidance Note (01/01/2012).

STAG Part 1 Appraisal Summary Tables (ASTs) will be prepared for each option assessed, detailing the findings in respect of:

- proposal details and background information;
- performance against the project specific objectives;
- implementability appraisal;

- environmental assessment;
- safety assessment;
- economic assessment;
- integration assessment; and
- accessibility and social inclusion assessment.

The following key elements of the work have been identified for progression during 2012/13.

- Technical Development - The first task required in this phase is to identify the physical implications of these improvements. Completion of this work will enable assessment of engineering feasibility and cost. This work will require the preparation of preliminary designs of structures for each concept.
- Environment - The key environmental attributes and characteristics of the study area will be summarised and environmental baseline data will be gathered from readily available existing information.
- Traffic Modeling – Further traffic modeling work will be carried out for each option being assessed and transport economic efficiency modeling work will also be undertaken.

The study will continue to involve full engagement with stakeholders throughout the appraisal process.

Part 1 ASTs and Volume 2 of the STAG report is anticipated to be completed for consideration by the end of March 2014. Members will be kept up to date on progress of this study through future reports or bulletins to this Committee.

- 1.3.5 It is therefore recommended that Members endorse development through Nestrans as outlined above, including Board decisions and progress on the 2012/13 programme of works.

2 Regional Transport Strategy and Local Transport Strategy

- 2.1 Reference is made to the September meeting of this Committee where Members agreed a response to Nestrans on the draft Regional Transport Strategy (RTS) Main Issues Report.
- 2.2 A formal 6 week consultation period on the refresh of the RTS commenced in early October with the Main Issues Report available on the Nestrans website and circulated to stakeholders. A separate Nestrans Board workshop took place on 9th October 2012 to consider the refresh, while it was also discussed at a meeting of the North East Transport Consultative Forum, a wide-ranging group of local and national stakeholders with an interest in all aspects of transport development in the north east, on 30th October 2012. Following the

consultation period, a revised RTS Action Plan will be considered by the Nestrans Board, with a Strategic Environmental Assessment (SEA) and Equalities and Human Rights Impact Assessment (EHRIA) prepared in parallel.

- 2.3 Aberdeen City Council's Local Transport Strategy (LTS) sets out a vision for transport in the City for a five year period. The current LTS was written in 2008 and is due to expire shortly.
- 2.4 The vision, aims, objectives and actions of the 2008 LTS were informed by the current RTS and are very much predicated on the AWPR being in place, with the resultant freed up network being 'locked in' to public transport, cycling and walking schemes to maximise the benefits of the new road. Since ongoing legal challenges have delayed the AWPR it is proposed that a full-scale review is not required at this time given that timescales for building the AWPR remain very much aligned with any future adoption of a Local Transport Strategy (i.e. 2013- 2018).
- 2.5 With the RTS in the process of being refreshed, therefore, what is proposed instead is to commence a similar refresh of the LTS, informed by the RTS refresh, and focusing on revisiting the visions, aims, objectives, actions and targets with text within the main document updated and revised as required. This will also reflect other relevant developments and strategies that have come to the fore since the LTS was originally developed in 2006/7, such as climate change legislation. Officers would also suggest an Action Plan for delivery to accompany the refresh.
- 2.6 It is anticipated that the refresh of the RTS will be completed by the end of December 2012, allowing the LTS refresh to commence in the New Year.
- 2.7 It is therefore recommended that Members instruct officers to initiate a refresh of the LTS and to report back to Committee with a suggested programme of activity including public and stakeholder engagement.

B) Issues for Information

Active Travel and Air Quality

3 Hands Up Survey 2012

- 3.1 The 2012 Hands Up Scotland school travel survey took place during the week beginning 10th September, with all schools in Aberdeen requested to participate. 90% of primary schools and 75% of secondary schools ultimately took part in the survey, compared to 82% and 75% respectively in 2011. Following efforts from Sustrans Scotland, who co-ordinates the survey nationally, to include as many nurseries, SEN (Special Educational Needs) schools and independent

schools in this year's survey, 31% of nurseries, 38% of SEN schools and 63% of independent schools also took part.

- 3.2 Results from local authority primary and secondary schools are almost identical to 2011 results, with 58% of children regularly walking to school, 3% cycling, 11% travelling by bus and 20% being driven to school in both 2011 and 2012. Despite there having been little movement since last year, however, these results are welcome in that they demonstrate, as was suggested by last year's results, that active travel rates are stabilising following a number of years of decline.
- 3.3 Nevertheless, there are some noteworthy results. The cycle to school rate amongst primary school children is at its highest since the survey began, with almost 4% of all primary school pupils cycling to school regularly. Two primary schools also have record numbers of pupils cycling to school - Airyhall has 14% and Fernielea 12%. Torry Academy also reports 90% of pupils travelling to school by active modes of transport, the highest proportion in the City.
- 3.4 National results will be published during Spring 2013, allowing Aberdeen City to benchmark travel to school figures with other Scottish local authorities, and will be reported to this Committee in a future report. The next survey will take place in September 2013.

4 Sustrans Community Links Fund 2012/13

- 4.1 In May 2012, a series of bids were made to Sustrans Scotland's Community Links Fund 2012/13 for the match-funding of upgrades to some walking and cycling paths within the City. The following applications have been successful:
 - Deeside Line - £60,000 awarded
 - Greenbrae - £35,000 awarded for path improvements in the area.
- 4.2 Officers are awaiting a response from Sustrans regarding which elements of the Greenbrae works they are willing to match-fund. The Deeside Line improvements will be delivered between November 2012 and January 2013. Match funded by Nestrans, this will see the tarmacing of the route, including drainage works, continuing to just short of Peterculter, leaving only 1500m of granite dust path left within the ACC boundary.

5 Sustrans School Cycle Parking Fund 2012

- 5.1 As reported to this Committee in September, £8,750 of match funding was received from Sustrans Scotland to install the following facilities at Aberdeen schools:
 - a cycle shelter at Torry Academy;
 - a shelter to cover the existing cycle stands at Ferryhill School and a new scooter rack for the school grounds; and

- a cycle shelter and scooter rack to benefit junior pupils at Fernielea School.

The remaining funding was supplied from the Council's Cycling, Walking and Safer Streets (CWSS) fund and the facilities were successfully installed in September.

6 Cycle Friendly and Sustainable Communities Fund

- 6.1 Muirfield School has been successful in an application for match funding from Cycling Scotland's Cycle Friendly and Sustainable Communities Fund. Entitled the 'Muirfield MAD (Make a Difference) Group', the school proposes to develop a cycle skills trail in the playground for children, staff and parents to practice on, with the facility also available for the wider community to use during evenings, weekends and school holidays. This will be accompanied by a soft measures campaign, involving cycle training, events, leaflets, maps and other promotional items, to get school pupils and the community as a whole excited about and engaged with cycling.
- 6.2 The aim of the project is to develop a love of cycling for all, to open up new lifestyle choices for children, to encourage them to take a leadership role and to improve health and fitness levels in the Mastrick community. Further updates on this project will be supplied to this Committee as the project progresses.

7 European Mobility Week 2012

- 7.1 European Mobility Week (EMW) 2012 took place during September and the Council marked the occasion with a series of events and promotions throughout the week. As well as a sustainable commuter challenge for staff, which received more than 70 entries, the Getabout Bike Roadshow visited Hazlehead School, Bucksburn Academy and the University of Aberdeen during the week to generate enthusiasm for cycling amongst pupils and students. These events all received extremely positive feedback from participants, with the Principal Teacher of Physical Education at Bucksburn Academy writing after the event to say, *The kids are really buzzed about cycling, especially any potential events or activities that may be coming off. It's almost like the event had kick started our plans again. We hope the pupils will lead ideas off the back of this event.*
- 7.2 EMW traditionally culminates in 'In Town Without My Car Day', when participating local authorities close a road to motor vehicles and reserve the space solely for pedestrians and cyclists. Aberdeen City Council, in partnership with Getabout, celebrated In Town Without My Car Day on Sunday 23rd September by closing a section of the Beach Esplanade to traffic and filling it with sustainable transport-themed events and activities. These included the Getabout Bike Roadshow, a free bike doctor and rickshaw taxis along the Esplanade. ACC and Getabout staff were also there to discuss transport and air quality

issues with members of the public, First and Stagecoach came along to promote their services and some recent innovations in bus travel, while Commonwheels were also present to promote the benefits of the Car Club. Officers estimate that between 500 and 700 members of the public attended the event.

8 Council Staff Travel Survey

8.1 Aberdeen City Council has a Travel Plan in place which demonstrates the Council's commitment to minimising the impact of staff travel on the environment by reducing the level of unnecessary travel and encouraging those who have to travel do so in a more environmentally-friendly and efficient way. The Travel Plan is monitored via a biennial staff travel survey which aims to find out how staff members usually travel to work and while on work and what would encourage them to adopt more sustainable travel habits.

8.2 During April and May 2012, the third biennial staff travel survey was undertaken. Results from 2012, as well as from previous surveys, can be found on The Zone at:

http://thezone/OurEnvironment/Transport/tra_counciltravelplansurvey.asp

8.3 One of the key questions asked is how employees usually travel to their main place of work. The results of this question can be seen in the table below.

Q. How do you usually travel to your main place of work?			
	2008	2010	2012
Solo driving	46.2%	43.2%	45.6%
Car sharing	11.7%	11.9%	13.3%
Public transport	19.5%	21.7%	22.3%
Walk	18.8%	17.5%	14%
Cycle	2.7%	4%	2.7%
Motorcycle	0.7%	0.8%	0.7%

Results show that the number of employees travelling to work by car is increasing following a period of decline, whilst walking and cycling rates have fallen since 2010. There has been a slight increase in the number of staff members travelling to work by bus.

8.4 Officers within E, P & I continue to progress initiatives to encourage a reduction in the number of staff travelling to work by car and an increase in the number travelling by active modes of transport including offering free membership of the Car Club, free membership of a car share scheme, promotion of salary sacrifice schemes for the purchase of bicycles and annual bus tickets, pool bicycles at Marischal College

and free advice and literature on travelling by bike, on foot or by public transport.

8.5 The next staff travel survey will take place in Spring 2014.

9 City Centre Transport Masterplan/Sustainable Urban Mobility Plan

9.1 In the September Report Members were informed of plans to carry out pedestrian footfall and on-street surveys to inform the background information for the City Centre Transport Masterplan. Two surveys were developed with the first taking the form of an online questionnaire and the second a series of questions asked face to face on-street over a two day period in five locations. In addition, cameras were set up in 27 city centre locations to observe pedestrian movements over a 7 day period. These were completed in conjunction with car park user surveys being undertaken to inform the development of a new City Centre Traffic Model. A series of “drop in” stakeholder workshops were also held at the end of October to gather the opinions of various transport issues across the City Centre. The outcomes of this process will be reported to the next meeting of the E, P and I Committee, with possible actions to be undertaken as a result.

9.2 In addition, it is proposed to reclassify the City Centre Transport Masterplan as a Sustainable Urban Mobility Plan (SUMP). Currently the European Commission provides the following guidelines on SUMPs:

A Sustainable Urban Mobility Plan aims to create a sustainable urban transport system, by addressing at least the following objectives:

- *Ensure the transport system is accessible to all;*
- *Improve the safety and security of its users;*
- *Reduce air and noise pollution, greenhouse gas emissions and energy consumption;*
- *Improve the efficiency and cost-effectiveness of the transportation of persons and goods;*
- *Enhance the attractiveness and quality of the urban environment.*

The policies and measures defined in a Sustainable Urban Mobility Plan should comprehensively address all modes and forms of transport in the entire urban agglomeration: Public and private, passenger and freight, motorised and non-motorised, moving and parking.

These aims and objectives fit with the City Centre Transport Masterplan and allow comparison with other Local Authorities across Europe. It also allows Aberdeen City to apply for EC grants and awards towards transport improvements.

10 Electric Vehicle Procurement Support Scheme

- 10.1 On the 10th of October 2012 the Scottish Government via Transport Scotland awarded Aberdeen Community Planning Partnership a grant of £120,000 for the 2012/13 financial year. This grant is for the supply and installation of electric vehicle charging infrastructure with connection to Transport Scotland's back office facility as part of the national "Plugged in Places" project.
- 10.2 The particular focus of this funding is to allow Aberdeen to become one of a number of country-wide strategic charging hubs, enabling electric vehicles to be driven throughout Scotland. The goal is to have 380 charging outlets in publicly accessible locations across Scotland, strategically placed along major routes at a maximum of 50 miles apart. Aberdeen City has been allocated a grant to purchase and install six charging points comprising two rapid charge, capable of recharging a standard electric car in under 30 minutes, two fast charge and two standard charge units within the City area. These will be made available to members of the public using a "Pay as you go" system.
- 10.3 Aberdeen City Council Officers have already expressed concern that this late funding announcement puts the Council under considerable pressure to have infrastructure procured and installed before the end of the current financial year, particularly as the value of the award will require a tender process to be undertaken. However, officers fully intend to spend the £120,000 that has been awarded and are currently drafting a programme to ensure that the work is undertaken in line with Transport Scotland timescales whilst ensuring compliance with procurement rules.
- 10.4 Officers continue to work with the local Electric Vehicle Association Scotland (EVAS) to establish preferred locations for publicly accessible charging infrastructure and to devise an electric vehicle strategy for Aberdeen which will outline how the network will operate.

11 Car Club

- 11.1 The Co-wheels Car Club (as it has been rebranded) in Aberdeen is now the fastest growing Car Club in the UK, with 305 members and 9 corporate members at the end of September.
- 11.2 A new car, located within the Donside development at Tillydrone in a dedicated parking space, is to be added to the fleet before the end of the year. This will be funded by the site developer with free membership of the Car Club offered to all residents of the development.

Major Projects

12 Strategic Transport Fund

- 12.1 A report on the implementation of the Strategic Transport Fund (STF) since the adoption of the Supplementary Planning Guidance (SPG): 'Delivering Identified Projects through a Strategic Transport Fund', was considered by the Nestrans Board on 9th October 2012. This report can be found at Appendix C.
- 12.2 The report outlined the income to the fund to date - £43,000 in the fund, of a total of £1,286,000 secured so far from committed development via Section 75 Legal Agreements and voluntary upfront payments. Officers in Planning and Sustainable Development continue to ensure this policy is consistently applied across the City.
- 12.3 The transport infrastructure projects outlined within the SPG to be delivered with this new fund are in the process of being prioritised for design and delivery, with this prioritisation being informed by information from the development industry across the region on likely build out rates of the housing and employment allocations within the respective Aberdeen City and Aberdeenshire Local Development Plans. Nestrans plans to hold a stakeholder workshop later this year to consult the development industry on a prioritised programme, with a view to reporting back to their Board in December 2012.
- 12.4 Planning and Sustainable Development officers continue to be involved in this prioritisation process and it is anticipated that a finalised programme will be commended to both Aberdeen City and Aberdeenshire Councils, respectively, for their consideration in the New Year.

The Supplementary Planning Guidance referred to above can be found at the following web link:

<http://www.aberdeencityandshire-sdpa.gov.uk/nmsruntime/saveasdialog.asp?IID=963&sID=38>

13 Aberdeen Western Peripheral Route

- 13.1 In July 2012, the UK Supreme Court heard an appeal against the decision of the Inner House of the Court of Session to refuse a legal challenge against the proposed route of the AWPR. On 17th October 2012 it was announced that the appeal had been rejected.
- 13.2 The defeat in this latest appeal now enables the planned route to go ahead. The Scottish Government has announced that it will be quantifying the total project costs and updating Parliament with revised figures as soon as possible. It is now anticipated that the route will be completed by Spring 2018.

14 Third Don Crossing

14.1 In May 2012, opponents of the scheme, including local residents served with Compulsory Purchase Orders (CPOs), launched a legal challenge at the Court of Sessions against the decision of Scottish Ministers to proceed with CPOs. The case is due to be heard in December 2012.

15 A90 (S) Park and Ride

15.1 Officers within Aberdeenshire Council are developing a new Park and Ride site by the A90 at Schoolhill, south of Aberdeen as part of the public transport objectives of the Regional Transport Strategy. This will compliment the existing Park and Rides at Bridge of Don and Ellon, and the A96 location that will be constructed within Aberdeen City. Preliminary design work is now complete, land negotiations are underway and a planning application has been submitted.

6. IMPACT

The contents of this report link to the Community Plan vision of creating a 'sustainable City with an integrated transport system that is accessible to all'.

All of the projects and strategies referred to in this report will contribute to delivery of the Smarter Mobility aims of *Aberdeen – The Smarter City*: “We will develop, maintain and promote road, rail, ferry and air links from the city to the UK and the rest of the world. We will encourage cycling and walking”, and “We will provide and promote a sustainable transport system, including cycling, which reduces our carbon emissions.”

The projects identified in this report will also assist in the delivery of actions identified in the Single Outcome Agreement (SOA), in particular the delivery of both Local and Regional Transport Strategies which will contribute directly and indirectly to 14 out of the 15 National Outcomes described in Aberdeen City Council's 2009/10 SOA.

The Local Transport Strategy (LTS) and Regional Transport Strategy (RTS) from which the transportation schemes within this report are an integral part have been subject to Equalities & Human Rights Impact Assessments.

7. BACKGROUND PAPERS

All background papers are referenced within the main body of the report.

8. REPORT AUTHOR DETAILS

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Appendix A

NORTH EAST SCOTLAND TRANSPORT PARTNERSHIP

Minute of Meeting of the North East Scotland Transport Partnership Board

Aberdeen, 20 June, 2012

Present: Councillors Finlayson, Grant, McCaig and Milne (Aberdeen City Council); Councillors Argyle, Buchan, Clark, and Latham (Aberdeenshire Council); Eddie Anderson, Jennifer Craw, and David Sullivan (External Members); and Stephen Archer (Director of Infrastructure Services, Aberdeenshire Council) and Dr Margaret Bochel (Head of Planning and Sustainable Development, Aberdeen City Council) (Advisers to the Board).

In Attendance: Derick Murray, Rab Dickson and Kirsty Morrison (Nestrans Office); Tom Buchan and Ewan Wallace (Aberdeenshire Council); Martin Allan (Aberdeen City Council); and Laura Grant (The Big Partnership).

Apologies: Derek Provan (External Member).

The agenda and reports associated with this minute can be located at the following link:

<http://www.nestrans.org.uk/48/board-meetings.html>

In the absence of a Chairperson, Eddie Anderson (Depute Chair) opened the meeting and welcomed all present to the first meeting of the Board following the recent local government elections.

APPOINTMENT OF CHAIRPERSON

1. Councillor Milne, seconded by Councillor Latham, proposed that Councillor Argyle be appointed as Chairperson of Nestrans.

The Board resolved:
to appoint Councillor Argyle as Chairperson.

At this juncture, Councillor Argyle took the Chair.

APPOINTMENT OF DEPUTY CHAIR

2. The Chairperson, seconded by Councillor Grant, proposed that Councillor Milne be appointed as a Deputy Chair of Nestrans.

The Board resolved:

- (i) to appoint Councillor Milne as a Deputy Chair; and
- (ii) to reaffirm Eddie Anderson as Deputy Chair.

APPOINTMENT OF A PROFESSIONAL ADVISER

3. The clerk to the Board advised that the relevant legislation and guidance relating to the membership and operation of Regional Transport Partnership's allows for the appointment of Advisers to the Board, for periods of four years, unless otherwise specified at the time of the appointment(s).

The Board resolved:

that due to the Council term being five years, to appoint Dr Margaret Bochel (Head of Planning and Sustainable Development, Aberdeen City Council) and Stephen Archer (Director of Infrastructure Services, Aberdeenshire Council) as Advisers to the Board for a period of five years.

MINUTE OF PREVIOUS MEETING

4. The Board had before it the minute of its previous meeting of 8 April, 2012.

The Board resolved:

to approve the minute as a correct record.

LIAISON BETWEEN REGIONAL TRANSPORT PARTNERSHIPS AND THE SCOTTISH GOVERNMENT AND OTHERS

5. With reference to article 3 of the minute of its previous meeting of 18 April, 2012, the Board had before it a report by the Director which provided an update on liaison with other Regional Transport Partnerships (RTPs), the Scottish Government and others.

The meetings included –

Regional Transport Partnership Lead Officers' meeting in Perth on 16 May, 2012

Health and Transport Action Plan Steering Group in Aberdeen on 22 May, 2012 and 5 June, 2012

Freight Forum in Aberdeen on 21 May, 2012

Local Authority and Bus Operator Forum Steering Group meeting in Aberdeen on 24 May, 2012

Scottish Transport Awards in Glasgow on 14 June, 2012

Scottish Government Health Directorate in Edinburgh on 15 June, 2012

Nestrans member induction in Aberdeen on 18 June, 2012

The Director advised that the information hubs for health transport were to be linked into appointments for patients; however, there was currently no Government funding available for this. The Scottish Ambulance Service was keen to progress this and had suggested that two members of its staff look at developing this system for six months. It was further suggested that Nestrans, Aberdeen City Council, Aberdeenshire Council, NHS Grampian contribute £6,000 each to fund these two posts for a further six months. This was contained within the Health and Transport Action Plan budget and would require its approval.

The Board resolved:

- (i) to agree to the proposal to fund posts to develop information hubs at a cost of £6000 (to be met from the Health and Transport Action Plan budget);
- (ii) to note progress on liaison arrangements with other RTPs, the Scottish Government, and others; and
- (iii) to note the arrangements for future meetings as detailed within the report.

STRATHCLYDE PARTNERSHIP FOR TRANSPORT – BUS REGULATION

6. With reference to article 3 of the minute of its previous meeting, the Board had before it a report by the Director which set out the north east response to the proposals by the Strathclyde Partnership for Transport (SPT) on the regulation of buses.

The Board resolved:

- (i) to note the joint north east response to the Strathclyde Partnership for Transport (SPT) proposals;
- (ii) to instruct officers to send a copy of this response to East Dunbartonshire Council in response to their letter requesting political support for these proposals; and
- (iii) to note that some of the issues detailed in the letter from East Dunbartonshire Council do not affect Nestrans but will require to be considered by the two local authorities.

ABERDEEN – INVERNESS RAIL ENHANCEMENTS

7. The Board had before it a report by the Director which updated members on the proposal to enhance journey times and frequencies on the Aberdeen – Inverness railway line.

The Board resolved:

- (i) to note the report and instruct officers to continue dialogue with Transport Scotland and Network Rail, and report back on the Aberdeen – Inverness enhancement project when the report is published; and

- (ii) to request that officers investigate the possibility of a case being made for an additional railway station at either Newtonhill or Persley.

PROGRESS REPORT

8. With reference to article 10 of the minute of its previous meeting of 18 April, 2012, the Board had before it a progress chart summarising the work in the three sub strategies of the Regional Transport Strategy as at 12 June, 2012.

The Sub Committee resolved:

to note the content of the progress chart.

PUBLICATIONS AND CONSULTATIONS

9. With reference to article 9 of the minute of its meeting of 15 February, 2012, the Board had before it a report by the Director which summarised and advised on a number of different publications and consultations as follows and sought approval of draft responses as appropriate –

- the airport masterplan
- new railway stations fund
- Reforming our Railways: Putting the Customer First
- rail fares and ticketing review
- rail decentralisation
- Network Rail – alternative solutions scoping document
- A96 Inveramsay Bridge Improvement

The Board resolved:

to note the report and the documents referred to above, and to endorse the comments as Nestrans' response to the consultations.

DECLARATION OF INTEREST

Laura Grant (Press Officer, BIG Partnership) declared an interest in the subject matter of the following article due to the nature of the business to be transacted therein, and withdrew from the meeting during consideration of this item.

PUBLIC RELATIONS

10. With reference to article 11 of the minute of its previous meeting of 18 April, 2012, the Board had before it a report by the Director which brought members up to date with discussions which had taken place between Aberdeen City and Shire Economic Forum (ACSEF) regarding a joint Public Relations contract.

The Board resolved:

to approve the tender proposal as detailed within the report.

BOARD MEETING DATES

11. The Board had before it proposed dates and times of future meetings for consideration. It was noted that two of the previously agreed dates (22 August, and 10 October) now clashed with meetings of Aberdeen City Council.

The Sub Committee resolved:

to agree that meetings would be held on the following dates, and that all meetings would commence at 2pm –

Wednesday 29 August

Tuesday 9 October

Wednesday 12 December.

EQUALITIES DUTIES

12. The Board had before it a report by the Director which provided members with an update on Nestrans' Equalities Duties, and presented an annual report in this regard.

The Board resolved:

to approve the report and the appendix as the Partnership's report on Equalities.

UNAUDITED STATEMENT OF ACCOUNTS AND ANNUAL AUDIT PLAN

13. The Board had before it a report by the Treasurer which presented the unaudited Statement of Accounts for 2011/12, and a plan by Audit Scotland proposing audit arrangements for the 2011/12 audit.

The Board resolved:

- (i) to note the unaudited financial statements for 2011/12;
- (ii) to approve the issuing of these statements to the external auditor and the Controller of Audit; and
- (iii) to note the Nestrans' 2012 audit planning report, as appended to this minute.

INFORMATION BULLETIN

14. With reference to article 15 of the minute of its previous meeting of 18 April, 2012, the Board had before it a report by the Director which provided information and updates for the Board on a number of matters not requiring a decision as follows –

ScotRail Customer Forum

Flights to London City / easyJet event in Parliament
Aviation consultation update / UK all party Parliamentary Group on Aviation
English review of strategic road network
Scottish Government review of community planning
Update on Northern Isles ferry services tender
Bridge of Dee study
ScotRail timetabling
A944 / B9119 junction improvements
Nestrans' press releases
Getabout events
Dyce Shuttle Bus

The Board resolved:

- (i) to request that the Director arrange a meeting between the press and the new Chairperson at an early date in order to get the key messages from Nestrans across;
- (ii) to request that the Director updates the Board on his proposal to organise an event for MPs in relation to the aviation policy;
- (iii) to note that the Director would speak to Derek Provan about the possibility of reinstating flights to Stansted;
- (iv) to note that a report on the possible reduction of congestion at Dyce Station associated with the shuttle bus (by utilising a different area of the station for turning of the bus) would be presented to a future meeting of the Board;
- (v) to note that for the shuttle bus the Director would provide information on customer feedback following a customer satisfaction survey and investigate whether the advertising of the bus within the airport could be improved and report back to a future meeting of the board; and
- (vi) to otherwise note the content of the bulletin.

CONFERENCES AND PRESENTATIONS

15. With reference to article 16 of the minute of its previous meeting of 18 April, 2012, the Board had before it a report by the Director summarising recent and forthcoming conferences of interest to the Partnership along with presentations by Nestrans and its partners.

The Board resolved:

to note the information as presented.

PENDING BUSINESS AND REPORTS FOR FUTURE MEETINGS

16. With reference to article 17 of the minute of its previous meeting of 18 April, 2012, the Board had before it a report by the Director detailing pending business and information on reports to be submitted to future Board meetings.

The Board resolved:
to note the information.
- PETER ARGYLE, Chairperson.

NORTH EAST TRANSPORT PARTNERSHIP

Minute of Meeting of the North East Transport Partnership

Aberdeen, 29 August, 2012

Present: Councillor Argyle (Chairperson); Councillor Milne (Deputy Chair); and Councillors Finlayson, Young (as substitute for Councillor Grant) and Yuill (as substitute for Councillor McCaig) (Aberdeen City Council), Councillors Buchan, Clark and Latham (Aberdeenshire Council), Jennifer Craw and Derek Provan (External Members).

In attendance: Martin Allan (Aberdeen City Council), Jenny Anderson (Nestrans), David Bell (Deloitte), Tom Buchan (Aberdeenshire Council), Rebecka Coull (Aberdeen City Council), Rab Dickson (Nestrans), Will Hekelaar (Aberdeen City Council), Dave McDermid (BIG Partnership), Derick Murray (Nestrans), Colin Parker (Harbour Board) (item 1 only), Katherine Purvis (Aberdeen City Council), Ewan Wallace (Aberdeenshire Council), and Karlyn Watt (Deloitte).

Apologies: Eddie Anderson (External Member), Stephen Archer (Adviser to the Board), Dr Maggie Bochel (Adviser to the Board), Councillor Grant (Aberdeen City Council), Councillor McCaig (Aberdeen City Council), and David Sullivan (External Member).

The agenda and reports associated with this minute can be located at the following link:

<http://www.nestrans.org.uk/48/board-meetings.html>

INTRODUCTION – HARBOUR OPERATIONS CENTRE

1. Colin Parker (Chief Executive, Aberdeen Harbour Board), welcomed members of Nestrans to the Harbour Operations Centre and provided a short presentation on the Harbour Board and Operations Centre. The Harbour Board's Annual Review document was circulated for information.

MINUTE OF PREVIOUS MEETING

2. The Board had before it the minute of its previous meeting of 20 June, 2012.

The Board resolved:

(i) to note that Jennifer Craw had been in attendance; and

(ii) to approve the minute as a correct record, subject to Jennifer Crow being added to the list of attendees.

LIAISON BETWEEN REGIONAL TRANSPORT PARTNERSHIPS AND THE SCOTTISH GOVERNMENT AND OTHERS

3. With reference to article 5 of the minute of its previous meeting of 20 June, 2012, the Board had before it a report by the Director which provided an update on liaison with other Regional Transport Partnerships (RTPs), the Scottish Government and others.

The meetings included –

- Scotland Railway Planning in Glasgow on 26 June, 2012
- Scottish Council for Development and Industry in Aberdeen on 26 June, 2012
- A press briefing in Aberdeen on 4 July, 2012
- Health and Transport Action Plan Steering Group in Aberdeen on 17 July, 2012 and 21 August, 2012
- Nestrans/ Hitrans/ Transport Scotland in relation to aviation policy in Edinburgh on 18 July, 2012
- Aberdeen and Grampian Chamber of Commerce in Aberdeen on 26 July, 2012
- Regional Transport Partnership Lead Officers' meeting in Perth on 15 August, 2012
- Local Authority and Bus Operator Forum Steering Group meeting in Aberdeen on 16 August, 2012
- Transport Scotland in Aberdeen on 20 August, 2012

The Board resolved:

- (i) to note progress on liaison arrangements with other Regional Transport Partnerships, the Scottish Government, and others; and
- (ii) to note the arrangements for future meetings as detailed within the report.

REGIONAL TRANSPORT STRATEGY REFRESH

4. The Board had before it a report by the Director which brought members up to date with the refresh of the Regional Transport Strategy that was being undertaken. The report presented proposals for the strategic environmental assessment, equalities impact assessment and consultation on the refresh.

The Director advised that it was his intent to consult Aberdeen City and Aberdeenshire Councils on this report.

The Board resolved:

- (i) to agree that the draft main issues report be circulated for consultation;

- (ii) to note the proposals in terms of the strategic environmental assessment and equalities impact assessment processes;
- (iii) to agree the proposals as detailed in the report in relation to the proposed consultation process; and
- (iv) to instruct the Director to arrange a workshop with members of the Board, in order for their views to be taken into account.

PROGRESS REPORT

5. With reference to article 8 of the minute of its previous meeting of 20 June, 2012, the Board had before it a progress chart summarising the work in the three sub strategies of the Regional Transport Strategy as at 22 August, 2012.

A number of queries were raised in connection with Western Peripheral Route, and it was agreed that the Director would write to Transport Scotland prior to the Regional Transport Strategy being circulated for consultation, in order that Nestrans would be in a position to answer these questions if they arise during the consultation period.

The Board resolved:

- (i) to note the content of the progress chart; and
- (ii) to instruct the Director to write to Transport Scotland prior to the Regional Transport Strategy being circulated for consultation, in order to get a formal response on the following questions (1) what is the current projected timetable for the Western Peripheral Route; (2) how long will it take from the end of any Court action, to commencement on site; and (3) have all legal avenues been exhausted at this stage?

DECLARATION OF INTEREST

During discussion of the following item, Derek Provan declared an interest due to his position as Managing Director at Aberdeen Airport. Mr Provan chose to remain in the meeting during this discussion.

PUBLICATIONS AND CONSULTATIONS

6. With reference to article 8 of the minute of its previous meeting of 20 June, 2012, the Board had before it a report by the Director which summarised and advised on a number of different publications and consultations as follows and sought approval of draft responses as appropriate –

- Inter City East Coast Rail franchise
- Ministerial Statement on Railways
- High level output statement
- First Aberdeen network changes

Draft Aviation Policy Framework.

A discussion took place in relation to the draft Aviation Policy, and it was suggested that it would be helpful if statistics and/ or other evidence could be provided in relation to emerging markets, and what the income for the area would be if the current projections are realised. The Director suggested that ACSEF or Scottish Enterprise may be able to assist with this.

The Board resolved:

- (i) to note the report and the documents referred to above, and to endorse the comments as Nestrans' response to the consultations; and
- (ii) to note that the draft response of the Aviation Policy would be brought to the next meeting for approval.

STATEMENT OF ACCOUNTS AND EXTERNAL AUDITOR'S REPORT

7. The Board had before it (1) a report by the Treasurer which presented the audited statement of accounts for 2011/12, and (2) a report by Deloitte which advised that they had undertaken an audit of Nestrans' financial statements, and presented their findings in this regard.

The Board resolved:

- (i) to note the audited statement of accounts for 2011/12, and the report by Deloitte; and
- (ii) to agree that a £10 charge for printing a copy of the statement of accounts be made; and
- (iii) to note that the statement of accounts could be downloaded free of charge from the Nestrans website.

BUDGET MATTERS

8. With reference to article 13 of the minute its meeting of 18 April, 2012, the Board had before it a report by the Treasurer which provided an update on spend and programming of the Partnership's 2012/13 budget and forecast outturn in this regard.

The Board resolved:

to note the monitoring position and forecast.

INFORMATION BULLETIN

9. With reference to article 14 of the minute of its previous meeting of 20 June 2012, the Board had before it a report by the Director which provided information and updates for the Board on a number of matters not requiring a decision as follows –

- Aberdeen to Stansted route
- Hydrogen buses launch

Stagecoach green bus launch
Jet 727, Jet connect and Deeside corridor audio announcements
launch
West coast main line franchise award
ARI bus interchange
Laurencekirk junctions
North east bus fares and ticketing strategy update
Nestrans' press releases.

The Board resolved:

to note the content of the bulletin.

CONFERENCES AND PRESENTATIONS

10. With reference to article 15 of the minute of its previous meeting of 20 June, 2012, the Board had before it a report by the Director summarising recent and forthcoming conferences of interest to the Partnership along with presentations by Nestrans and its partners.

The Board resolved:

to note the information as presented.

PENDING BUSINESS AND REPORTS FOR FUTURE MEETINGS

11. With reference to article 16 of the minute of its previous meeting of 20 June 2012, the Board had before it a report by the Director detailing pending business and information on reports to be submitted to future Board meetings.

The Board resolved:

to note the information

- **PETER ARGYLE, Chairperson.**

Appendix B
Bridge of Don Park and Ride Site Economic Assessment Summary

7 SUMMARY AND CONCLUSION

7.1 Summary

Aberdeen City Council (ACC) appointed SIAS Limited (SIAS) under the North East Framework Commission to undertaking an Economic Assessment of the potential options for a A90(N) Park & Ride site.

The four transport objectives of a Bridge of Don Park & Ride are to:

- 1 Support the implementation of the Local Development Plans in a manner which contributes to reduced congestion, improved journey times and benefits public transport users
- 2 Support the Regional and Local Transport Strategies by encouraging modal shift from private car use to public transport and active travel modes
- 3 Deliver overall environmental benefits
- 4 Reduce negative environmental impacts to a minimum level when developing infrastructure, including provision of appropriate mitigation measures

The study has included a Demand Assessment Process for the AM and PM Peak Periods on selection Options that utilise a range of data sources to determine potential demands for a series of site options with estimates of potential minimum and maximum occupancy utilisation. From the option analysis it has been possible to evaluate the transport impacts of each option in relation to a 2016 Reference Base condition, in turn allowing a comparison between the merits of each option for specific study objectives. The Reference Base case uses the existing utilisation rates and occupancy of the Bridge of Don Park & Ride applied to 2016 passing traffic flows, to give a baseline condition for 2016 to compare other options against. The economic assessment of Options was based upon 81% occupancy of the Park & Ride sites' car parking spaces by the end of the AM peak period.

The Demand and Economic Assessment has considered the following Park & Ride site options:

- Option 1 Do-Nothing: Closure of Existing P&R
- Option 2 Do-Minimum: Existing 500 space P&R Site remains open with increased utilisation
- Option 6 Closure of Existing and opening of 1,000 space AECC P&R

7.2 Conclusion

To conclude; based on the potential benefits resulting from the Demand and Economic Assessment, the following site Options provide the most potential benefits to the local network:

- Option 2 Do-Minimum: Existing 500 space P&R Site remains open with increased utilisation
- Option 6 Closure of Existing and opening of 1,000 space AECC P&R

Option 2 and Option 6 demonstrated positive net present value of benefits to the economy of between around £32 million to £49 million over 25 years. To yield the maximum benefits shown in the Economic Assessment for Option 2 and Option 6, improved Park & Ride utilisation rates require to be achieved.

To be able to economically define the benefit to cost ratio of Option 2, it is recommended that any costs associated with increasing the utilisation to achieve maximum usage are included in the analysis.

The results of the Economic Assessments for Site Option 1 (Do-Nothing: Closure of Existing Park & Ride) showed that there may be at the detriment of the local network, with a negative net present value to the economy of around £46 million over 25 years.

Appendix C
Nestrans Board Report – Strategic Transport Fund

NORTH EAST SCOTLAND TRANSPORT PARTNERSHIP – 9 October 2012

Strategy -

3b Strategic Transport Fund

o Purpose of Report

The purpose of this report is to update the Board on the implementation of the Strategic Transport Fund (STF) since the adoption of the supplementary guidance 'Delivering Identified Projects through a Strategic Transport Fund'. It also outlines the proposed timescales for the next steps.

The report also asks the Board to designate two board members as 'Proper Officers' able to sign Section 75 agreements relating to the STF in the absence of the Director.

o Background

The Strategic Development Planning Authority adopted the supplementary guidance in December 2011. This guidance is non-statutory supplementary guidance in support of the Aberdeen City and Shire Structure Plan 2009 and was adopted with the intention that it would be reviewed and adopted as statutory guidance as part of the new Strategic Development Plan.

The purpose of the supplementary guidance is to seek contributions from housing and non-residential development for the delivery of identified strategic transport projects identified as required as a result of the scale of development proposed in the development plan. The calculations proposed for housing are based on a per unit cost linked to the number of bedrooms with an average cost of £2,064 per house.

For employment uses, a per hectare contribution is applied based on the Use Classes Order and weighted on the basis of relative land value and trip generation with an average cost per hectare of £62,010.

These contributions are to be used specifically for the delivery of the strategic transport projects identified through the Cumulative Transport Appraisal and developers will continue to make appropriate payments towards local transport improvements and other required infrastructure such as education and affordable housing.

o Update on the Fund

Since the adoption of the supplementary guidance in December 2011, contributions have been formally agreed for four developments. Three of these have been agreed through Section 75 agreements for which payment will be received on completion of the development and one through a payment upfront for which the 20 year period for retention of these funds will start on implementation of the planning permission.

These agreements account for a total agreed contribution of £1,286,000 of which £43,000 has already been paid.

Discussions are taking place within the two Councils on a large number of other sites that are coming forward and that will require to make an STF contribution.

Full details of the contributions received into the fund and expenditure will be provided in the Nestrans Annual Report for 2012/13.

o **Proposed timescales**

There are two streams of work that are currently being taken forward in relation to the STF:

- Prioritisation of the strategic infrastructure interventions; and
- Review of the supplementary guidance (required prior to adoption as statutory supplementary guidance under the Strategic Development Plan). This provides the opportunity to review and respond to a number of points that have arisen during the early implementation.

It is proposed that a discussion paper be prepared that covers both of these issues and that this is used as the basis to consult with the development industry through a stakeholder workshop in late 2012.

The prioritisation of infrastructure would then be reported back to the Board in December while the comments received on the supplementary guidance would be used to inform a review of the document and production of a revised draft for further consultation. The timescales the SDPA are working to in terms of producing the final supplementary guidance are that it be complete by the end of 2013 at the latest so that it is in a position to be adopted alongside the approval of the Strategic Development Plan.

o **Proper Officer**

At its meeting in April 2012, the Board homologated the decision to appoint the Nestrans Director as a Proper Officer under Schedule 1, paragraph 13 of the Transport (Scotland) Act 2005 which states that “a transport partnership shall appoint one of its employees as its proper officer for the purposes of sections 190, 191 and 193 of the Local Government (Scotland) Act 1973”. The provisions under those sections of the 1973 Act refer to the service of legal proceedings and notices and the authentication of documents. This decision allows the Director to sign Section 75 agreements that relate to the Strategic Transport Fund on behalf of Nestrans.

Legal advice has been sought on designating further Proper Officers who are able to sign these agreements in the absence of the Director. It has been recommended by legal that the Board designate two of its Councillor members as Proper Officers who can perform this role if necessary. It is proposed that the Chair and Deputy Chair of Nestrans would be the most appropriate for this role.

o **Recommendation**

The Board is recommended to:

- Note the update on payments into the Strategic Transport Fund;
- Agree the proposed timescale for prioritisation;
- Note the proposed timescale for the SDPA to carry out a review and update of the supplementary guidance.
- Agree the appointment of the Chair and Deputy Chair as Proper Officers able to sign Section 75 agreements for the Strategic Transport Fund on behalf of Nestrans.

KM 26 September 2012

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ABERDEEN CITY COUNCIL

COMMITTEE:	Enterprise, Planning and Infrastructure
DATE:	6 November 2012
DIRECTOR:	Gordon McIntosh
TITLE OF REPORT:	Permit Misuse Policy
REPORT NUMBER:	EPI/12/224

1. PURPOSE OF REPORT

To update the Committee on the monitoring of permit misuse which has been undertaken to inform the development of a formal policy for dealing with misuse of all parking permits within the city, such as residents' permits, business permits, contractors' permits and Blue Badges.

2. RECOMMENDATION(S)

1. That the committee notes the levels and type of Blue Badge misuse and instructs officers to deal with such misuse by adopting the policy detailed in Appendix A.
2. That the Committee instructs officers to undertake an additional three month monitoring period for residential parking permits and to report back to the EP&I Committee in January regarding the progress of the monitoring period.

3. FINANCIAL IMPLICATIONS

It is anticipated that the cost of staff time to monitor and deal with Blue Badge misuse can be absorbed within current staffing levels. However, depending on the results of the extended monitoring period for other permits and the levels of misuse found, the creation of a specific enforcement team dealing with permit misuse may be required.

4. OTHER IMPLICATIONS

None

5. BACKGROUND / MAIN ISSUES

5.1 Background

5.1.1 There has been an increasing number of complaints from the public regarding perceived misuse of Residents' Parking Permits and officers are also aware of attempts to sell and purchase such permits.

5.1.2 An initial report on permit misuse was submitted to the EP&I Committee on 11 September. The report outlined proposals to carry out a monitoring period from July to September 2012, to ascertain the levels of permit misuse, including misuse of Blue Badges and residential permits.

5.2 Blue Badge misuse

5.2.1 In order to determine if Blue Badges were being used as intended, it was necessary for the City Wardens to be present when the vehicle was arriving or leaving and for the Badge to be inspected.

5.2.2 When the City Wardens were present where a driver or passenger was still in a car or entering/exiting a car displaying a Blue Badge, they asked to see the Badge. Wardens inspected Blue Badges on 42 occasions and found 18 instances where the Badge was not being properly used. These are detailed in Appendix B.

5.2.3 Each Blue Badge holder is provided with a copy of the Transport Scotland leaflet "The Blue Badge Scheme in Scotland – Rights and responsibilities of a Blue Badge holder".

5.2.4 Section 4 of this leaflet details common instances of Badge misuse, and states that these include the following:

- Allowing family members or carers to use your badge when you are not with them to do something on your behalf, such as shopping or collecting something for you
- Allowing non disabled people to take benefit from your badge while you sit in the car
- Displaying a badge photograph side up so that the details on the front cannot be read
- Copying or tampering with the badge
- Using the badge if it is expired or if you are no longer eligible

5.2.5 In instances of serious misuse, further investigation would be required to a standard that would secure a conviction, and would require the agreement of the Procurator Fiscal to pursue a case against the Badge Holder. Without this the Badge cannot be withdrawn, as per The Disabled Persons (Badges for Motor Vehicles) Scotland Regulations 2000. Under the same legislation however, the Council can refuse to issue another Badge upon expiry, and this can be done without having to secure a conviction.

5.2.6 The instances of misuse are mostly of a minor nature and appear to be the result of Badge Holders not familiarising themselves with their duties and responsibilities as a Badge Holder.

- 5.2.7 It is anticipated that the City Wardens will continue to ask to inspect Blue Badges as part of their daily duties and will then pass any instances of misuse on to the Road Safety and Traffic Management Team for further action
- 5.2.8 The instances of misuse are generally in line with that experienced by the majority of other local authorities in Scotland, with the exception of the City of Edinburgh Council.
- 5.2.9 A proposed policy is outlined in Appendix A. This policy is in line with the approach adopted by other local authorities, again with the exception of the City of Edinburgh Council, which has created a dedicated parking enforcement team.
- 5.2.10 It is therefore recommended that the Council adopt the policy outlined in Appendix A, to deal with misuse of Blue Badges.

5.3 Misuse of other parking permits

- 5.3.1 It has proved more difficult to obtain evidence of misuse of residents' parking permits etc than it has for Blue Badge misuse. This appears to be because the residents' permits are being used by commuters, and therefore the Wardens have to be on site at exactly the time the driver is parking or driving away, which can be over the course of a couple of hours in the morning and again in the evening.
- 5.3.2 In the case of Blue Badges, drivers are parking at various times of day which coincides with the Wardens carrying out their patrols and this misuse is therefore easier to confirm.
- 5.3.3 With misuse of residents' permits, if being used for commuter parking then it is dependent on the working hours of the driver when they are parking. Wardens may either arrive too late to witness the car being parked or driven away, or they may have to wait on site for a long period of time which severely impacts on their other duties and inhibits their ability to deal with other parking issues. There have therefore only been two instances which the Wardens have been able to fully investigate and confirm as misuse.
- 5.3.4 In both these instances, the Wardens waited for a considerable period of time on site, in order to speak to the driver. Both instances were misuse of a flexible parking permit, where a resident had purchased this permit and sold it to a colleague. In both cases, the Warden asked that the permit be surrendered and the driver did so.
- 5.3.5 The permit holder has been written to and reminded that the conditions of issue state that the permit may be withdrawn if it is been misused, including if it has been sold.
- 5.3.6 There is no policy in place with regard to the circumstances under which the Council would seek to withdraw a permit, and whether it would wish to pursue a conviction for fraud before doing so.

5.3.7 With the publication of the report to the September EP&I Committee, officers also received an email of support from a resident, stating they were pleased to see the monitoring period being undertaken and alleging widespread misuse of residents' permits in a specific area.

5.3.8 Given the difficulties in monitoring the misuse as outlined above it is recommended that a further monitoring period be undertaken, to target misuse of residents' permits and to investigate the claims made by the resident in response to the previous report.

5.4 Conclusion

5.4.1 On conclusion of the initial three month monitoring period, officers have a clearer understanding of the nature of Blue Badge misuse within the city, and propose to deal with this by the adoption of a policy outlined in Appendix A.

5.4.2 Officers do not feel that they have been able to adequately gauge the type and extent of residential parking permit misuse, and therefore propose that a further three month monitoring period be carried out to target this misuse, with a report to be provided to the January EP&I Committee.

6. IMPACT

Within the Community Plan, protecting and enhancing the built and natural environment is identified as a strategic priority, and underpinning the Community Plan is an aim to deliver Local & Regional Transport Strategy commitments by, among other means, using enforcement.

7. BACKGROUND PAPERS

Outstanding Business statement of Enterprise Planning and Infrastructure Committee meeting on 31 May 2012, the allocation of Business Permits to Offices Report submitted to the Controlled Parking Area Working Group meeting on 5 April 2012 and the Permit Misuse Policy report no EPI/12/151, submitted to the EP&I Committee on 11 September 2012.

8. REPORT AUTHOR DETAILS

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Appendix A

Enforcement of Blue Badges

Note: Enforcement of Blue Badges is subject to a Code of Practice in accordance with The Chronically Sick and Disabled Persons' Act 1970 and The Disabled Persons (Badges for Motor Vehicles) Scotland Regulations 2000, both as subsequently amended.

- Some flexibility in using powers, in order to address local circumstances
- Where the Badge Holder remains in the car to enable the driver or passenger to take advantage of the Blue Badge, the City Wardens will remind the Badge Holder about conditions of use and the Council will write to the Badge Holder reminding them of their duties and responsibilities in ensuring correct use of the Badge
- Where a family member or friend has been found using a Blue Badge when not entitled to do so, the City Wardens will issue a PCN and the Council will write to the Badge Holder reminding them of their duties and responsibilities in ensuring correct use of the Badge
- For the above instances of misuse, PCN's will be issued on each occasion of misuse, letters will be sent out on two occasions and on the third occasion the Council will class this as habitual misuse and will ask the Procurator Fiscal to prosecute under The Chronically Sick and Disabled Persons' Act 1970 and then withdraw the badge once a conviction has been secured, as per The Disabled Persons (Badges for Motor Vehicles) Scotland Regulations 2000
- Where a Badge is habitually misused, under The Disabled Persons (Badges for Motor Vehicles) Scotland Regulations 2000 the Council will refuse to issue a new or replacement Badge, where it has reasonable grounds to believe that the applicant would permit another person to use it.
- Where a Badge has been found to have been copied, altered, faked or forged, the Council will ask the Procurator Fiscal to prosecute under the Road Traffic Regulation Act 1984
- Where a Badge has been found to have been copied, altered, faked or forged, under The Disabled Persons (Badges for Motor Vehicles) Scotland Regulations 2000 the Council will refuse to issue a new or replacement Badge, due to the seriousness of this offence
- Where a lost/stolen Badge has been found to be used by someone other than the Badge Holder, the Council will also ask the Procurator Fiscal to prosecute the offender under the Road Traffic Regulation Act 1984

Appendix B
Misuse of Blue Badges

Observed number of occasions of Blue Badge misuse	Type of misuse	Action Taken
7	Badge holder remained in car while able bodied driver or passenger entered shops	The Badge Holders were reminded of the conditions of use and it was explained that any driver or passenger who was not mobility impaired should be using a pay and display or other available parking bay.
5	Expired Badge was displayed	These appeared to have been genuine errors by the Badge Holders. No reminders are issued and it is the duty of the Badge Holder or their carer to renew the Badge as required. The Badge Holders were advised to apply for a new Badge and return the expired Badge.
3	Family members were found to be using the Badge without the Badge Holder being present.	Drivers were advised of the requirements for Badge use and the Badge Holders have been written to with a request for an explanation into the circumstances under which their Badge was misused. They have also been reminded of the duties and responsibilities of a Badge Holder and advised that being convicted of misusing the Badge carries a fine of £1000 and can result in withdrawal of the Badge.
1	Badge Holder was parked on "School Keep Clear" markings	Advised that this constituted misuse of the Badge. The Badge Holder has also been written to as above.
1	Driver was parked illegally and claimed to be a Badge Holder but said his Badge was on a friend's car.	Driver has been written to as above, and also reminded that City Wardens should not be subject to abusive language when carrying out their role.
1	Driver of illegally parked vehicle was questioned and produced a Blue Badge, but Wardens were still suspicious and issued a PCN	Badge was reported lost within a month of issue and a duplicate Badge was issued. Grampian Police were contacted, as it was suspected that the lost badge may have been found and was being misused. The Police spoke to the Badge Holder and reminded him of his duties and responsibilities. The Badge Holder has been written to, asked for a written explanation of the circumstances and has been asked to return the duplicate Badge

ABERDEEN CITY COUNCIL

COMMITTEE	ENTERPRISE, PLANNING & INFRASTRUCTURE
DATE	6 November 2012
DIRECTOR	GORDON McINTOSH
TITLE OF REPORT	Garthdee Controlled Parking Area- Permit Charges
REPORT NUMBER:	EPI /12/233

1. PURPOSE OF REPORT

This report advises the Committee that the current legal agreement with Robert Gordon University to meet the administration and enforcement costs for the Controlled Parking area within Garthdee expired on the 30th September 2012 and that consideration should be given to the introduction of charges for residents permits.

2. RECOMMENDATION(S)

That the committee ;

- a) Approve the introduction of residential parking permit charges for the existing Garthdee controlled parking area in line with existing peripheral parking areas.
- b) Instruct officers to amend the Garthdee Controlled Parking Traffic Regulation order through the legislative process to include the agreed permit charges

3. FINANCIAL IMPLICATIONS

- 3.1 There will now be an ongoing revenue cost to this council for the administration, enforcement and maintenance of the existing Garthdee controlled parking area for which there is no budget allocation. In order to meet budget expectations it would be necessary to apply a charge for residential permits

4. OTHER IMPLICATIONS

None at this time

5. MAIN ISSUES

- 5.1 The controlled parking area within the Garthdee area has now been operational for 10 years and was implemented to support sustainable transport objectives for travel to the RGU campus and to protect the residential environment / amenity of Garthdee from indiscriminate commuter parking.
- 5.2 The need for the controlled parking area was recognised within the Transportation Assessment submitted in support of the planning application for the expansion of the Garthdee campus and implemented in line the approved planning conditions.
- 5.3 In association with the planning permission RGU entered into a legal agreement that required that they meet the administration and enforcement costs for the zone for a period of 10 years. The legal agreement expired at the end of September 2012 and concluded RGU's obligations in this respect.
- 5.4 Since the introduction of the controlled parking area residents have not been charged for the issue of residential parking permits as there have been no direct costs to this council in relation to the administration of permits and enforcement of the parking restrictions.
- 5.5 The costs for the administration and enforcement of the controlled parking area will now have to be borne in full by this council. It is considered that given the current financial constraints that in order to meet the costs associated with the issue of permits and enforcement that it would be prudent to introduce charges for permits in line with those for the other peripheral controlled parking areas at Foresterhill and Old Aberdeen.
- 5.6 The residents within the Garthdee area have an entitlement to two permits per house in line with the other similar areas. At present an average of 1100 residential permits have been issued for the area with many issued to residents who have available off street parking but also choosing to take up the option for permits. Should charges be introduced it is reasonable to assume that the demand for permits will diminish with only those requiring on street parking applying for permits when existing permits expire. However experience at Foresterhill following the introduction of charges for residential permits has shown an overall increase in demand for permits.
- 5.7 It is thought that there is an element of abuse of the existing permit system related to parking at the university campus and it is considered that the introduction of permit charges may assist in the reduction of this abuse.
- 5.8 In conclusion it is proposed that in order to off set the cost associated with the operation of the Garthdee controlled parking area that residential parking permit charges, in line with the existing peripheral areas (the

current annual charge for permits within the peripheral zones is £80 for the first permit and £120 for the second) be introduced.

6. IMPACT

Corporate - This report links to the Service Plan for Enterprise, Planning & Infrastructure Strategic Priorities 4(4), 4(5) & 5(2)

Public - This report is likely to be of interest to the public as it involves a potentially significant alteration to a junction on one of the main arterial corridors in the city.

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

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Consultees comments

Councillor Angela Taylor – *has been consulted 02/10/12*

Councillor Gordon Townson – *has been consulted 02/10/12*

Councillor Ian Yuill – *has been consulted 02/10/12*

Council Officers

Barry Jenkins, Head of Finance, Corporate Governance – *has been consulted*

Jane MacEachran, Head of Legal and Democratic Service – *has been consulted*

Ciaran Monaghan, Head of Service, Office of Chief Executive – *has been consulted*

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – *has been consulted*

Hugh Murdoch, Head of Asset Management and Operations, E,P and I – *has been consulted*

Margaret Bochel, Head of Planning & Sustainable Development – *has been consulted*

Mike Cheyne, General Manager, Operations – *has been consulted*

Neal Carnegie, Community Safety Manager – *has been consulted*

Dave Young, Account Manager, Corporate Governance – *has been consulted*

Laura Watson, Service Co-ordinator E P & I

Mark Masson, Committee Services Officer

ABERDEEN CITY COUNCIL

COMMITTEE	Enterprise Planning and Infrastructure
DATE	6 November 2012
DIRECTOR	Gordon McIntosh
TITLE OF REPORT	City Wide Flooding Issues
REPORT NUMBER:	EPI/12/240

1.0 PURPOSE OF REPORT

To update the Committee on recent city wide flooding incidents and to outline the ongoing work necessary to assess and manage the risk of flooding.

2.0 RECOMMENDATION(S)

It is recommended that the Committee:

2.1 Notes :-

- a) The extent of the flooding problem as experienced on 25 August.
- b) The progress made to investigate and alleviate the causes of flooding.

2.2 Approves :-

- c) That a briefing session is arranged to update elected members on the national and local strategy to risk manage and alleviate flooding.

3.0 FINANCIAL IMPLICATIONS

Minor works to improve drainage and flood protection are funded from existing revenue and capital budgets.

At present flood prevention schemes with a capital value up to £2M do not attract grant funding from the Scottish Government. Approved schemes in excess of this value can attract 80% funding. The continuation of this threshold could disadvantage the City.

4.0 OTHER IMPLICATIONS

The Council has a statutory obligation to work in partnership with the North East Local Plan District (NELPD), SEPA, Scottish Water and others to develop a Local Plan for Flood Risk Management. The plan will include prioritised schemes for flood management. The deadline for publishing the first draft plan is December 2014. Some schemes, particularly major schemes, will be eligible for grant funding. An explanation of this strategy and partnership working will be the subject of a briefing session for elected members.

5.0 BACKGROUND/MAIN ISSUES

5.1 Flooding Events – 25 August

Heavy rain on 25 August caused a large number of flooding incidents city wide, affecting both domestic and commercial properties, as well as disrupting travel. A full list of the incidents reported to the flood management team is given in appendix A. The appendix also details progress to date to mitigate recurrences of these incidents. Some of the issues can be dealt with locally; others require a robust understanding of the watercourse and drainage system to allow development of a solution, some of which may involve major capital works.

The rain on the 25th was localised, intense and of relatively short duration, because of this, no official measurement of the rainfall is available. However it is assessed that up to 30mm fell within one hour, placing the downpour in excess of a 1 in 100 year event. It should be noted however that the 1 in 100 year event is based on statistical analysis of limited historical records. If increased rainfall, as has been experienced in recent years, is the product of climate change, then the intensity of the predicted 1 in 100 year event will increase, as will the frequency of flooding.

6.0 IMPACT

There is considerable public interest in this, particularly from those affected by flooding issues and from the media.

7.0 BACKGROUND PAPERS

- a) Potentially Vulnerable Area Data Sheets 06/15-19 available at - http://www.sepa.org.uk/flooding/flood_risk_management/national_flood_risk_assessment/datasheets.aspx.
- b) Flood Risk Management Strategies and Local Flood Risk Management Plans available at - http://www.sepa.org.uk/flooding/flood_risk_management/national_flood_risk_assessment.aspx.
- c) Aberdeen Integrated Catchment Study – report to 31 May 2012 EP&I Committee.
- d) Flood Risk Management (Scotland) Act 2009 – Governance Arrangements for the North East Local Plan District (NELPD) – report to 12 September 2012 EP&I Committee

8.0 REPORT AUTHOR DETAILS

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Appendix A

Record of Flooding Incidents for Saturday 25th August 2012

Incident Number	Location	Description of Incident	Action Taken
1	Jack Brae	Denburn – out of channel flow – resulting in flooding of garage & yard. This is an area of repeat problems. Work has already been carried out to prevent flood water flowing down Jack's Brae an into Denburn car park etc.	Survey carried out 28/08/12. Owner requested meeting with ACC / SW / Lawyer Discussed with SW & Planning 03/09/12. Meeting with Owners 17/09/12 & 01/10/12. Lower trash screen removed 21/09/12. ACC to carry out work to adjust wall heights and driveway levels as soon as budgets permit. Owner to fit non-return valves to drains. Owner to propose alterations to burn walls and ACC to facilitate meeting with Conservation Team to discuss Sandbags issued to homeowner on 12/10/12
2	Braeside Avenue	Flooding in rear garden. Blocked Culvert.	CCTV survey carried out. discussed with SW 03/09/12. Blockage Cleared.
3	Golf Road / Accommodation Road	Social club flooded. Manhole surcharged at junction of Pittodrie Street. Overloading of combined sewer due to rainfall	Raised with SW 03/09/12. Social club to discuss with SW. Discussed with SW on 15/10/12 – problem a result of ceased valve at York Street pumping station – valve has been repaired.
4	Kirk Brae	Culvert unable to cope with volume of water	Scheme to be implemented by ACC as soon as resources permit.
5	Fountainhall Road / Albert Lane	Property flooded - water coming from Denburn overflowing in private lane & surface water on Fountainhall Road. The Denburn had reached capacity where it flows under Fountainhall Road. Detailed assessment of Denburn Capacity Required.	Survey carried out 29/08/12. Consultant employed by local businesses. Meeting with Consultant on 17/09/12. Meeting with Councillor Jennifer Stewart & affected proprietors Temporary sand bag wall built 24/09/12 –to be replaced with permanent masonry wall – masonry wall completed by 11/10/12. Sandbags issued on 12/10/12 Meeting arranged with SW for 15/10/12 to discuss joint survey of sewers and drains in area. Joint survey agreed.
6	Jesmond Place	Manhole surcharged, lifting road surface	Manhole reset - waiting on a vented cover to replace existing cover. Discussed with SW on 03/09/12. SW responsibility.
7	Langstracht	a) Flood Waters	Survey required - Large runoff from Garden centre. Gullies don't connect directly into carrier drains. It is assumed that the outlets are blocked / silted up. - To be investigated.
		b) School flooded	CCTV required to inspect condition of Springhill culvert. The SW combined sewer may not have had capacity. Discussed with SW on 15/10/12 – SW to look at improving details on combined sewer.

		c) Rear garden flooded	Met with owner 30/08/12. Gullies have been cleaned & members enquiry responded to. Langstracht scheme required to alleviate future flood incidents.
		d) Eday Walk - property flooded	Langstracht scheme.
8	Abbotswell Road	a) Water run off from ACC road	Awaiting materials to programme works. ACC to repair gully / possibly add another large capacity gully.
		b) Property flooded twice	Awaiting materials to programme works. ACC to repair gully. Drainage channel to be installed – to Tullos burn.
9	Monymusk Terrace	Flooded greenhouse	ACC have cleared blockage under Craigbuckler Avenue - Replied to Councillor Greig.
10	North Deeside Road	a) Flooding at Bairds Brae	Either SW or private matter.
		b) Muddy water coming up the toilet & bath / surface water from road	SW to fit non-return valves.
11	Westholme Avenue	Partial blockage of culvert	Burn cleared of vegetation etc upstream of bridge.
12	Midstocket Road	Flooding in garden	Owner given advice on how to protect property with flood boards and vent covers.
13	Den of Cults	a) Flooding from Cults burn has damaged road surface	Private road / burn - Owners to repair.
		b) Flooding from Cults burn has damaged road surface	Private road / burn - Owners to repair.
		c) Flooded garden & damage to boundary retaining wall	Private road / burn - Owners to repair.
14	Greenbank Road	Flooding of yard	Owner is arranging the cleaning of internal drains.
15	Abbotshall Gardens	Water in back garden	Survey carried out 26/09/12 Lack of road drainage - run off into gardens. Being investigated. Possible regarding of road.
16	Derbeth Grange	Water surfacing between fence line of two properties	Investigation ongoing - Site drawings obtained from developer.
17	Deeside Gardens	Flooding in back garden	Discussed with SW 03/09/12. No reported flooding on 25/08/12 therefore improvement works by owner may have been effective.
18	Craigieburn Park	a) Silt & foam in pond	ACC met Chair of residents association on 18/09/12. Advice given on how to protect pond from silting & responded to Councillor Greig.
		b) Dip in road & lifting of carriageway	SW responsibility
19	Culter House Road	Water running down Culter House Road	ACC cleared blocked drain near Bellend Walk.

20	Pitfodels Station Road	Water running into garden from road during heavy rain.	ACC met with owner on 06/09/12. Arranged to get gullies cleaned. The land profile has been altered. ACC issued letter to owner's agent.
21	Greenwell Road	Standing water at dip in road	Tullos cleared two gullies in dip of road.
22	Countesswell Road	Ground water / Retaining wall	Council Engineer visited on 04/09/12. Owner to discuss with solicitor
23	Earlspark Crescent	Standing water at boundary wall	Council Engineer met with owner 12/09/12 Upstand to be installed.
24	Springfield Road	Manhole on footpath nr. Braeside Ave	SW to fix manhole - road inspector to check.
25	Sycamore Place	Road flooding	Gullies cleaned
26	Dubford Park	Water running between property & sub station	New gully to be installed.
27	Manor Drive	Edge of carriageway & footway lifted by flood water	Site investigation carried out 27/09/12. Evidence of water issuing from SW manhole on the combined sewer on Manor Drive.
28	Springfield Place		SW responsibility.
29	Hazledene Road	Flooding to warehousing etc on Council owned site.	Culvert size inadequate. Situation could be improved by larger culvert or removal of culvert and reinstatement of open channel. To be discussed with tenant.

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ABERDEEN CITY COUNCIL

COMMITTEE	ENTERPRISE, PLANNING & INFRASTRUCTURE
DATE	6 November 2012
DIRECTOR	GORDON McINTOSH
TITLE OF REPORT	MARKET STREET/GUILD STREET JUNCTION – RATIONALISATION OF TURNING MOVEMENTS TO IMPROVE CAPACITY
REPORT NUMBER:	EPI /12/227

1. PURPOSE OF REPORT

This report seeks to advise the Committee of a proposal to improve the capacity and operational efficiency of the traffic signal controlled junction of Market Street and Guild Street through the rationalisation of turning movements.

2. RECOMMENDATION(S)

That the committee ;

- a. Instruct the appropriate officers to introduce of a temporary ban on the movement of vehicles westbound from Virginia Street into Guild Street over the period from 19/11/12 until 28/1/13.
- b. Instruct the appropriate officers to commence the necessary legal procedures for the introduction of a permanent ban on the movement of vehicles westbound from Virginia Street into Guild Street.

3. FINANCIAL IMPLICATIONS

Funding will require to be identified to allow for the necessary Traffic Regulation Orders (TRO's) to be introduced. Physical alterations to the geometry of the Market Street/Guild Street junction will be required should the permanent ban be implemented and the cost of the necessary works is estimated at £15000. Should the temporary ban be approved temporary signing works will be necessary the cost of which is estimated at £2000. The temporary works cost can be accommodated within the existing Traffic Management and Road Safety budget and whilst the works for the proposed permanent TRO would be included in the Traffic Management and Road Safety budget allocation for 2013/14.

4. OTHER IMPLICATIONS

It is anticipated that the introduction of a permanent ban on the movement of vehicles westbound from Virginia Street into Guild Street will reduce the potential for traffic conflicts at this busy junction and will improve junction efficiency thereby reducing congestion, delays and air pollution.

5. BACKGROUND/MAIN ISSUES

Traffic volumes on the Market Street and Guild Street corridor are consistently high with the junctions operating at or beyond capacity. Since the opening of the Union Square development in October 2009 traffic volumes on Market Street have increased, particularly at weekends and in the early evening. As a result traffic congestion and delays have increased during the morning and evening peak periods and at peak shopping times. The existing traffic signal phasing and conflicting east / west traffic movements on Guild Street and Virginia Street restrict the capacity of the junction and regularly create long delays for eastbound traffic movements on Guild Street, impacting on the journey time for buses exiting the bus station.

At the weekends, during the run-up to Christmas, traffic levels increase, significant congestion occurs which further influences the operation of the surrounding road network leading to increased delays and driver frustration.

In addition complaints have been received from bus operators regarding difficulties experienced, on a day-to-day basis, by drivers wishing to turn right from Guild Street into Market Street. Following a period of monitoring by officers alterations were made to the traffic signal timings providing additional green time to turning traffic. Although the situation has been improved it has not been fully resolved, further improvements can only be realised through re-allocating green time from another traffic stage.

Banning the westbound straight through manoeuvre from Virginia Street onto Guild Street will allow the operational efficiency of the Market Street/Guild Street junction to be improved. This improvement will be achieved by the reduction in the number of traffic stages from 4 to 3 which will also lead to reduction in the amount of time lost during stage changes.

Traffic monitoring data indicates that approximately 240 vehicles per hour travel westbound from Virginia Street into Guild Street, this equates to around 11 vehicles per cycle being redistributed onto the surrounding network. It is considered that there is sufficient capacity to

accommodate this redistribution within the existing city centre road network.

From a traffic modelling perspective these changes will be most beneficial to traffic proceeding eastbound on Guild Street. Currently drivers wishing to travel from Guild Street onto Virginia Street only use the nearside lane as traffic in the offside lane is frequently delayed by right turning traffic unable to complete their manoeuvre by oncoming vehicles. They are only released when this opposing traffic is stopped. If there is no opposing traffic, vehicles turning right into Market Street from Guild Street will experience far fewer delays, which will encourage full utilisation of both traffic lanes.

It is also considered that further minor efficiencies will also be gained through the proportional redistribution of the green time.

Recent Experience

Recently Scottish and Southern Energy carried out extensive works on electricity cables located under Guild Street, in front of St Magnus House.

Due to the scale of the works it was necessary to introduce significant traffic management measures to reduce Guild Street to one lane in both directions. In order to reduce the likelihood of congestion on Guild Street impacting on the operation of the Market Street/Guild Street junction a decision was taken to put in place a Temporary Traffic Regulation Order (TTRO) banning vehicles from proceeding westbound from Virginia Street into Guild Street for the duration of the works.

During monitoring of the works it was noted that during the initial period traffic congestion increased during the peak periods along Virginia Street and Union Street. However this was a short-term issue with drivers migrating onto alternative routes reducing congestion levels on Virginia Street to a more normal level.

During the period of the works no significant delays to traffic travelling eastbound on Guild Street were noted. It is considered that this is due to the reduction in traffic conflicts realised through the banning of the westbound straight through manoeuvre from Virginia Street.

Interim Short Term Proposal

Given the traffic congestion experienced at the junction of Market Street and Guild Street during the festive period it is felt that introducing a ban on vehicles travelling westbound from Virginia Street onto Guild

Street would, as a short-term measure over the Christmas period and into mid January, prove beneficial.

The high cycle time, 160 seconds, that this junction has to run in order to manage the traffic flowing through it, limits the ability of officers to make changes to the operation of adjacent junctions without detrimentally affecting the linkages between them.

Reducing the number of traffic stages, as outlined above, will permit a reduction in the cycle time and allow officers greater scope for improving the efficiency of the network and aid the management of traffic during periods of peak traffic flow.

Long term Proposal

It is anticipated that the longer term benefits gained from a permanent ban on vehicles travelling westbound from Virginia Street onto Guild Street will be enhanced by alterations to the management of vehicles entering and exiting the bus depot, further reducing congestion and delay for drivers using Guild Street and Market Street.

Conclusion

In conclusion it is considered that banning vehicles travelling westbound from Virginia Street into Guild Street will reduce congestion and delay on the local road network through the rationalisation of turning movements. It is recommended that a TTRO is introduced as a short-term measure over the festive period, whilst officers progress the introduction of a permanent order as a long term improvement.

6. IMPACT

Corporate - This report links to the Service Plan for Enterprise, Planning & Infrastructure Strategic Priorities 4(4), 4(5) & 5(2)

Public - This report is likely to be of interest to the public as it involves a potentially significant alteration to a junction on one of the main arterial corridors in the city.

7. BACKGROUND PAPERS

None

8. REPORT AUTHOR DETAILS

Donald Kinnear
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Consultees comments

Councillor Alan Donnelly – *has been consulted 02/10/12*

Councillor Graham Dickson – *has been consulted 02/10/12*

Councillor James Kiddie – *has been consulted 02/10/12*

Councillor Yvonne Allan – *has been consulted 02/10/12*

Council Officers

Barry Jenkins, Head of Finance, Corporate Governance – *has been consulted*

Jane MacEachran, Head of Legal and Democratic Service – *has been consulted*

Ciaran Monaghan, Head of Service, Office of Chief Executive – *has been consulted*

Gordon McIntosh, Director of Enterprise, Planning and Infrastructure – *has been consulted*

Hugh Murdoch, Head of Asset Management and Operations, E,P and I – *has been consulted*

Margaret Bochel, Head of Planning & Sustainable Development – *has been consulted and are comfortable with the outcomes of the modeling. Slight concern about the removal of the ability for traffic to route through to Willowbank Rd and the A93 but satisfied that traffic will find another route and for the surrounding roads to be monitored during the temporary order.*

Mike Cheyne, General Manager, Operations – *has been consulted*

Neal Carnegie, Community Safety Manager – *has been consulted*

Dave Young, Account Manager, Corporate Governance – *has been consulted*

Laura Watson, Service Co-ordinator E P & I

Mark Masson, Committee Services Officer

**ENTERPRISE, PLANNING AND INFRASTRUCTURE
COMMITTEE BUSINESS**

Following the Meeting of 11 September 2012

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
1.	Enterprise, Planning and Infrastructure Committee 26.11.09 article 18	<p><u>Berryden Corridor – Transport Infrastructure Improvements</u></p> <p>The Committee resolved to approve the preferred option for progression to detailed design (i.e. the so-called “Do Something” option for the corridor, in association with a bus gate at Bedford Road/Powis Terrace), and that the officials report back on detailed design, costs and programming.</p>	<p>Definitive dates for the major transportation projects are difficult to provide as much of the work required to get to the next key stage for reporting will either be several months away or at least more than 12 months - in which case we don't have committee dates, and as always subject to budget and resource availability. Strategic Transport Projects reports which contain updates on all these projects are provided to the Committee by way of a report or bulletin as and when necessary.</p> <p>As per the update of 24 May funding has been secured for this financial year to enable development boundaries to be established.</p> <p>Suggest that this may now be considered for removal as future progress on this item will be recorded in the Strategic and Local Transport update report which is now on every cycle of the Committee</p>	Margaret Bochel		No report required this financial year

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
2.	Enterprise, Planning and Infrastructure Committee 26.11.09 article 19	<p><u>(1) Access from the North – An Integrated Transport Solution - (2) Access from the North Proposals “Third Don Crossing”</u></p> <p>The Committee resolved, amongst other things to request a regular report back on progress in these matters, including the development of a Delivery Programme.</p>	<p>At its meeting on 18 January, 2011, the Committee resolved:-</p> <p>(a) agree, in principle, the provisional programme for delivery of the ‘Access from the North’ integrated transport proposals;</p> <p>(b) instruct officers to continue discussions with BEAR and Transport Scotland regarding options on the trunk road network;</p> <p>(c) instruct officers to progress schemes from the full delivery list as priority and funding would permit, subject to consultation and referral to future Committees as required; and</p> <p>(d) to instruct officers to keep the Committee up to date with progress of the delivery plan as timescales might be amended subject to agreement of future years spend profiles of the Non-Housing Capital budget and other relevant sources of funding.</p> <p>Future updates will be provided as funding is identified for delivery of this programme. It is proposed to</p>	Margaret Bochel		

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
			<p>update Members on an annual basis on progress of the whole programme, with separate reporting should individual elements require further Committee decisions. Where key progress is made during the year, ad-hoc updates can be provided as part of the bulletin or included within the Strategic Transport projects update report.</p> <p>A progress report was considered on 31 May 2012.</p> <p>Suggest that this may now be considered for removal as future progress on this item will be recorded in the Strategic and Local Transport update report which is now on every cycle of the Committee.</p>			
3.	Housing and Environment Committee 16.02.10 article 20	<p><u>Furnishings Contract</u></p> <p>The Committee resolved to request officers to submit a report to the Enterprise, Planning and Infrastructure Committee exploring the possibility of the overall amendment of the Council's procurement procedures to take account of social enterprises etc.</p>	<p>At its meeting on 31 May 2010, the Committee resolved, amongst other things, to instruct officers within Corporate Governance to continue to work on the development of a robust corporate policy on the use of community benefit clauses within the Council's procurements, and that the policy be submitted to a future meeting of the Committee for approval.</p>	Craig Innes	31.05.12	22.01.13

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
			<p>At its meeting on 13 September, 2011, the Committee noted that a report seeking approval of a guidance policy on delivering community benefits through procurement would be considered by the Corporate Policy and Performance Committee on 22 September, 2011, and that if approved, officers would report to the next meeting of the Enterprise, Planning and Infrastructure Committee with a user friendly guide to the policy specifically for this service.</p> <p>The Corporate Procurement Unit ("CPU") are taking a lead on community benefits and will shortly be providing officer training on this topic. Category Managers are now aligned to Services and this will allow them to provide a more tailored Service on issues such as community benefits. It is therefore thought that the CPU will be able to cater for the previously identified needs in respect of community benefits. An update report on the Council's use of the community benefit clauses will be submitted in the next 6-9 months to ensure that community benefit needs are being met.</p>			

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			<p>It is the intention of the Head of Procurement to submit a report to the next EP&I Committee to advise of recent tender exercises, the outcomes of which will be known at that time, where it has been possible to introduce community benefit clauses.</p> <p>There are a number of tenders, containing community benefit clauses specifically relating to apprenticeship schemes on construction projects which are being actively assessed at the moment. There is also a number of forthcoming requirements where the clauses will be added and Officers are scoping out as to how best specify and weight them from experience gained from the initial tenders to ensure maximum benefit is offered by interested parties.</p> <p>The CPU had hoped to advise the Committee of this progress at the November meeting but this will now be presented to the January 2013 Committee</p>			

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4.	Enterprise, Planning and Infrastructure 23.02.10 article 27 Council Budget 10.02.11	<p><u>Aberdeen Western Peripheral Route – Progress Report</u></p> <p>The Committee resolved to instruct officers to provide a further report to the appropriate Committee during the procurement process providing an updated cost estimate and programme</p> <p><u>General Fund Revenue Budget 2011/2012 and Indicative 2012/2013 to 2015/2016 Budget</u></p> <p>To instruct the Director for Enterprise, Planning and Infrastructure to enter into negotiations with the Scottish Government with a view to having the 3rd Don crossing delivered as part of the Aberdeen Western Peripheral Road Scheme and report back to the relevant Committee on the progress and outcome.</p>	<p>An appeal by Mr Walton against the judgement of Lord Tyre ([2011] COSH 131) sitting in the Outer House of the Court of Session was heard before Lords Clarke, Bonomy and Philip sitting in the Inner House of the Court of Session between 13th & 16th December 2011.</p> <p>Their Lordships delivered their judgement on 29th February 2012 dismissing the appeal.</p> <p>Following the issuing of the judgement, the unsuccessful party has a statutory right of appeal within 42 days to the UK Supreme Court in London, which acts as the final court of appeal for all United Kingdom civil cases. Mr Walton on the 12 April 2012 submitted an appeal to the UK Supreme Court. A date for a hearing has yet to be set.</p>	Margaret Bochel		06.11.12

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
			<p>An appeal by Mr Walton against the judgement of Lord Tyre ([2011] COSH 131) sitting in the Outer House of the Court of Session was heard before Lords Clarke, Bonomy and Philip sitting in the Inner House of the Court of Session between 13th & 16th December 2011. Their Lordships delivered their judgement on 29th February 2012 dismissing the appeal.</p> <p>A further appeal against that decision was made to the UK Supreme Court and was heard on 9th and 10th July.</p> <p>A report is on the current agenda.</p>			

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
5.	Enterprise, Planning and Infrastructure 15.03.11 article 4	<p>Parking Charges/Emissions-based Permit System The Committee resolved, amongst other things, in relation to recommendations from the Controlled Areas Parking Working Group -</p> <p>(i) to agree that a review of parking charges and entitlement set by other Council services be undertaken and reported to the Enterprise, Planning and Infrastructure Committee for consideration and possible revision;</p> <p>(ii) to request officers to submit a detailed report on the possibility of establishing a city wide emissions-based permit system, on the proviso that such a proposal would be revenue neutral to the Enterprise, planning and Infrastructure Committee at its meeting on 13 September, 2011;</p>	<p>An update on these are as follows:-</p> <p>(i) All other relevant Services within ACC have been contacted to request details of the locations within their service where charging occurs and to request details of the charges applied. Officers are still awaiting a response and the review may return to the Committee at its meeting on 31 January, however it is more likely to be reported on 31 May 2012;</p> <p>(ii) In relation to resolution iii, work regarding the possibility of establishing a city wide emissions-based permit system is ongoing; this will be considered at the Controlled Parking Areas - Working Party on 3 February 2012. Thereafter, the proposal will be developed in conjunction with the review of 2012/2014 parking charges to be undertaken in autumn 2012 and reported to Committee in Spring 2013.</p>	Hugh Murdoch	13.09.11	Spring 2013
6.	Enterprise, Planning and Infrastructure 13.09.11 article	South Aberdeen Coastal Regeneration Project (SACRP) – Programme Developments		Gerry Brough	31.05.12	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
	24	The Committee resolved, amongst other things, agree to receive a report back to the Enterprise, Planning and Infrastructure Committee in the April/May 2012 cycle, linking coastal regeneration initiatives into the mainstream development of climate change adaptation strategies and flood risk management.				
7.	Enterprise, and Infrastructure 13.09.11 article 39	<u>Greenbrae Cycle Project</u> The Committee resolved, amongst other things, to instruct officers to report back to this Committee on an annual basis on progress/ impact and intermittently as legislative decisions required.	Progress will be included in the Strategic and Local Transport Projects Update Report. Suggest that this may now be considered for removal as future progress on this item will be recorded in the Strategic and Local Transport update report which is now on every cycle of the Committee.	Margaret Bochel		
8.	Enterprise, and Infrastructure 13.09.11 article 41	<u>Green Badge Scheme</u> The Committee resolved to instruct officers to review the possible impact on the City Council's Green Badge scheme and to report back to a future meeting of the Committee following consultation with interested parties.		Hugh Murdoch	31.01.12	
9.	Enterprise, and	<u>Technical Advice Note : The Repair And Replacement Of</u>	The Scottish Government have recently issued new guidance on	Margaret Bochel	31.05.12	6.11.12

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
	Infrastructure 15.11.11 article 17	<u>Windows And Doors</u> The Committee resolved, amongst other things, to agree that following completion of the relevant consultation, any comments received and subsequent amendments to the draft advice note be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee.	the historic environment and on permitted development rights. This means that the document will have to be re-written. A report will be submitted in November 2012. There is a report included on this agenda.			
10.	Enterprise, and Infrastructure 15.11.11 article 18	<u>Supplementary Guidance : Wind Turbine Development In Aberdeen City</u> The Committee resolved, amongst other things, to agree that following completion of the relevant consultation, any comments received and subsequent amendments to the draft Supplementary Guidance be presented to a future meeting of the Enterprise, Planning and Infrastructure Committee.	Officers have had to carry out a Strategic Environmental Assessment on this Supplementary Guidance which is almost complete. However, SNH have indicated that we may also be required to carry out a Habitats Regulation Assessment (basically to see if the guidance would affect the River Dee Special Area of Conservation). We would intend reporting back after we do these and carry out the necessary consultation. There is a report included on this agenda.	Margaret Bochel	31.05.12	6.11.12
11.	Enterprise, and Infrastructure 31.01.12 article 42	<u>Audit Scotland Report On Transport For Health And Social Care – Self Assessment And Action Plan</u>	At this stage there is still one Health Transport Action Plan (HTAP) partner organisation to go through their self assessment. This is expected imminently following	Margaret Bochel	31.05.12	22.01.13

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
		<p>The Committee resolved, amongst other things, to request officers to report back to the Committee at its meeting on 31 May, 2012, with a detailed action plan setting out how the Council and partner organisations would tackle the areas for development following self assessment and how the Council would meet the recommendations as set out by the Audit Scotland report.</p>	<p>which all self assessments will be collated and an action plan will be drawing up as a collective with all HTAP partners. As we are not at this stage yet, officers are not able to provide the requested action plan report to Committee at this cycle.</p> <p>We are not able to report the requested action plan for this cycle. We have still not seen the self-assessment from Scottish Ambulance Service and therefore are still unable to prepare a collective Action Plan with our partner organisations. It is important that there is a joint approach to the action plan therefore we can not do this in isolation.</p> <p>All partner organisations Self-Assessments have been completed and the HTAP are cross referencing all assessments, in order that the areas needing improvement across the agencies are identified and a full action plan can be produced. It is anticipated that this can be reported to the January Committee.</p>			

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12.	Enterprise, Planning and Infrastructure 31.05.12 article 6	<p><u>Four Traffic Orders – Outcome Of Main Statutory Advertisement Stage – Newhills Avenue</u></p> <p>The Committee resolved to approve the recommendations in the report with the exception of (b)(ii) in relation to Newhills Avenue, where the Committee requested that the proposal for Newhills Avenue be removed from the Aberdeen City Council (City Wide Traffic Management Measures) (No 2), and that officers explore the possibility of providing five, rather than four, car parking spaces at the bend of Newhills Avenue, and re-commence the traffic regulation order process.</p>	<p>This scheme is about to go out to a combined Initial Statutory and public advert where the results will be reported to the January Committee</p>	<p>Hugh Murdoch</p> <p>Jane MacEachran</p>	6.11.12	22.01.13
13.	Enterprise, Planning and Infrastructure 31.05.12 article	<p><u>On-street Parking Charges at Foresterhill and Garthdee</u></p> <p>The Committee resolved, amongst other things, in relation to recommendation (d) from the Controlled Areas Parking Working Group, (Article 6 of the minute of 5 April) to agree to postpone the increase of on-street parking charges in both the Foresterhill and Garthdee Zones until a review of NHS Grampian's new parking scheme had been undertaken and that officers report back to the Committee at this time.</p>	<p>A report is on the current agenda in relation to the Garthdee area.</p>	<p>Hugh Murdoch</p>	22.01.13	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
14.	Enterprise, Planning and Infrastructure 31.05.12 article 28	<u>Ashley Lane, Cornhill Drive/Ash-Hill Drive, Craigshaw Crescent, Midstocket View, North Deeside Road (Lane At 229/231), Peterculter, North Deeside Road, Peterculter, North Deeside Road, Peterculter, Old Skene Road, Kingswells, Oscar Road, Provost Graham Avenue, Royal Court, Queen's Road, Summerhill Road, Virginia Street, Pharmacy/Residential Development At Learig, Kingswood Drive, Kingswells And Wellheads Drive, Dyce</u>	<p>These schemes have gone out for initial statutory consultation and no objections were received therefore these will now proceed to public advert and return to a future committee.</p>	<p>Hugh Murdoch</p> <p>Jane MacEachran</p>	6.11.12	

The Committee resolved to:-

(i) approve the proposals, in principle, with the exception that officers promote the proposed 'at any time' waiting restrictions on the west side of North Deeside Road (Lane at 229/331), Peterculter, as well as the east side, and instruct officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required, and if no significant objections were received, then to progress with the public advertisement and report the results to a future meeting of the Enterprise,

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
		<p>Planning and Infrastructure Committee; and instruct officers to commence the combined statutory consultation for the traffic order for the list of Blue Badge parking spaces and to report back to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>				
15.	Enterprise, Planning and Infrastructure 31.05.12 article 31	<p>Strategic And Local Transportation Projects Update Report</p> <p>The Committee resolved, amongst other things, to:-</p> <p>(i) instruct the Director of Enterprise, Planning and Infrastructure to bring a further report to the Enterprise, Planning and Infrastructure Committee reporting on any potential conflict between the Development Framework and the preferred option for a permanent site for the Bridge of Don Park and Ride;</p> <p>(ii) agree that in the interim the current site continue as a temporary Park and Ride location; and</p> <p>(iii) request that the Director of Enterprise, Planning and Infrastructure further discuss all possible site opportunities and</p>	There is an update report included on this agenda.	Margaret Bochel	6.11.12	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
16.	Housing and Environment 22.05.12 article 10	<p>provide a detailed report back on all options to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p> <p><u>Housing Car Parking – Permit And Enforcement Consultation Results</u></p> <p>The Housing and Environment Committee agreed, amongst other things, to defer recommendation (d) pending a committee report on parking and a broader report on parking policy which will incorporate the Housing Revenue Account and parking that the Enterprise, Planning and Infrastructure Service is responsible for.</p>		Hugh Murdoch Donald Urquhart	6.11.12	22.01.13
17.	Enterprise, Planning and Infrastructure 11.09.12 article 16	<p><u>City Events Programme 2013/14</u></p> <p>The Committee agreed:- (1) that officers report back to the Committee in early 2013 providing information in relation to the Festive Communities Grant, specifically identifying the benefits of the grant, outlining previous and present uptake figures and geographical spread, and advising what the grant monies had been spent on previously; and (2) that officers review the arrangements for the Santa Parade and Christmas Lights Switch-on</p>		G Brough	22.01.13	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
	Finance and Resources 04.10.12 (article 22)	<p>event, specifically with an option that they be held jointly, and report back to the Committee in early 2013 in this regard.</p> <p>Note that officers will submit a report to the Enterprise, Planning and Infrastructure Committee prior to the end of February 2013, assessing the potential for a 2013 Hogmanay event and requesting approval of an appropriate budget to deliver a safe, inclusive and enjoyable family event in 2013.</p>				
18.	Enterprise, Planning and Infrastructure 11.09.12 article 20	<p><u>Management of Events in Parks and Open Spaces</u></p> <p>The Committee agreed to request that officers report back to the appropriate Committee on how future events of the type described are to be managed in Park and Ride sites.</p>		G Brough	22.01.13	
19.	Enterprise, Planning and Infrastructure 11.09.12 article 21	<p><u>Visit Aberdeen Business Plan</u></p> <p>The Committee agreed to request officers to submit a report to the next meeting of the Committee, confirming the support needed from the Council to enable Visit Aberdeen to undertake the activities, and deliver the outputs, set out in their business plan.</p>	A report is included on the agenda for consideration.	G Brough	06.11.12	06.11.12

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
20.	Enterprise, Planning and Infrastructure 11.09.12 article 23	<u>Aberdeen The Smarter City: A Strategy for 2020</u> The Committee agreed to instruct officers to report back to the Committee in August 2013 with the strategy and implementation plan for approval		M. Bochel	August 2013	
21.	Enterprise, Planning and Infrastructure 11.09.12 article 24	<u>West Huxterstone, Kingswells Masterplan</u> The Committee agreed :- (1) to delay consideration of the report until the next meeting of the Committee, during which time officers to consult with Kingswells Community Council on the Masterplan; and (2) that officers be instructed to report back to the next meeting of the Committee on the masterplanning process and to also receive a presentation in this regard	There is a report included on this agenda.	M. Bochel	06.11.12	
22.	Enterprise, Planning and Infrastructure 11.09.12 article 25	<u>Various Small Scale Traffic Management and Development Associated Proposals (New Works)</u> The Committee agreed:- (1) to instruct the appropriate officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation		H Murdoch	Date to be confirmed.	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
		<p>orders required as described in the report. If no significant objections are received, then progress with the public advertisement and report the results to a future meeting of this Committee;</p> <p>(2) to instruct the appropriate officers to commence the combined statutory consultation for the traffic regulation order for the list of Blue Badge parking spaces and report back to a future meeting of this Committee.</p> <p>(3) in terms of the proposal for Foresterhill Road, to note in relation to Councillor Cormie's request that the speed limit extension be monitored, that as part of the statutory consultation process officers would receive feedback from partners including Grampian Police and NHS Grampian, and would report back to the Committee following the consultation period.</p>				
23.	Enterprise, Planning and Infrastructure 11.09.12 article 26	<p>(1) The Aberdeen City Council (A96/Old Meldrum Road/Mugiemoss Aberdeen) Pedestrian/Cycle Path (Amendment) Order 2012; and</p> <p>(2) The Aberdeen City Council (Dubford/Greenbrae Area) (Various Footways)(Redetermination of Means of Exercise of Public Right</p>		H Murdoch	September 2014	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
		<p><u>of Passage) Order 2012.</u></p> <p>The Committee agreed that officers be instructed to report back in two years time on how these Traffic Orders have progressed.</p>				
24.	Enterprise, and Infrastructure 11.09.12 article 29	<p><u>Extension of Pay by Phone Parking</u></p> <p>The Committee agreed to instruct officers to report back on the feasibility of paying parking charges by credit/debit cards at parking machines</p>		H. Murdoch	22.01.13	
25.	Enterprise, and Infrastructure 11.09.12 article 32	<p><u>Palmerston – Revised Traffic Management and Controlled Parking Zone Proposals</u></p> <p>The Committee agreed:- (1) to instruct officers to progress the preliminary design of the traffic management proposals and Controlled Parking Zone; (2) to instruct officers to establish cost estimates for the traffic management proposals and Controlled Parking Zone and develop a business case for the Controlled Parking Zone; and (3) to instruct the appropriate officers to report back on the findings of the above to a future Committee meeting.</p>		H. Murdoch	Date to be confirmed	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
26.	Enterprise, Planning and Infrastructure 11.09.12 article 33	<p><u>Permit Misuse Policy</u></p> <p>The Committee agreed to instruct officers to report back to the Committee in November on the levels of misuse and with draft policies, procedures, actions and any financial implications arising from resources to address the misuse of parking permits and Blue Badges.</p>		H Murdoch	06.11.12	
27.	Enterprise, Planning and Infrastructure 11.09.12 article 38	<p><u>Bus Shelter Advertising Contract</u></p> <p>The Committee agreed:- (1) to instruct officers to enter negotiations with Clear Channel with the aim of agreeing a reduced term for the Bus Shelter Advertising Contract; and (2) to instruct officers to report back on the outcome of the above negotiations and with a plan for retendering the Contract and Variation</p>	<p>Letter to Clear Channel has been drafted and is with the Legal team for progressing.</p>	M Bochel	22.01.13	
28.	Enterprise, Planning and Infrastructure 11.09.12 article 4	<p><u>Speed Limit Review</u></p> <p>The Committee agreed:- (1) to instruct the appropriate officers to commence the necessary legal procedures of preliminary statutory consultation for the traffic regulation orders required as described in the</p>		Hugh Murdoch	22.01.13	

<u>No.</u>	<u>Minute Reference</u>	<u>Committee Decision</u>	<u>Update (new updates in bold)</u>	<u>Lead Officer(s)</u>	<u>Report Due</u>	<u>Report Expected (if known)</u>
		<p>report. If no significant objections are received, then progress with the public advertisement and report the results to a future meeting of this Committee;</p> <p>(2) to report back on the feasibility and costs for converting existing 20's Plenty Zone speed limits to mandatory 20mph speed limits; and</p> <p>(3) to report back on reducing the speed limit on (1) the A944 city boundary at Westhill to Hazlehead from 50mph to 40mph, including the possible introduction of fixed speed cameras for enforcement and all costings associated with the proposal, consulting with Aberdeenshire Council in the production of this report; and (2) Skene Road from 50mph to 40mph as a result of the West Huxterstone, Kingswells development, including all associated costings.</p>				
29.	Enterprise, Planning and Infrastructure 11.09.12 article 13	<p>Traffic Management</p> <p>The Committee agreed that officers report back to the Committee on the feasibility of utilising the projected underspend within the revenue budget in 2012/13 for road, pavement and street lighting repairs.</p>		Hugh Murdoch	22.01.13	

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ENTERPRISE, PLANNING AND INFRASTRUCTURE

MOTIONS LIST - 11 SEPTEMBER 2012

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
1.	<p><u>Motion by Councillor Yuill</u></p> <p>“That Aberdeen City Council agrees to instruct officers to prepare a report on both the feasibility of developing, in partnership with Aberdeenshire Council and Perth and Kinross Council, a long distance footpath – The Royal Deeside and Perthshire Way – from Fittie to Perth via Deeside and ways in which this project might be funded.”</p>	27.04.11	<p><u>Enterprise, Planning and Infrastructure 13.09.11</u></p> <p>Amongst, other things, to request officers to participate in the Royal Deeside, Angus and Perthshire Way Steering Group which was already discussing the establishment of the “Pictish Way”, and that officers report back to the next meeting of the Enterprise, Planning and Infrastructure Committee regarding the terms of the motion, and in particular detailing the outstanding aspects required to establish and signpost the proposed “Pictish Way”, as well as the financial cost of achieving this.</p>	<p>At its meeting on 31 January 2012, the Committee resolved to:-</p> <p>(i) instruct officers to monitor developments on the Pictish Way by partners and attend Steering Group meetings (so far held in Forfar) where resources allowed; and</p> <p>(ii) instruct officers to report back to the Enterprise, Planning and Infrastructure Committee after the summer recess on the findings of a report that Angus Council intended to commission into the development of the route.</p> <p>Recommendation (b) of the report EPI/12/030 to the 31st January meeting was that officers report back to this Committee on the findings of a feasibility study commissioned by Angus Council. A finalised copy of that study is not yet</p>	<p>Head of Planning and Sustainable Development</p>	<p>31.05.12</p> <p>Now due 06.11.12</p>	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
2.	<p><u>Motion by Councillor Yuill</u></p> <p>“That, given the ongoing difficulties caused by HGVs using Broomhill Road as a through route, Council instructs officers to report to the appropriate committee on the introduction of an ‘except for access’ weight or width restriction on some or all of Broomhill Road between Holburn Street and South Anderson Drive.”</p>	17.08.11	<p><u>Enterprise, Planning and Infrastructure 13.09.11</u></p> <p>To request officers to report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee.</p>	<p>available however, and a meeting of the Steering Group made up of the various authorities involved, to discuss the findings and agree a way forward, is yet to take place. It is therefore proposed that a report will be presented to the 6th November meeting.</p> <p>At its meeting on 31 January 2012, the Committee resolved to request officers to explore other options available, not excluding the introduction of an “except for access” width and/or weight restriction, to address the concerns of residents on Broomhill Road, and to report back to a future meeting of the Enterprise, Planning and Infrastructure Committee on these options, as well as the implications of the introduction of each.</p> <p>At its meeting on 31 May 2012, the Committee resolved to:- (a) agree that, given the strategic importance of</p>	Head of Asset Management and Operations	31.5.12	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
				<p>Broomhill Road within the road network and the proportionately small percentage of HGVs that were using Broomhill Road as a through route, restricting LGVs/HGVs along Broomhill Road would not be appropriate;</p> <p>(b) agree that no action be taken at this time; and</p> <p>(c) and to request officers to undertake further speed, volume and parking surveys on Broomhill Road following the implementation of the new puffin crossing and report back to the Committee with this information.</p>			

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
3.	<p><u>Notice of Motion by Councillor Townson</u></p> <p>“Council agrees to develop a youth employment strategy for Aberdeen in consultation with relevant public and private organisations.”</p>	13.06.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee</p>	<p>Officers are still working on this. A preliminary report will be submitted to Committee outlining the scale of the issue and some possible actions that will require further assessment as part of any strategy development process</p>	Economic & Business Development	06.11.12	No
4.	<p><u>Notice of Motion by Councillor Malone</u></p> <p>“To instruct the Director of Enterprise, Planning and Infrastructure to support the request by the International School of Aberdeen and install a safe pedestrian crossing similar to other crossings on North Deeside Road at the entrance of the School. At present there is no safe crossing and pupils and community members must negotiate a very busy road.”</p>	13.06.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>The Committee agreed to defer consideration of this item until the next meeting.</p>		Asset Management & Operations		No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
5.	<p><u>Notice of Motion by Councillor Jennifer Stewart</u></p> <p>"Council instructs officers to report on the feasibility and implications of removing/ending on street Sunday parking charges"</p>	22.08.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee</p>		Asset Management & Operations	06.11.12	No
6.	<p><u>Notice of Motion by Councillor Yuill</u></p> <p>"That this Council agree to instruct officers to report to the appropriate committee on the feasibility of providing local communities with the materials and equipment necessary to undertake snow clearing and gritting and that this report should seek to include information about such schemes operating elsewhere in Scotland and the UK."</p>	22.08.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee</p>		Asset Management & Operations	06.11.12	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
7.	<p><u>Notice of Motion by Councillors Yuill, Delaney and Malone</u></p> <p>“That this Council agrees to write to the Scottish Government and Aberdeen’s MSPs calling for a change in the regulatory regime for bus operators providing scheduled public services to introduce a ‘public service obligation’ which would require any bus operator providing more than 50% of the bus services in a particular local authority area to provide a basic level of bus service (including in evenings and at weekends) to communities across that local authority area.”</p>	22.08.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee</p>		<p>Planning & Sustainable Development</p>	06.11.12	No
8.	<p><u>Notice of Motion by Councillor Yuill</u></p> <p>“That this Council:- 1. Expresses concern at the impact the withdrawal of evening bus services to Airyhall will have on</p>	22.08.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee</p>		<p>Planning & Sustainable Development</p>	06.11.12	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
	<p>residents of that community, including increased social isolation for some.</p> <p>2. Instructs officers to report to the relevant committee on the feasibility and likely cost of introducing a City Council supported evening bus service to Airyhall and to further report on options for funding such a supported service.”</p>						
9.	<p><u>Notice of Motion by Councillor Greig</u></p> <p>“That this Council produces a report on identifying funding sources for the bus services which are scheduled for withdrawal from Seafield and Craigiebuckler.”</p>	22.08.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>To request officers to submit a report on the terms of the motion to a future meeting of the Enterprise, Planning and Infrastructure Committee</p>		<p>Planning & Sustainable Development</p>	06.11.12	No

<u>No.</u>	<u>Motion</u>	<u>Date of Council Meeting</u>	<u>Committee Motion referred to / date/ decision of Committee</u>	<u>Action taken / Proposed Future Action</u>	<u>Responsible Head(s) of Service</u>	<u>Due Date</u>	<u>Is authority sought to remove motion from list?</u>
10.	<p><u>Notice of Motion by Councillor Delaney</u></p> <p>“That this Council:-</p> <ol style="list-style-type: none"> 1. Deplores First Aberdeen’s decision to terminate the Park & Ride service from Kingswells at Aberdeen Royal Infirmary. 2. Instructs officers to seek a meeting with First Aberdeen to emphasise the need for effective Park & Ride services in order to reduce congestion caused by commuter traffic and their role in achieving this. <p>Instructs the Chief Executive to seek a meeting with the Transport Minister and Group Leaders to discuss recent reductions in bus frequencies across the city and First Aberdeen’s failure to deliver on local and national transportation policies in Aberdeen.”</p>	22.08.12	<p><u>Enterprise, Planning and Infrastructure 11.09.12</u></p> <p>Following a vote the Committee agreed to adopt the following amendment to the motion:-</p> <p>That this Council (1) regrets First Aberdeen’s decision to terminate the Park & Ride service from Kingswells at Aberdeen Royal Infirmary; (2) instructs officers to seek a meeting with First Aberdeen to emphasise the need for effective Park & Ride services in order to reduce congestion caused by commuter traffic and their role in achieving this; and (3) instructs the Chief Executive to seek a meeting with the Transport Minister and Group Leaders to discuss recent reductions in bus frequencies across the city.</p>	<p>(2) Officers have already raised concerns with First and highlighted the importance of P&R services. A further specific meeting to discuss P&R is to be arranged, we propose to wait until we have at least 1 months data of P&R usage since the network review, so that this can be used as a comparator.</p>	Planning & Sustainable Development	06.11.12	No

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